

BRITISH EUROPEAN AIRWAYS

OPERATIONS MANUAL

Vol III

FLIGHT PLAN DATA

BRITISH EUROPEAN AIRWAYS

BEA

OPERATIONS MANUAL VOL. IV. A.

AERODROME INFORMATION

AREA 1 U.K. & N.W. EUROPE

The information contained in this part of the Operations Manual is for use in connection with BEA operations in accordance with the standards, limitations and procedures established by BEA. The information should not therefore be used in connection with the operations of any other person or organization.

BRITISH EUROPEAN AIRWAYS

AERODROME INFORMATION

(OPERATIONS MANUAL—VOL IVA)

EXPLANATORY NOTES I

EXPLANATORY NOTES

1 INTRODUCTION

- 1.1 The BEA Aerodrome Information Books (Volume IVA) are part of the BEA Operations Manual. There are three books each containing diagrams and information sheets for aerodromes within three areas of Europe as defined on the Key Sheet. The size of the books and the format chosen for this data has been determined by the size of Chart Holder which is being installed in BEA aircraft.
- 1.2 The contents of the books are comprised of :-
- 1.2.1 Instrument Approach Procedures in Plan and Profile.
 - 1.2.2 Aerodrome Charts showing the layout of all runways and taxiways etc., together with details of aerodrome lighting.
 - 1.2.3 Emergency Communications Procedures for all areas and aerodromes contained in the volumes.
 - 1.2.4 Descriptive sheets showing the ICAO runway Marker Board System.
 - 1.2.5 Descriptive sheets showing the ICAO Visual Approach Slope Indicator System and the VASI System used on French Airports.
 - 1.2.6 Area Key Sheet defining the areas of Europe to which each of the Aerodrome Information books applies.
 - 1.2.7 Legend of conventional signs used on the sheets contained in the Aerodrome Information books.
 - 1.2.8 Miscellaneous sheets which refer to special procedures in force at individual aerodromes, such as parking arrangements, noise abatement procedures, cross wind limits, special taxiway routeings etc.

BRITISH EUROPEAN AIRWAYS

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Presentation and
minor text amendment

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EXPLANATORY NOTES I

Prepared and issued by Flight Operations
Dept as part of the Operations Manual.

2 DESCRIPTION OF SHEETS

2.1 Instrument Approach Procedures

2.1.1 Format

The form of portraying Instrument Approach Procedures follows the established lines laid down by ICAO and adopted by most Aerodrome Authorities. Plan and Profile Sections for each procedure have been drawn to the maximum scale possible, and the heights which have to be maintained in the execution of each procedure are clearly shown at the appropriate position.

2.1.2 Obstructions

Information on obstacles has been obtained from data issued by each Aerodrome Authority where this is available, and from reliable sources where it has not been possible to obtain official information. Some measure of selection has been applied, where possible, to avoid undue clutter.

Some diagrams showing ILS Procedures do not show any obstructions, because official sources have not provided the information. Replacement sheets for these Procedures showing obstacles will be issued as the information becomes available.

2.1.3 Height of Obstructions

Two heights are shown against each obstruction or spot height. These represent the height above mean sea level and the height above the lowest R/W threshold.

2.1.4 Sector Safe Altitudes

Safety altitudes above mean sea level are shown in each quadrant of the Instrument Approach Procedure with the exception of some ILS Procedures.

EXPLANATORY NOTES (Continued)

EXPLANATORY NOTES 3

2.1.4 (contd.)

The height shown represents the safe altitude within 25 n.m. of the main facility allowing vertical clearance of 1,000 ft. over the highest obstacle in the quadrant. In deciding these altitudes an extra 5 n.m. all round the sector is taken into account.

2.1.5 Distance

Distances are shown in metres or nautical miles in accordance with the guidance information on dimensional units issued by ICAO.

2.1.6 R/W Elevations

In addition to "Official Aerodrome" and "Lowest Threshold" elevations, the height of the R/W threshold with which the procedure is aligned is also stated.

2.1.7 Frequencies

Approach, Tower, and all relevant facility frequencies are shown on each chart. GCA/PAR, Ground Control and ILS glide path frequencies are not shown, the details of these are in the Facility Listings, (IAL Supplement to Flight Guides - Operations Manual Vol. IVC).

2.1.8 Missed Approach

Instructions and Notes respecting missed approach procedures are shown in the right hand panel of the chart.

2.2 Aerodrome Charts

All lighting information is grouped in the right-hand panel; the panel along the bottom edge being reserved for special notes, warnings, etc. It is intended to incorporate on these sheets all possible aerodrome information at present promulgated in various parts of our technical data.

Horizontal distances are shown in metres.

2.3 Miscellaneous Sheets

These sheets fall, in general, into the following categories :-

EXPLANATORY NOTES (Continued)

EXPLANATORY NOTES 4

2.3 (contd)

- 2.3.1 Taxiway and Parking Area diagrams.
- 2.3.2 Diagrams defining BEA cross wind limits for particular aerodromes.
- 2.3.3 Visual Approach and Manoeuvring charts.
- 2.3.4 Noise Abatement procedures.
- 2.3.5 Inbound, Outbound and/or transit routeing sheets.
(There are 14 Standard Instrument Departure (SID) Charts and 4 Area Charts in the Chart Wallet (BEA Operations Manual IVB). Copies of these charts are also issued with the Flight Plan Data issued for each Service.

3 AMENDMENT SERVICE

When official information regarding changes affecting sheets contained in this Volume is received, replacement sheets will be produced and inserted in each book as quickly as possible. Amendment lists will be issued twice weekly. Information will be retained on amendment lists until it is known that the appropriate replacement sheet has been inserted into all copies of the Aerodrome Information Books carried on our Aircraft.

Amendment lists will not contain details of temporary unserviceabilities of short duration. When a facility is withdrawn for a definite period, action will be taken to withdraw or amend the sheet if the period justifies this action. For unserviceabilities of up to six weeks it is more satisfactory to leave the data in the volumes on the basis that pilots will become aware of the period of unserviceability from the airport briefing information service (AIS). When serviceabilities are promulgated with the category UFN (until further notice) every effort will be made to obtain an estimate of the expected duration, following which each case will be dealt with in accordance with the circumstances.

4 CHARTER OPERATIONS, SPECIAL FLIGHTS ETC.

The composition of this volume has been designed to meet the requirements of BEA scheduled services. Aerodromes included are those which are contained in current BEA technical data. Enquiries should be made through the Aeronautical Information Service for information regarding any other airport.

BRITISH EUROPEAN AIRWAYS

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Reason for Re-issue:

Presentation and

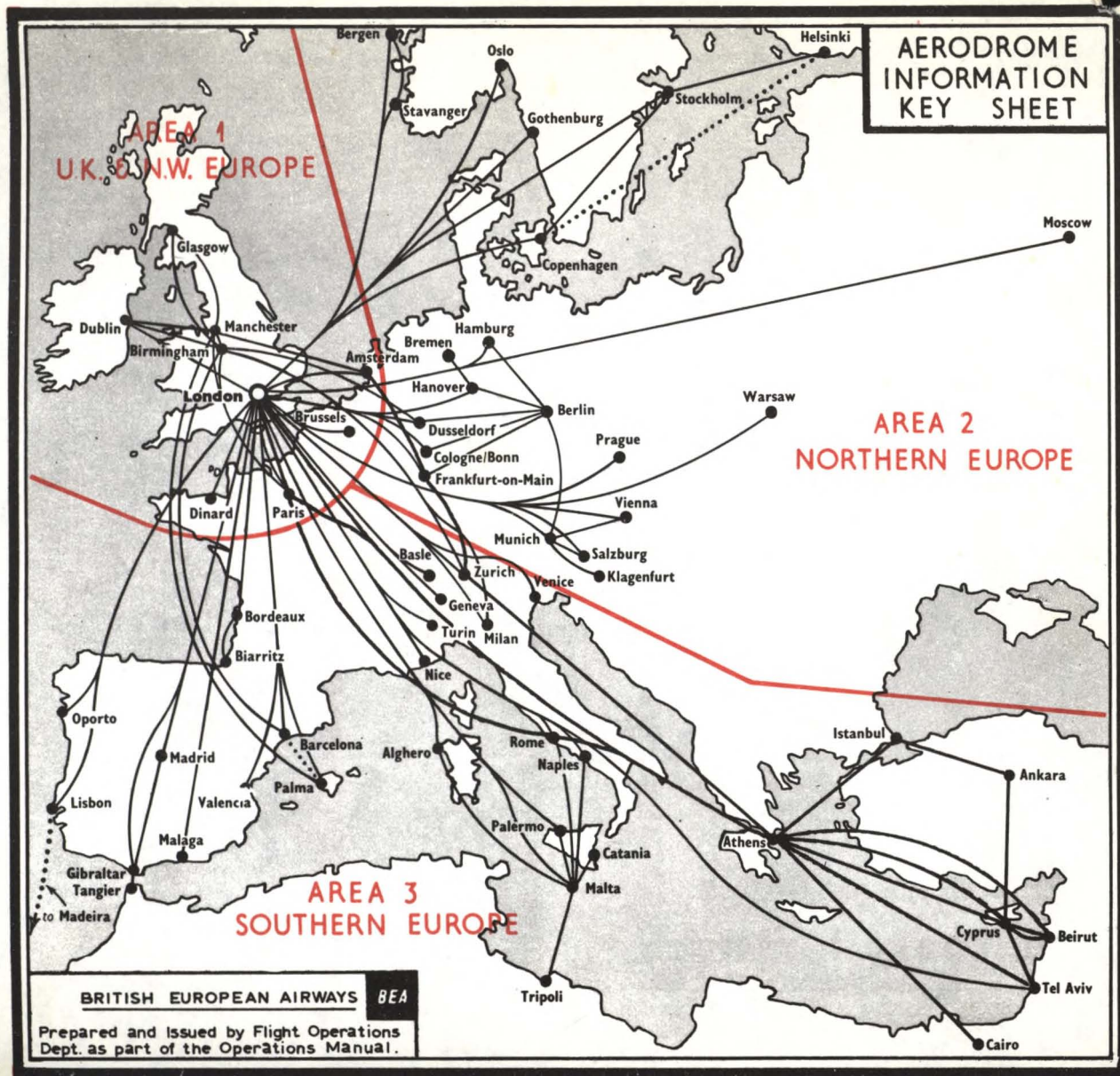
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EXPLANATORY NOTES 4

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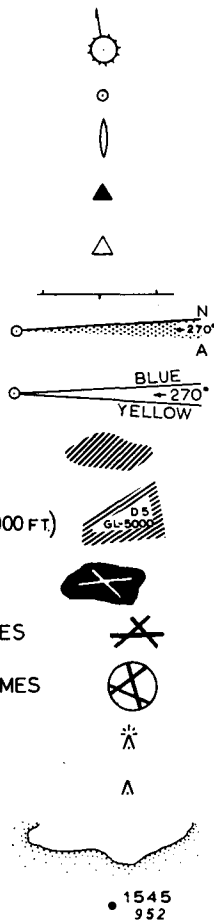
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INSTRUMENT APPROACH & AERODROME CHARTS

LEGEND

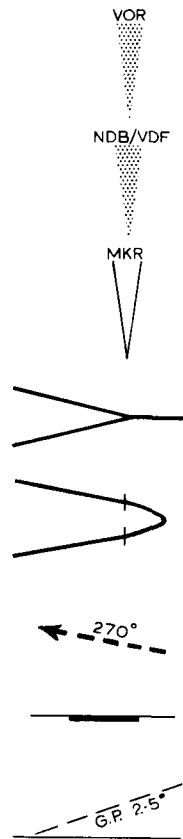
PLAN VIEW

VOR
NDB OR VDF
FAN MARKER
REPORTING POINT
(COMPULSORY)
REPORTING POINT
(ON REQUEST)
F.I.R. BOUNDARY
RADIO RANGE LEG
ILS BEAM
PROHIBITED AREA
DANGER AREA
(eg. GROUND LEVEL TO 5000 FT.)
MAIN AERODROMES
OTHER CIVIL AERODROMES
OTHER MILITARY AERODROMES
OBSTRUCTION (LIT)
OBSTRUCTION (UNLIT)
COAST LINE
SPOT HEIGHTS



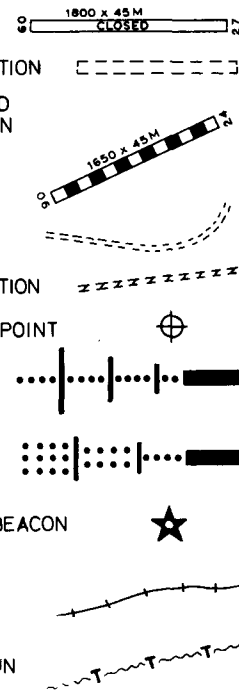
PROFILE VIEW

VOR
NDB OR VDF
FAN MARKER
LEVEL TURN
DESCENDING TURN
MISSED APPROACH
AERODROME BLOCK
ILS GLIDE PATH
(eg. 2° 30')



AERODROME CHART

RUNWAY CLOSED
RUNWAY UNDER CONSTRUCTION
RUNWAY OFFICIALLY CLOSED
BUT AVAILABLE TO CERTAIN
BEA CAPTAINS UNDER
SPECIFIED CONDITIONS
TAXIWAY CLOSED
TAXIWAY UNDER CONSTRUCTION
AERODROME REFERENCE POINT
APPROACH LIGHTING
(CENTRE-LINE & BARS)
APPROACH LIGHTING
(CALVERT)
IDENT. OR AERODROME BEACON
RAILWAYS
OVERHEAD TRANSMISSION
LINES



EMERGENCY COMMUNICATION
PROCEDURES

EMERGENCY 1

The following are the basic communication procedures to be used by aircraft under emergency conditions. Although the circumstances of the emergency will largely dictate the action which can be taken, these procedures should be adhered to as closely as possible.

- 1 DISTRESS - To be used when the aircraft is threatened by grave and imminent danger and in need of immediate assistance.

DISTRESS CALL

MAYDAY MAYDAY MAYDAY

THIS IS

AIRCRAFT CALLSIGN (3 TIMES)

FREQUENCY IN USE

DISTRESS MESSAGE

MAYDAY

AIRCRAFT CALLSIGN AND TYPE

ESTIMATED POSITION

HEADING (TRUE OR MAGNETIC)

INDICATED AIRSPEED

ALTITUDE, FLIGHT LEVEL OR HEIGHT

NATURE OF DISTRESS AND KIND OF ASSISTANCE DESIRED

INTENTIONS OF CAPTAIN AND ANY OTHER RELEVANT INFORMATION

EMERGENCY 1

EMERGENCY 2

The DISTRESS CALL AND MESSAGE should be transmitted on the en-route VHF or HF frequency in use. In the event of no acknowledgement being received from either

(a) the Controlling Authority

or

(b) other stations capable of acting as relay

any intention to change frequency should be broadcast and the new frequency advised.

If no communication contact is achieved the VHF Emergency Channel of 121.5 mc/s should be used and the DISTRESS CALL AND MESSAGE repeated.

2 URGENCY - To be used to indicate that the aircraft has a very urgent message to transmit concerning the safety of a ship, aircraft, or other vehicle, or of some person on board or within sight.

Urgency messages will normally be addressed to the controlling authority or depending on circumstances to more than one or all stations using the en-route VHF or HF frequency.

In the event of no communications contact the VHF Emergency Channel of 121.5 mc/s should be used and the Urgency Message repeated at intervals until a reply is received.

URGENCY SIGNAL

PAN PAN PAN

URGENCY MESSAGE

CALLSIGN OF STATION(S) CALLED

THIS IS

AIRCRAFT CALLSIGN

TEXT

OVER

BRITISH EUROPEAN AIRWAYS

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Complete Revision

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EMERGENCY 2

3 CANCELLATION OF DISTRESS OR URGENCY - If after initiating Distress or Urgency communications procedures, the emergency conditions cease to exist and no further assistance is required, the aircraft must cancel the state of Distress or Urgency. This should normally be done by transmitting a cancellation message giving the necessary information to the station controlling the Distress traffic, who will then be responsible for advising all stations on the frequency in use that the Distress traffic has ended. Alternatively, if circumstances require it, the aircraft may cancel the Distress by a message to "all stations". It is important that the Distress should be cancelled on all frequencies used during the Distress traffic.

4 GENERAL NOTES ON EMERGENCY COMMUNICATIONS

- (a) The Distress Call has absolute priority over all other transmissions and serves to clear the channel for Distress traffic. All stations hearing a Distress call shall immediately cease any transmission which is capable of interfering with the Distress traffic, and shall continue to listen on the frequency used for the Distress call. A Distress call should not normally be acknowledged until after the Distress Message has been sent.
- (b) An aircraft which is not in a position to render assistance, but which has heard a Distress Message which has not been acknowledged immediately should take all possible action to attract the attention of other stations which are in a position to render assistance. In these circumstances an aircraft may re-broadcast a Distress Message, prefixed with the Distress Call MAYDAY RELAY, its own callsign (three times), followed by the intercepted Distress Call and Message.
- (c) As a general rule, aircraft stations intercepting a Distress message should not reply to it UNLESS there is no immediate reply from an appropriate ground station, or other station which is in a better position to render assistance. If necessary, an aircraft should act as a relay between Distressed aircraft and appropriate control station.

EMERGENCY 4

- (d) Control of Distress traffic is the responsibility of the station in Distress, which may delegate control to another station (normally appropriate A.T.C. station when Distress communications have been established). The control station and aircraft in Distress have authority to impose radio silence on any or all stations on the frequency used for Distress traffic by the use of the phrase STOP TRANSMITTING - DISTRESS.
- (e) The frequency of 123.1 mc/s may be used as an auxilliary frequency to 121.5 mc/s for search and rescue operations.

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Reason for Re-issue:

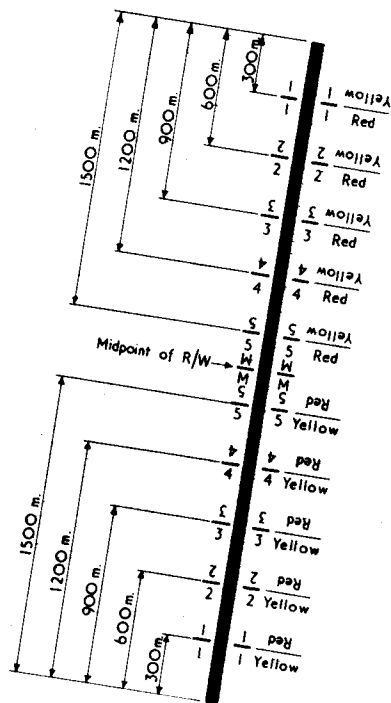
Complete Revision

SERIAL No. 40225

DATE: 20.10.64.

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EMERGENCY 4



TYPICAL LAYOUT

R/W MARKER BOARD SYSTEM

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NOTES

1. The marker boards are 4½ ft. high. The distance between the R/W lights & the marker boards is 14 metres.
2. When seen from the take-off or landing direction the yellow marker boards along the first half of the runway & on both sides of it show the accumulated distance of runway from the threshold, figure '1' that 300M. has been accumulated, figure '2' 600M. & so on. The midpoint of the runway is shown by red boards marked 'M'. From here other red boards show the length of the runway remaining.
3. The precise arrangement of marker boards varies according to the aerodrome and a note on the aerodrome chart will explain any difference from the layout shown above.

SERIAL No. 40060

DATE 25.8.64

Reason for Re-Issue:

R/W MARKER BOARD SYSTEM

STANDARD V.A.S.I SYSTEM

V.A.S.I. 1

1. This system is composed of a number of units which are arranged to form two lighted wing bars on both sides of the runway, the distance between them defining the touch down area. Each wing bar is 32' long and the innermost end is on the runway edge.

The optical system is such that the colour of light projected above the upper sector of the glide slope is white and the lower sector red, the change of colour between the sectors being gradual over approximately a $\frac{1}{2}^{\circ}$ sector, which brackets the ILS or GCA glide path. The red colour is thus used in the natural sense to indicate to a pilot that he is too low.

2. In making an approach the pilot should keep in the white sector of the nearer bars and in the red sector of the further bars in order to touch down in the correct area. If both bars are red he is too low and if both bars are white he is too high and will overshoot. If well below the glide path the red bars will tend to merge into one bold red signal on each side of runway. Pilots must take care not to continue an approach in the pink sector of the nearer bars as this will tend to lead to under-shooting.
3. From a position on the approach slope at maximum range, the white bars of the system may become visible shortly before the red bars. Under certain conditions, the white bars of the system may appear to be yellowish and a pilot on the approach slope may see one red bar and one yellowish bar, and above the approach slope two yellowish bars.

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Reason for Re-Issue:

SERIAL No. 40050

V.A.S.I. 1

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4. It is probable on approaches carried out in conditions of low visibility (RVR 500) the system will not provide visual approach slope indication as both pairs of bars will not be visible until reaching the threshold of the runway. Under these circumstances the facility can only be used to indicate a possible undershoot.
5. System illustrated overleaf.

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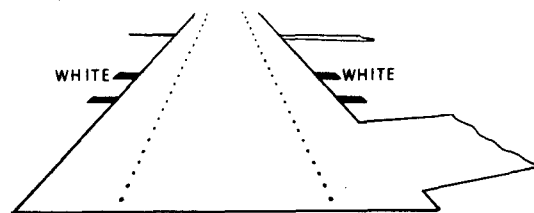
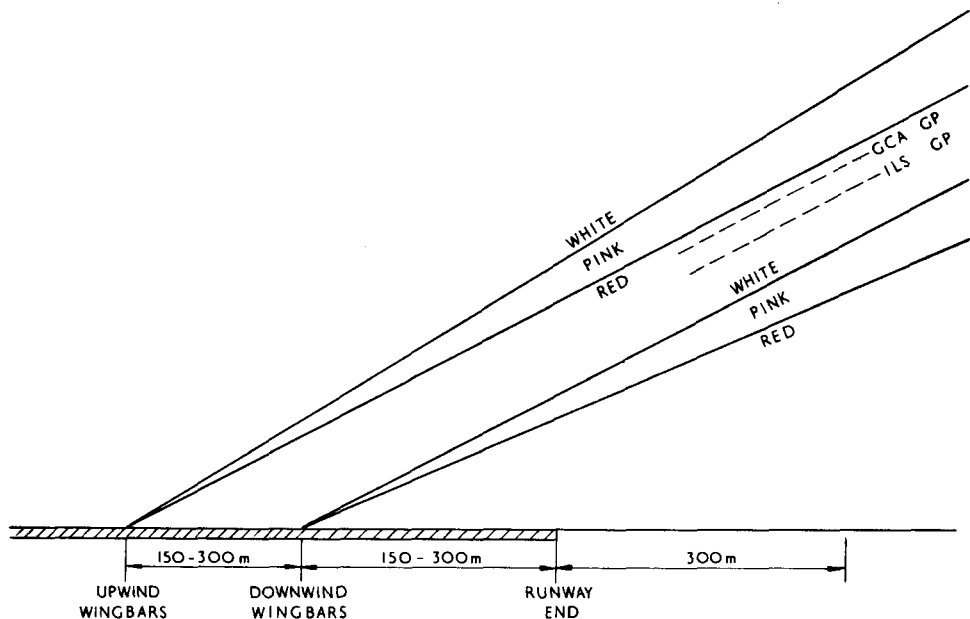
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DIAGRAMMATIC ONLY

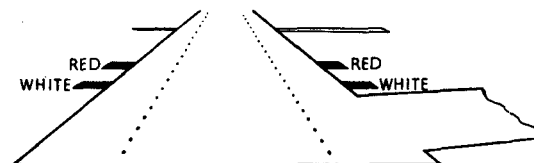
VASI

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Appearance of system when on approach



HIGH



ON THE GLIDE PATH



LOW

- NOTE :-
- (1) RUNWAY LIGHTS ARE INDICATED BY DOTTED LINES
 - (2) WHEN ON THE G.P. THE WIDTH OF THE WING BARS APPEARS EQUAL TO THE SEPARATION BETWEEN THE DOWNWIND AND UPWIND BARS.

SERIAL No. 29350

DATE: 28.7.64

VASI

NON STANDARD V.A.S.I.(FRENCH)

1. The French Authorities have developed and are equipping certain aerodromes in their territories with a V.A.S.I. which does not conform to the I.C.A.O. standard specification.

This system comprises two groups of three lights in the form of a triangle positioned on each side of the approach end of the runway. Initially a runway may only have one group and in this case it will be positioned to the left of the runway.

All lights have white characteristics and glide slope guidance is obtained by the relative brilliance of the apex lights and the base lights of the triangle or triangles.

2. On the correct glide slope all lights will have equal brilliance. If the apex light is brighter the aircraft is above the glide path and if the base lights are brighter it is below the glide path.

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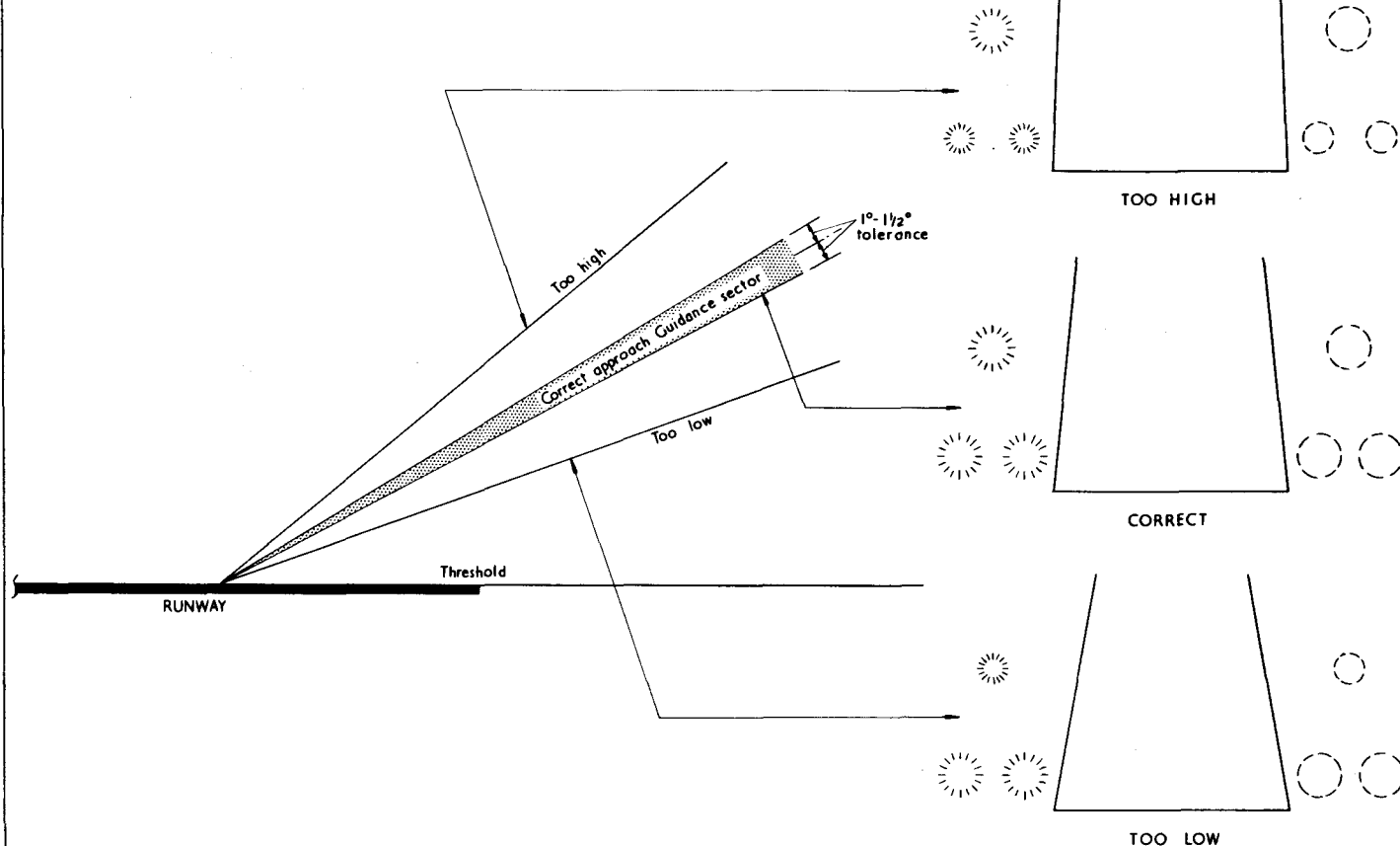
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DIAGRAMMATIC ONLY

FRENCH VASI

SIZE OF CIRCLES INDICATES
RELATIVE BRILLIANCE OF LIGHTS
ALL LIGHTS ARE WHITE

Note.
Right side slope
indicator lights
may not be fitted.



INDICATOR AS SEEN BY PILOT

SERIAL No 29358

DATE 28.7.64

FRENCH VASI

ABERDEEN AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
17	H.I. WHITE & L.I. RED CENTRE LINE & 2 BARS	3°
35	H.I. WHITE CENTRE LINE & 5 BARS L.I. RED TEE	3°
06/24	LOW INT. RED CENTRE-LINE & BAR	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
17/35	GREEN	VAR. INT. OMNI-DIREC- TIONAL
06/24	GREEN	ELEVATED OMNI-DIREC- TIONAL

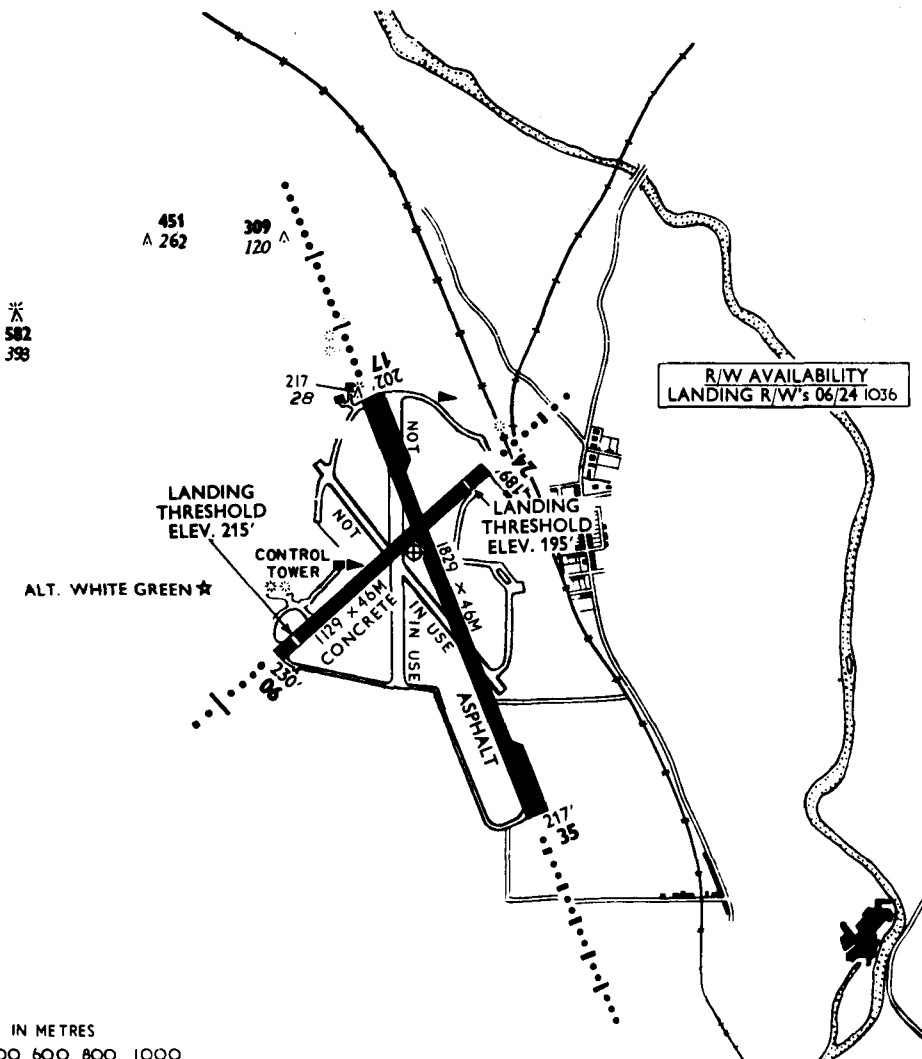
OBSTRUCTION, TAXIWAY, ADME BEACON

ELEVATIONS (FT) :-

OFFICIAL AERODROME	235
LOWEST THRESHOLD (24)	189
LAT 57° 12' N, LONG 02° 12' W	
VARIATION 10° W	Bearings all magnetic
SERIAL No. 50242/1	DATE: 2.6.65
Reason for Re-issue:	

LIGHTING ETC

AERODROME CHART
ABERDEEN



R/W AVAILABILITY
LANDING R/W's 06/24 1036

SCALE IN METRES

0 200 400 600 800 1000

NOTE:

- RIGHT HAND CIRCUITS R/W 35 WHEN GLIDER FLYING IN PROGRESS.
- WARNING** UNCONTROLLED TRAFFIC ON ROAD 91 M BEFORE THRESHOLD R/W 06.

SECT. SAFE ALT
25NM 3400

30° 20° 10° 02° 00'W

SECT SAFE ALT
25NM 2300

NOTES

1. Full scale deflection of localiser may not be maintained within sectors 10 to 20 degrees either side of centre line.
2. Narrow beam type localiser.

OLD MELDRUM
TV MAST
1290
1101

HOLDING AND
LET-DOWN PATTERN
MIN. HOLDING ALT. 2000'

YELLOW

BLUE

NDB
ADN377

LOM

MM

ILS
ADN109.9

NO BACK BEAM

57°N
20'

57°N
10'

SECT SAFE ALT
25NM 3600

SECT SAFE ALT
25NM 1900

ABERDEEN
ILS R/W 17

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APPROACH ABERDEEN APP

120.9

TOWER ABERDEEN TWR

118.3

TRANS ALT 3000 FT.

G/S(KTS)	70	90	110	130	150
LOM TO TURN	2.09	1.40	1.22	1.09	1.00
LOM TO T/H	3.26	2.33	2.05	1.46	1.32
DESCENT ON G/P (FT/MIN)	370	480	590	690	800
G/S	170	190	210	230	250
LOM TO TURN	0.53	0.47			
LOM TO T/H	1.21	1.12			
DESCENT ON G/P (FT/MIN)	900	1010			

OVERSHOOT

CLIMB AHEAD
TO 2000 /1811

ELEVATIONS (FT) —

OFFICIAL AERODROME	235
LOWEST THRESHOLD	189
THRESHOLD R/W 17	202

VARIATION 10°W Bearings all magnetic
SERIAL No. 50242/1 DATE 2.6.65

Reason for Re-issue:

PROCEDURE HTS

ILS R/W 17
ABERDEEN

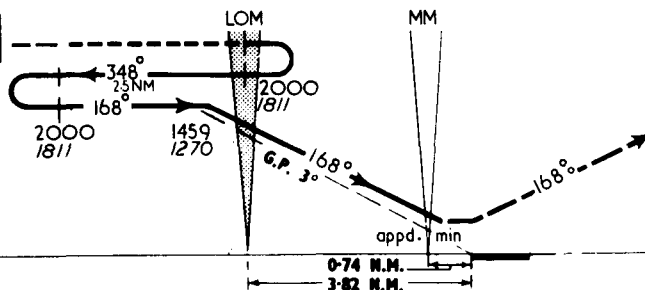
Initial approach
min en route alt

Shuttle on
holding pattern

Rate one
turn left

HGT. at MM when
GP. inoperative
599 410

HGT at MM 479 290



Runway Availability
R/W 01 : T/O & Landing 2915m. (last 240m. not available).
R/W 19 : Landing 2915m. (first 240m. not available).

69
82

⁷⁹
A 92

BEA

LIGHTING

V.A.S.
I CAC
Type
Appk

R/W	APPROACH	Type Angle
19	H.I. & L.I. WHITE CENTRE-LINE & BARS (1 RED) SEQUENCED FLASHERS ON OUTERMOST 150M	
23	H.I. & L.I. WHITE CENTRE-LINE & BARS (1 RED)	
01	H.I. WHITE CENTRE-LINE & BARS (1 RED) L.I. RED LINE (LEFT SIDE)	
05	L.I. RED LINE (LEFT SIDE)	
32	L.I. RED LINE (LEFT SIDE)	
14	NIL	
24	NIL	
06	L.I. YELLOW CENTRE-LINE & BAR	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
19 23 01 05	GREEN OMNI-DIRECTIONAL	WHITE VAR. INT. OMNI-DIRECTIONAL (LAST 610M YELLOW)
32 14 24 06	GREEN OMNI-DIRECTIONAL	WHITE H.I. OMNI-DIRECTIONAL (LAST 610M YELLOW)

OBSTRUCTION, TAXIWAY, ADME BEACON
LANDING INDICATOR, FLOODLIGHTS

ELEVATIONS (FT) :—

OFFICIAL AERODROME	13' BELOW SEA LEVEL
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LAT 52° 19' N, LONG 04° 47' E

VARIATION	5° W	Bearings all magnetic
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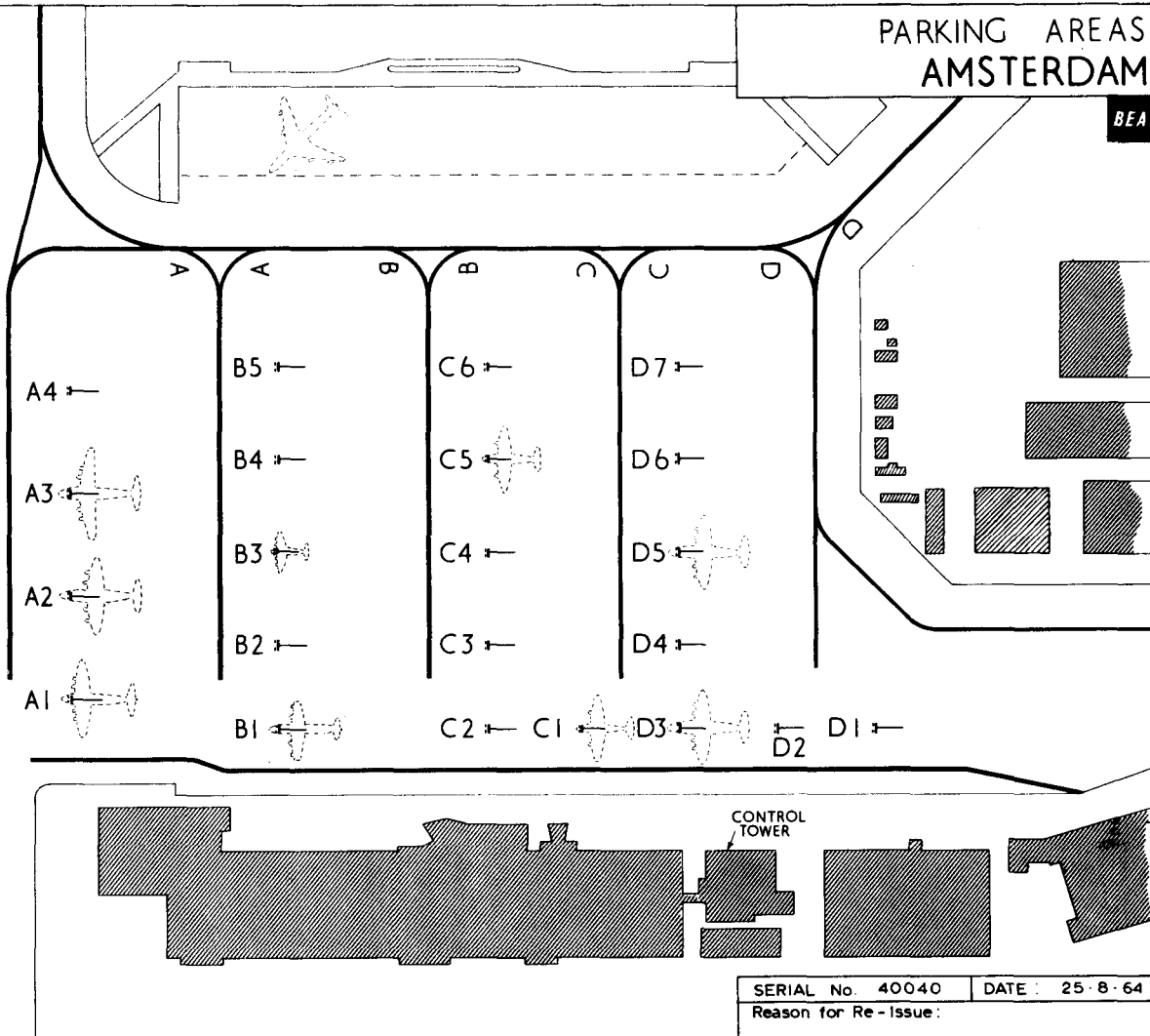
Reason for Re - Issue:

OBST. 79' ADDED

AERODROME CHART
AMSTERDAM (SCHIPHOL)

PARKING AREAS AMSTERDAM

BEA



CONTROL
TOWER

SERIAL No. 40040

DATE: 25-8-64

Reason for Re-issue:

TAXYING ROUTES ARE INDICATED BY WHITE REFLECTING LINES JOINING THE CENTRE LINE MARKING OF THE TAXIWAYS.
THE ROUTES TO BE FOLLOWED ON THE PLATFORM WILL BE SIGNALLLED BY THE MARSHALLER. AIRCRAFT SHALL FOLLOW THE INDICATED
ROUTE LINE ON THE PLATFORM UNTIL SIGNALLED BY THE MARSHALLER INTO THE ALLOCATED PARKING STAND.
DEPARTING AIRCRAFT AFTER LEAVING THE PARKING STAND SHALL FOLLOW THE ROUTE-LINE IN THE DIRECTION OF THE RUNWAY IN USE.

PARKING AREAS AMSTERDAM

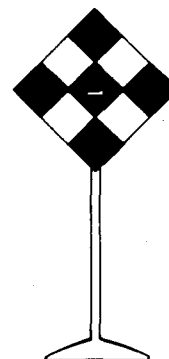
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BARRA AERODROME CHART

DIAGRAM OF
MARKERS 'A' 'B' & 'C'



BLACK & WHITE
SQUARES
LETTERS OF MARKERS
WHITE ON BLACK
BACKGROUND

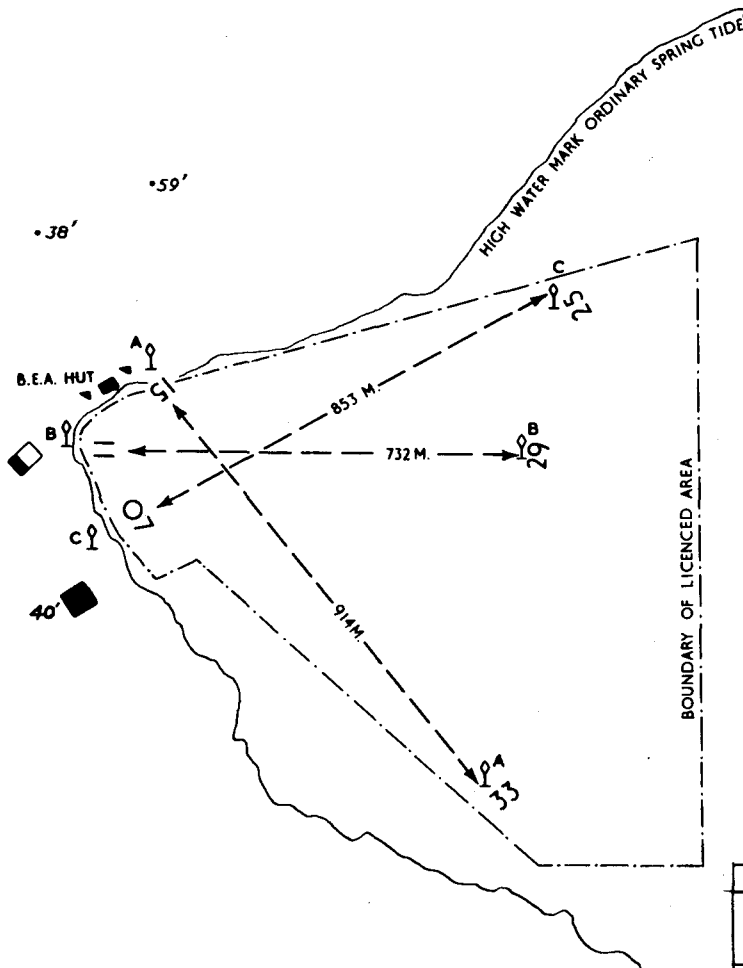
SERIAL No. 40226/1 DATE: 7.12.64

Reason for Re-issue:

REVISED AREA BOUNDARY

AERODROME CHART BARRA

SCALE IN METRES
100 0 100 200 300 400 500



BARRA

SPECIAL OPERATING CONDITIONS FOR TAKE-OFF & LANDING AT BARRA

Flying shall only be carried out when the state of the sands and tides permit, and subject to the following conditions:-

1. High Water

When tide more than $10\frac{1}{2}$ feet above mean sea level. Landings and take-off may not take place within 3 hours of high tide.

2. High Water

When tide is less than $10\frac{1}{2}$ feet above mean sea level. Landings and take-off can be carried out at any time during the day.

3. When consulting tide tables, high tide at Barra is 50 minutes earlier than the tide at Stornoway.

WARNINGS:

1. Under certain weather conditions the landing strip may still be under water during the times stated above.
2. Captains are not to land at this airfield until they have established either by radio with Bealme Barra (118.3 m/cs.) or by telephone, that the sea is clear of the landing run to be used, and is likely to remain clear until the subsequent take-off.

BRITISH EUROPEAN AIRWAYS

BEA

Reason for Re-Issue:

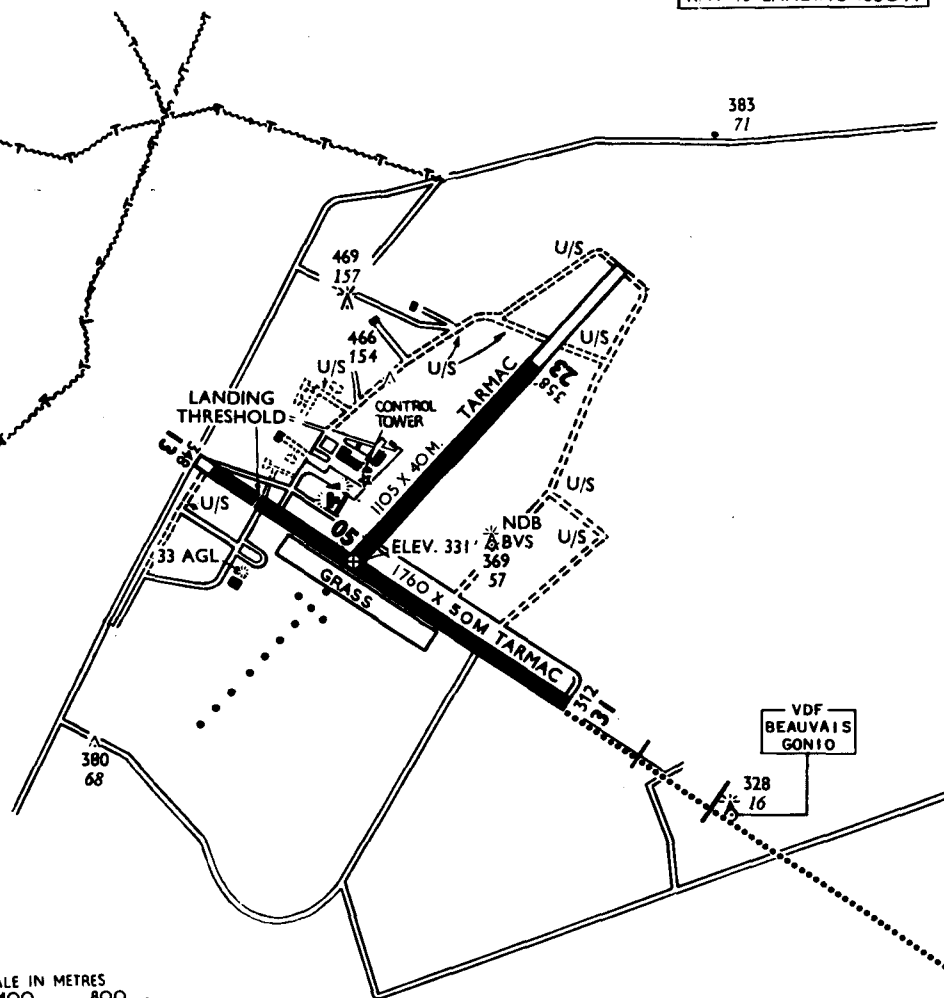
SERIAL No. 40043

DATE: 25.8.64

BARRA

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R/W RESTRICTIONS
R/W 13 LANDING 1530 M



BEAUVAIS AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
31	ALTERNATE H.I. RED/SODIUM (2000M.) SODIUM BAR AT 375M BAR 750M FROM THRESHOLD.	
13	NIL	
05	H.I. WHITE CENTRE LINE & BAR	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
31	L.I. GREEN	H.I. SODIUM & L.I. WHITE ON FIRST 1300M LAST 230M L.I. BLUE
13	L.I. GREEN 230 M ALONG R/W	FIRST 230M L.I. BLUE THEN H.I. SODIUM & L.I. WHITE
05	RED BAR BEFORE THRESHOLD, GREEN WINGS.	

OBSTRUCTION, TAXIWAY, IDENT BEACON G(WHITE)
WIND AND LANDING INDICATORS

ELEVATIONS (FT) : —

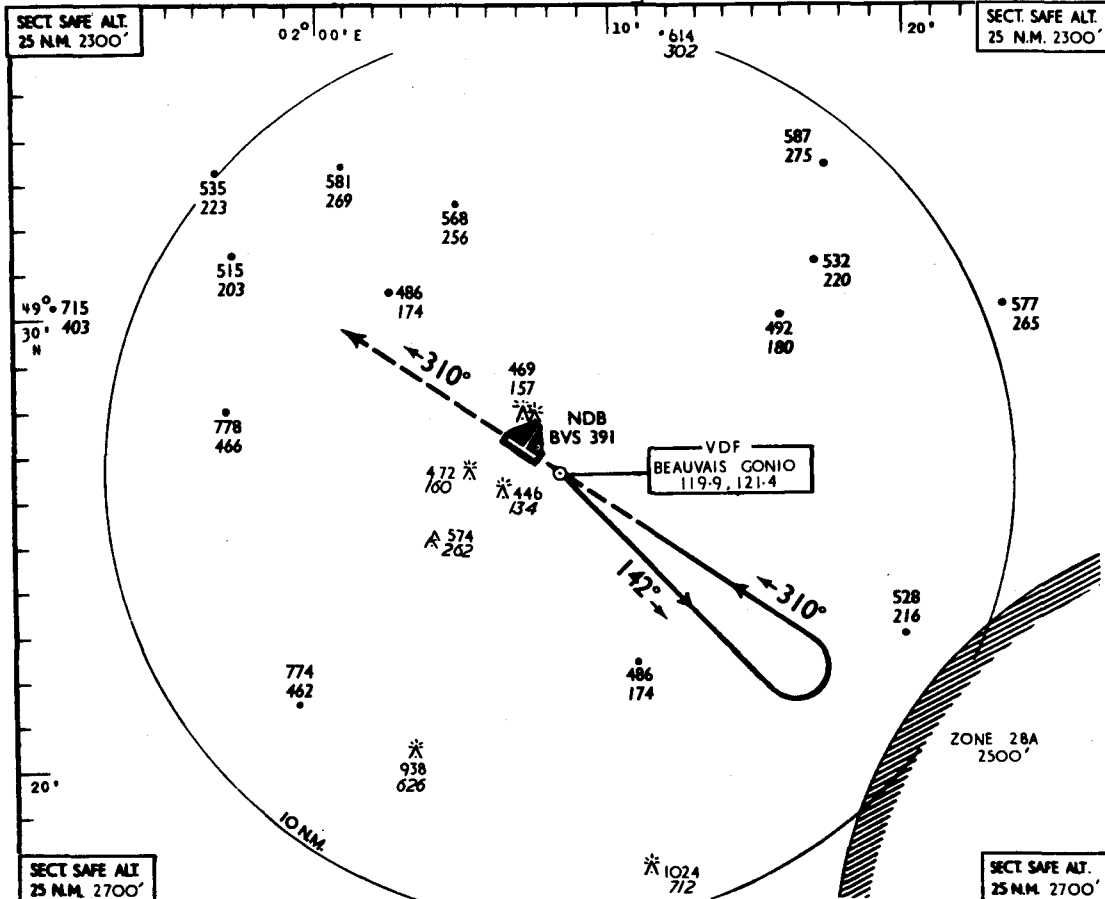
OFFICIAL AERODROME	358
LOWEST THRESHOLD (31)	312
LAT 49° 27' N, LONG 02° 07' E	
VARIATION 6° W	Bearings all magnetic
SERIAL No. 50019 /3	DATE: 19 . 1 . 65

Reason for Re-issue:

R/W 13/31 LENGTH.

AERODROME CHART BEAUVAIS

STOPWAY: R/W 05: - 500M.
R/W 23: - 60M.



BEAUVAIS VDF R/W 31

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH

TOWER

BEAUVAIS APT.

121.4

TRANS ALT

2950

FT.

G/S (KTS)

70

90

110

130

150

DESCENT ON G/P (FT/MIN)

G/S

170

190

210

230

250

DESCENT ON G/P (FT/MIN)

CRITICAL HEIGHT (FT)

500

OVERSHOOT

CLIMB AHEAD ON 310° TO 1900 / 588
AND REQUEST INSTRUCTIONS.

ELEVATIONS (FT) :—

OFFICIAL AERODROME 358

LOWEST THRESHOLD 312

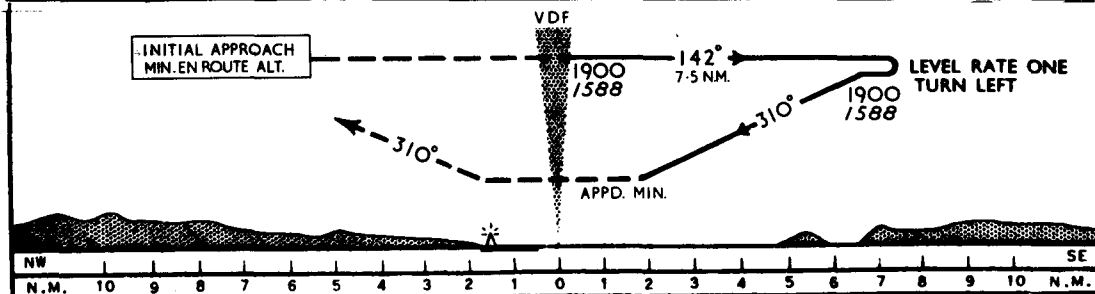
THRESHOLD R/W 31 312

VARIATION 6° W Bearings all magnetic

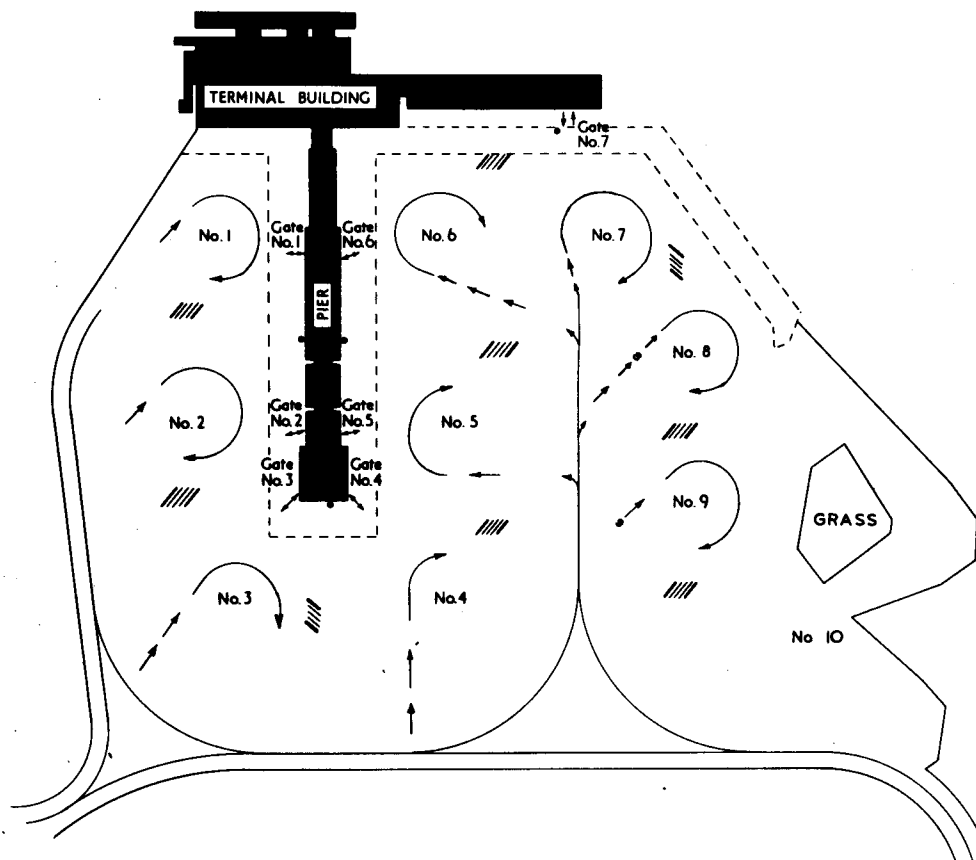
SERIAL No. 50018 / 2 DATE : 19 - 1 - 65

Reason for Re-Issue:
SECTOR SAFE ALT. AND
OBSTRUCTION REVISED

VDF R/W 31
BEAUVAIS



Key
• Floodlights



SCALE IN METRES
0 20 40 60 80 100 200

APRON AND PARKING POSITIONS BELFAST (ALDERGROVE)

BRITISH EUROPEAN AIRWAYS

BEA

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1) Marshalling is not provided at this Airport and, in order to ensure safe manoeuvring on the apron, pilots should follow closely the taxiway routings indicated by the centre-line markings and nose wheel guide lines.

2) The Stands are designed for the following types of aircraft, according to the radius of the nose wheel turning circles and the wing spans, but will accommodate other aircraft that can manoeuvre with the respective limits:-

Stands 1, 6, 8 and 9 Vanguard and Viscount
Stands 2, 5 and 7 Britannia, Vanguard and Viscount

Stands 3 and 4 Britannia, Vanguard, Viscount and medium jet aircraft

Stand 10 Types of aircraft smaller than Viscount

3) The procedure for manoeuvring on to a stand will be for the pilot to follow the nose wheel guide line until the fore and aft axis of the aircraft is parallel with the direction of the five parallel lines of the stop position indicator ahead of him, when he will roll forward for about 5 feet to straighten the nose wheel and stop. The nose wheel guide lines and the stop position indicator provide for the correct positioning of aircraft in relation to the passenger gates and ensure the necessary wing tip clearance from obstructions and other aircraft on the apron.

4) Gate positions for stands 1 to 7 are identified by illuminated signs attached to the building face. Illuminated signs are also provided for stands 8 and 9 at the eastern boundary of the apron.

SERIAL No. 50330/1 DATE: 31.5.65

Reason for Re-issue:

REVISED TEXT

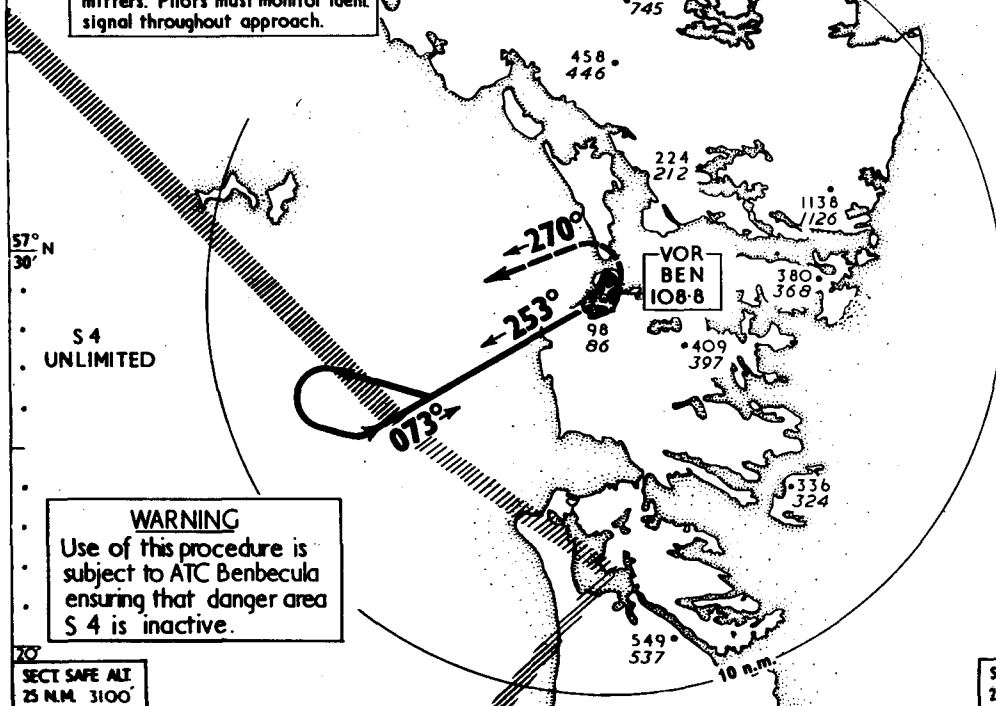
APRON AND PARKING POSITIONS
BELFAST (ALDERGROVE)

SECT SAFE ALT.
25 N.M. 3100'

40° 07° 30' W

SECT SAFE ALT.
25 N.M. 3000'

NOTE
Delay may occur in change-over from main to standby VOR transmitters. Pilots must monitor ident signal throughout approach.

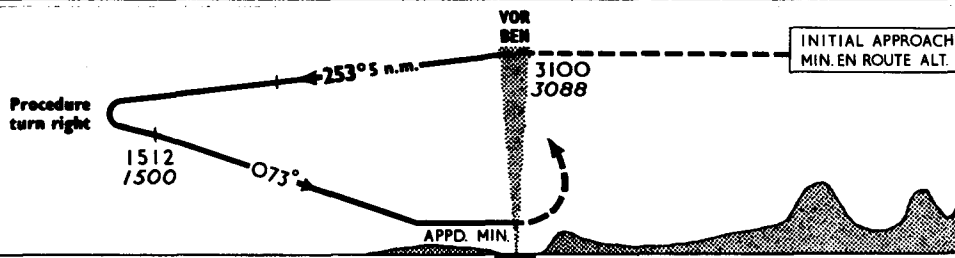


S4
UNLIMITED

WARNING
Use of this procedure is subject to ATC Benbecula ensuring that danger area S4 is inactive.

SECT SAFE ALT.
25 N.M. 3100'

SECT. SAFE ALT.
25 N.M. 3100'



SW NE
N.M. 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 N.M.

BENBECULA VOR R/W 07

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH

TOWER BENBECULA TWR

118.3

TRANS ALT 3000 FT.

G/S(KTS)	90	110	130	150
VOR TO TURN	3.20	2.44	2.19	2.0

G/S	170	190	210	230	250
VOR TO TURN	1.47	1.35	1.26	1.18	1.12

OVERSHOOT

IMMEDIATE TURN LEFT ONTO 270° & RETURN
TO VOR AT 3100 3088

ELEVATIONS (FT) :—

OFFICIAL AERODROME	19
LOWEST THRESHOLD	12
THRESHOLD R/W 07	13

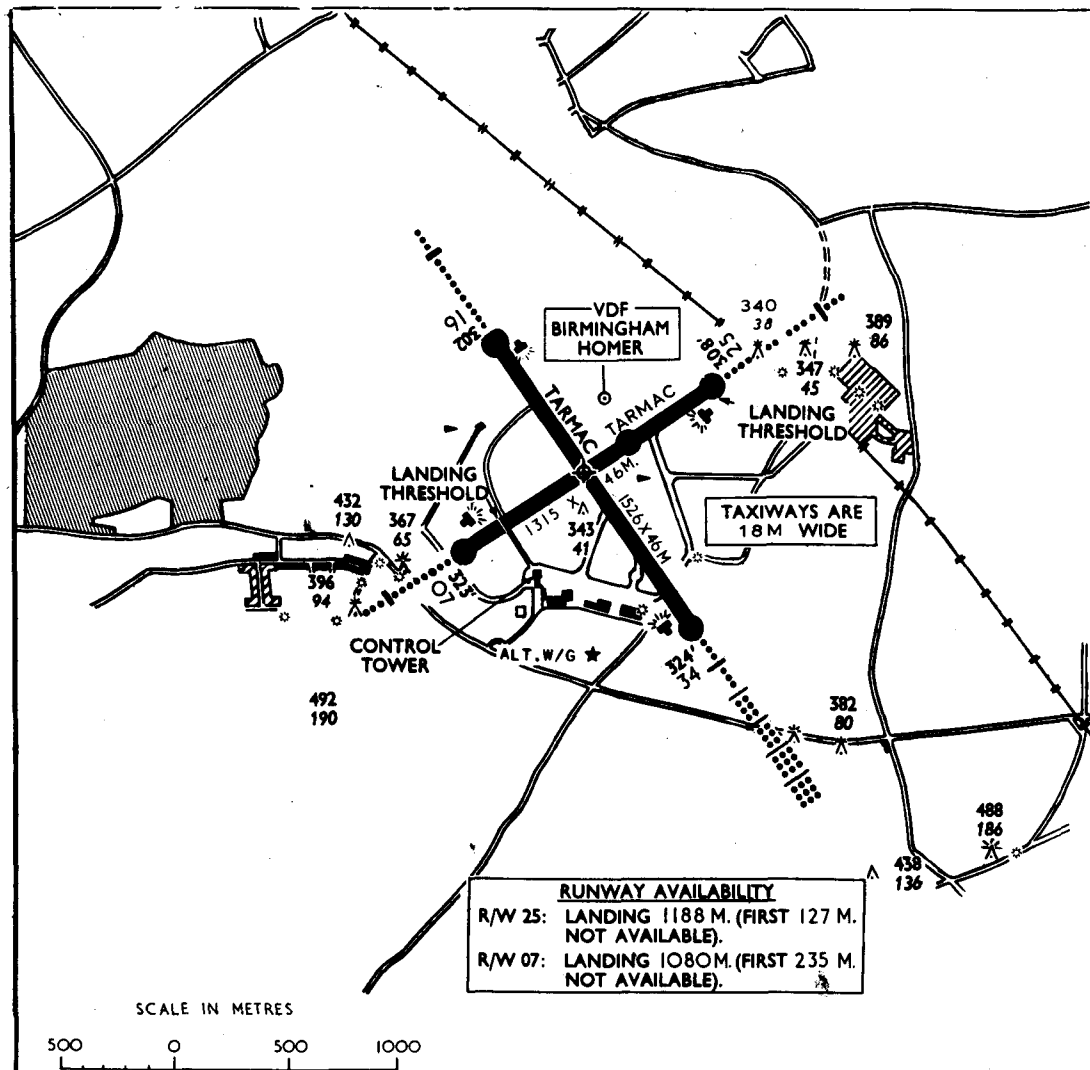
VARIATION 13° W | Bearings all magnetic

SERIAL No. 50358/2 DATE: 3.6.65

Reason for Re-issue:

WARNING NOTE ADDED

VOR R/W 07
BENBECULA



RUNWAY AVAILABILITY
 R/W 25: LANDING 1188 M. (FIRST 127 M. NOT AVAILABLE).
 R/W 07: LANDING 1080 M. (FIRST 235 M. NOT AVAILABLE).

BIRMINGHAM AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
07	H.I. SODIUM & L.I. RED CENTRE-LINE & BAR	
25	SODIUM & L.I. RED CENTRE-LINE & BAR	3°
16	SODIUM & L.I. RED CENTRE-LINE & BAR	3°
34	H.I. CALVERT & 5 BARS, L.I. RED & 2 BARS.	3.25°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
07	L.I. GREEN (WINGS) ELEVATED	L.I. WHITE OMNI-DIRECTIONAL (ELEVATED)
25	L.I. GREEN WINGS (ELEVATED)	L.I. WHITE OMNI-DIRECTIONAL (ELEVATED)
16	H.I. & L.I.	H.I. BI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL (ELEVATED)
34	GREEN	L.I. OMNI-DIRECTIONAL (ELEVATED)

OBSTRUCTION, TAXIWAY (07/25 TO NW END 16/34 GREEN C/L WITH RED STOP BARS), A'DME BEACON, STOPWAY LIGHTING. APRON FLOODLIGHTS.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	325
LOWEST THRESHOLD (16)	302
LAT 52° 27' N, LONG 01° 45'	
VARIATION 8° W Bearings all magnetic	
SERIAL No. 50056/2	DATE: 9.2.65

Reason for Re-issue:

R/W 25 VASI ADDED.

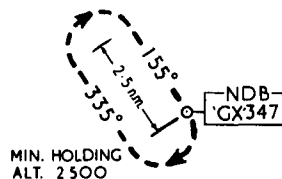
AERODROME CHART BIRMINGHAM

STOPWAYS: R/W 25 - 72M. R/W 16 - 91M. R/W 34 - 61M.

NOTE:
 VARIABLE CIRCUITS AT A.T.C.'S DISCRETION.

SECT. SAFE ALT
25 NM. 2500'

NO BACK BEAM



52°N
30'

ILS
BIR 110.1

MM

NDB
GM334

OM

YELLOW

BLUE

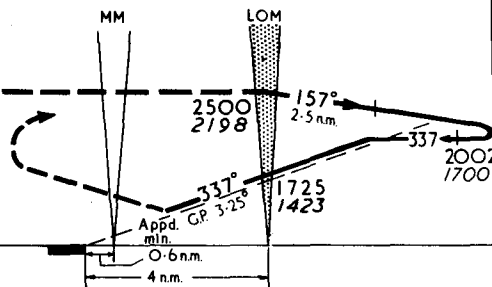
MIN. HOLDING
ALT. 2500

HONILEY
(ABANDONED)

SECT. SAFE ALT
25 NM. 2100'

INITIAL APPROACH
MIN. EN ROUTE ALT.

SHUTTLE ON
HOLDING PATTERN



A/C may descend in holding
pattern to 2002 /700 and
thence intercept G/P.

PROCEDURE
TURN RIGHT

Do not descend below
585 283 until past MM.

SECT. SAFE ALT
25 NM. 2500'

WARNINGS

1. Localiser narrow beam type & must not be used outside sector 10° either side of approach centre-line of runway.
2. ILS not to be used below 300' a.a.l.
3. Localiser offset—intersects extended R/W C/L (335°) 1420m. before T/H.
4. Slight bends & fluctuations exist on both localiser & glidepath.

BIRMINGHAM

ILS R/W 34

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH BIRMINGHAM APP

118.05

TOWER BIRMINGHAM TWR

118.3 121.5E

TRANS ALT 3000 FT.

G/S(KTS)	70	90	110	130	150
OM TO ADME	3.26	2.40	2.08	1.50	1.36
DESCENT ON G/P (FT/MIN)	405	515	630	745	860
G/S	170	190			
OM TO ADME	1.25	1.16			
DESCENT ON G/P (FT/MIN)	975	1090			

OVERSHOOT

CLIMB AHEAD TO 802 500 TURN LEFT
RETURN TO LOM AT 2002 /700

ELEVATIONS (FT) : —

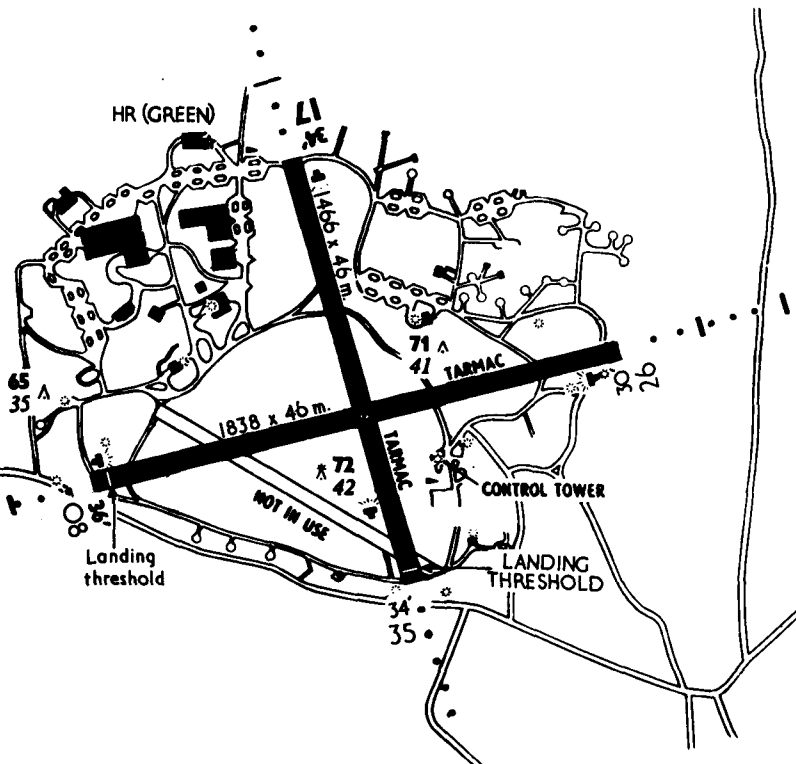
OFFICIAL AERODROME	325
LOWEST THRESHOLD	302
THRESHOLD R/W 34	324
VARIATION 8°W	Bearings all magnetic
SERIAL No. 50321/2	DATE: 14.5.65
Reason for Re-Issue:	REVISED TURN HTS
EFFECTIVE 3.6.65	

ILS R/W 34

BIRMINGHAM

R/W Availability	
R/W 08	Landing 1783 m. first 55 m. sterile.
R/W 26	T/O & Landing 1783 m, last 55 m. sterile.
R/W 35	Landing 1422 m., first 44 m. sterile.

HR (GREEN)



SCALE IN METRES

0 200 400 600 800 1000

WARNING R/W 08/26 very slippery when wet

BOURNEMOUTH (HURN) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
08	SODIUM CENTRE - LINE & CROSSBARS	3°
26	HIGH INT WITH RED LOW INT. CENTRE - LINE & CROSSBARS	3°
17	SODIUM CENTRE - LINE & 1 CROSS BAR	
35	SODIUM CENTRE LINE	

N.B. NO ENTRY UNDER VASI. ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08	H.I. GREEN ELEVATED	H.I. BI-DIRECTIONAL
26	WITH GREEN WING BARS.	L.I. OMNI-DIRECTIONAL
17		BOTH ELEVATED
35	GREEN WING BARS	

OBSTRUCTION, TAXIWAY, IDENT BEN 'HR' GREEN
LANDING INDICATOR.

ANGLE OF APPROACH INDICATORS R/W 17/35.
(2AAI'S EACH END OF R/W. PORT AAI 2.5°
STBD AAI 3.5°)

ELEVATIONS (FT) : —

OFFICIAL AERODROME	36
LOWEST THRESHOLD (26)	30
LAT 50° 47' N, LONG 01° 50' W.	
VARIATION 8° W	Bearings all magnetic
SERIAL No. 50324/3	DATE: 4.6.65

Reason for Re-Issue:

LIGHTING & WARNING NOTE

AERODROME CHART BOURNEMOUTH (HURN)

BOURNEMOUTH

NOISE ABATEMENT PROCEDURE

1 Take-off:

Runway 17: Continue climb maintaining runway heading until the coast is crossed before turning.

Other runways: Climb straight ahead to at least 1,000 feet AGL before turning.

2 Circuit Height:

When cloud base permits, maintain a circuit height of at least 1,000 feet AGL, and whenever landing or taking-off descend and climb as steeply as is compatible with safety.

BRITISH EUROPEAN AIRWAYS

BEA

Reason for Re-issue:

Text revision

SERIAL No. 40253

DATE : 23.10.64

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BOURNEMOUTH

Runway Availability

R/W 20 Landing 2600m. (Threshold displaced 220 m.)
 R/W 08R Landing 2167 m. (Threshold displaced 123 m.)
 R/W 08L Landing 3560m. (Threshold displaced 78 m.)
 R/W 26R Landing 3390 m. (Threshold displaced 250m.)
 R/W 30 Landing 2317 m. (Threshold displaced 75 m.)
 R/W 02 Landing 2564 m (last 256m not available.)

BRUSSELS NATIONAL AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
20	Sodium L.I. omni-directional C/L & bars	
02	White H.I. (var.) uni-directional C/L & bars	
08R	Sodium L.I. omni-directional C/L & bars	
26L	White H.I. (var.) uni-directional C/L & bars	2.5°
08L	Sodium L.I. omni-directional C/L & bars	
26R	Sodium L.I. omni-directional C/L & bars	
12	Sodium L.I. omni-directional C/L & bar	
30	Sodium L.I. omni-directional C/L & bars	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

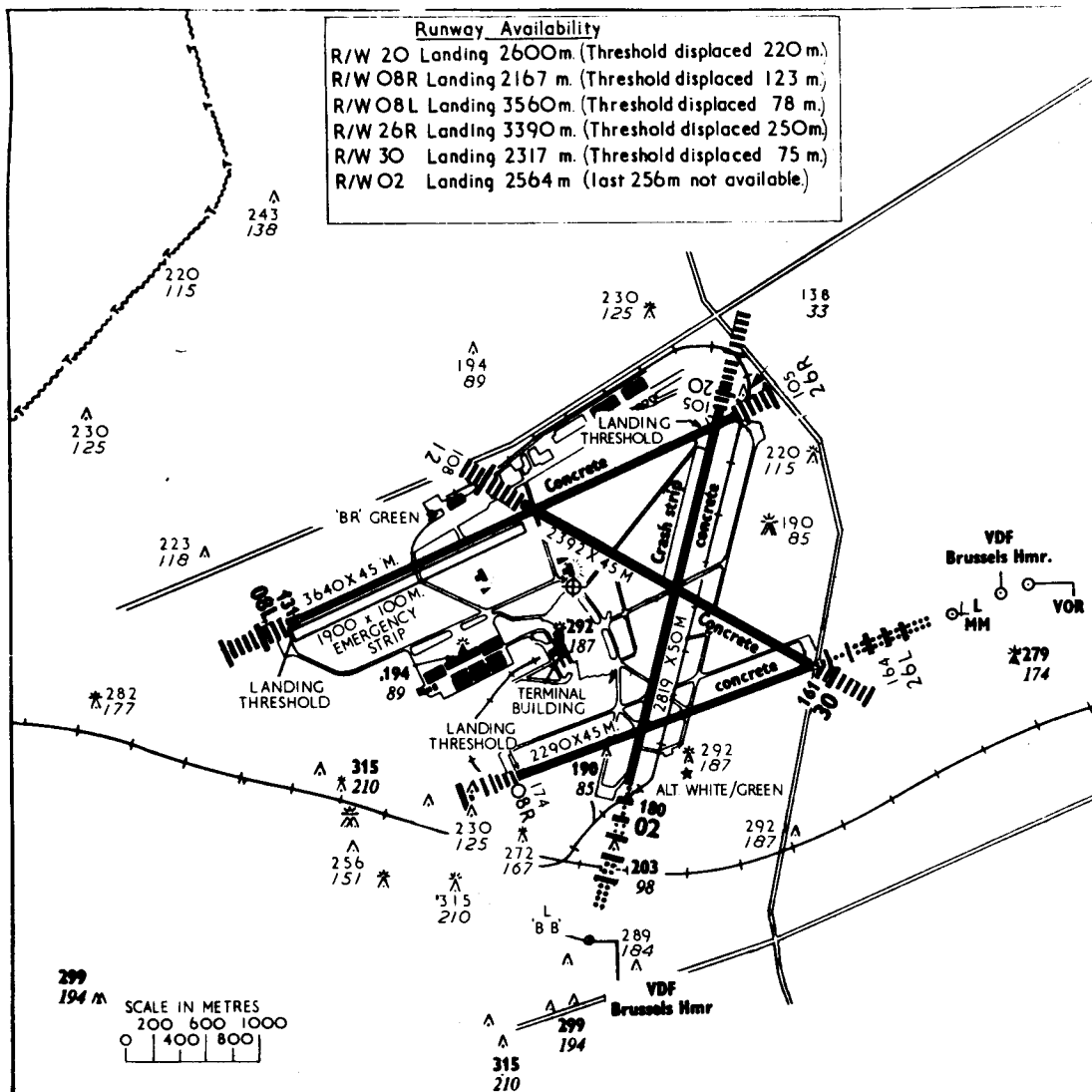
R/W	THRESHOLD	RUNWAY
20	H.I. green wingbars (220m up R/W)	White H.I. bi-directional
02	H.I. green	
08R	H.I. green wingbars (125m up R/W)	
26L	H.I. green	
08L	H.I. green (80m up R/W)	
26R	H.I. green wingbars (245m up R/W)	
12	H.I. green	
30	H.I. green wingbars (75m up R/W)	

OBSTRUCTION, TAXIWAY, ADME BEACON,
 IDENT BEACON 'BR' (GREEN) LANDING
 INDICATOR, APRON FLOODLIGHTS,
 TOUCHDOWN ZONE R/W 26 (WHITE).

ELEVATIONS (FT) :

OFFICIAL AERODROME	180
LOWEST THRESHOLD (20 & 26R)	105
LAT 50° 54' N, LONG 04° 29' E	
VARIATION 6° W	Bearings all magnetic
SERIAL No. 50440/3	DATE : 7.7.65
Reason for Re-Issue:	
R/W 02/20 REOPENED WEF 15.7.65	

AERODROME CHART BRUSSELS NATIONAL

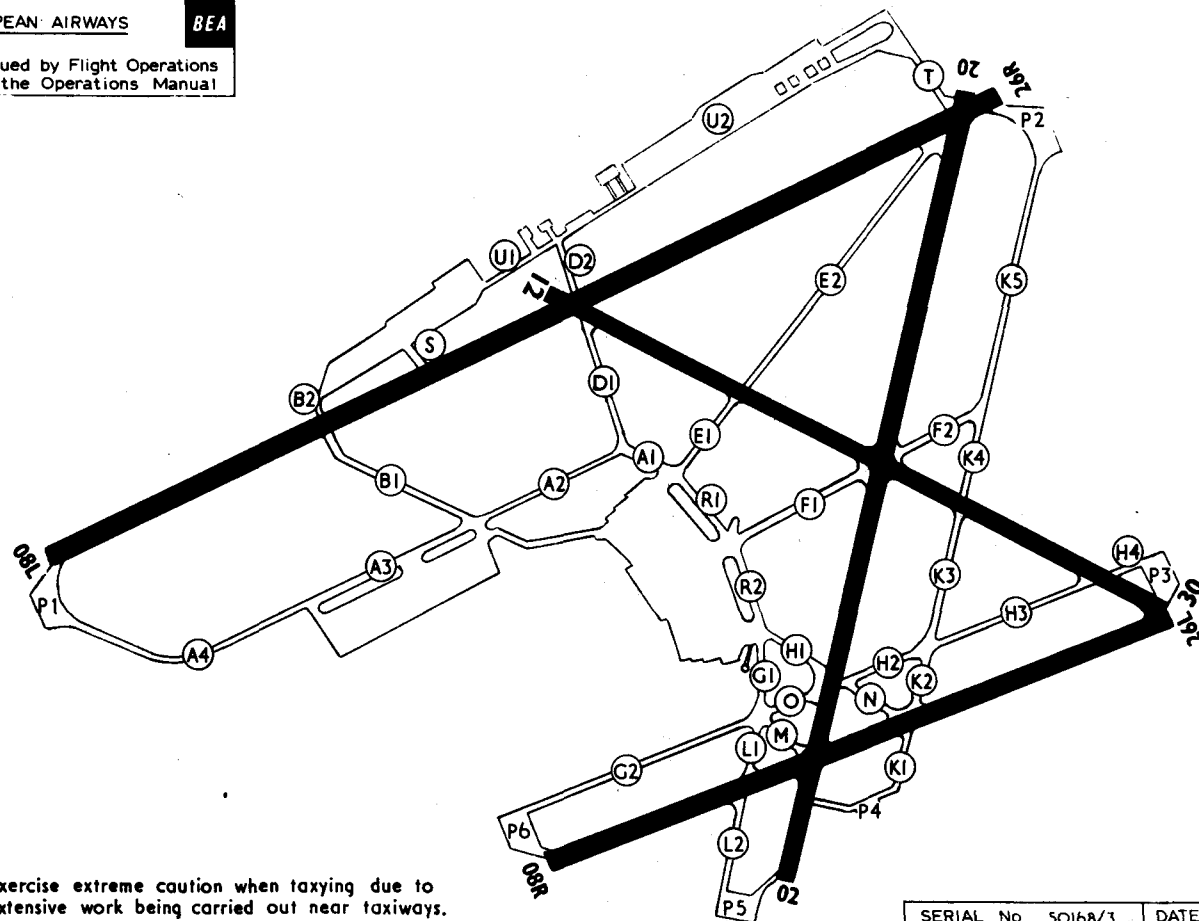


BRUSSELS (NATIONAL) TAXIWAYS

BRITISH EUROPEAN AIRWAYS

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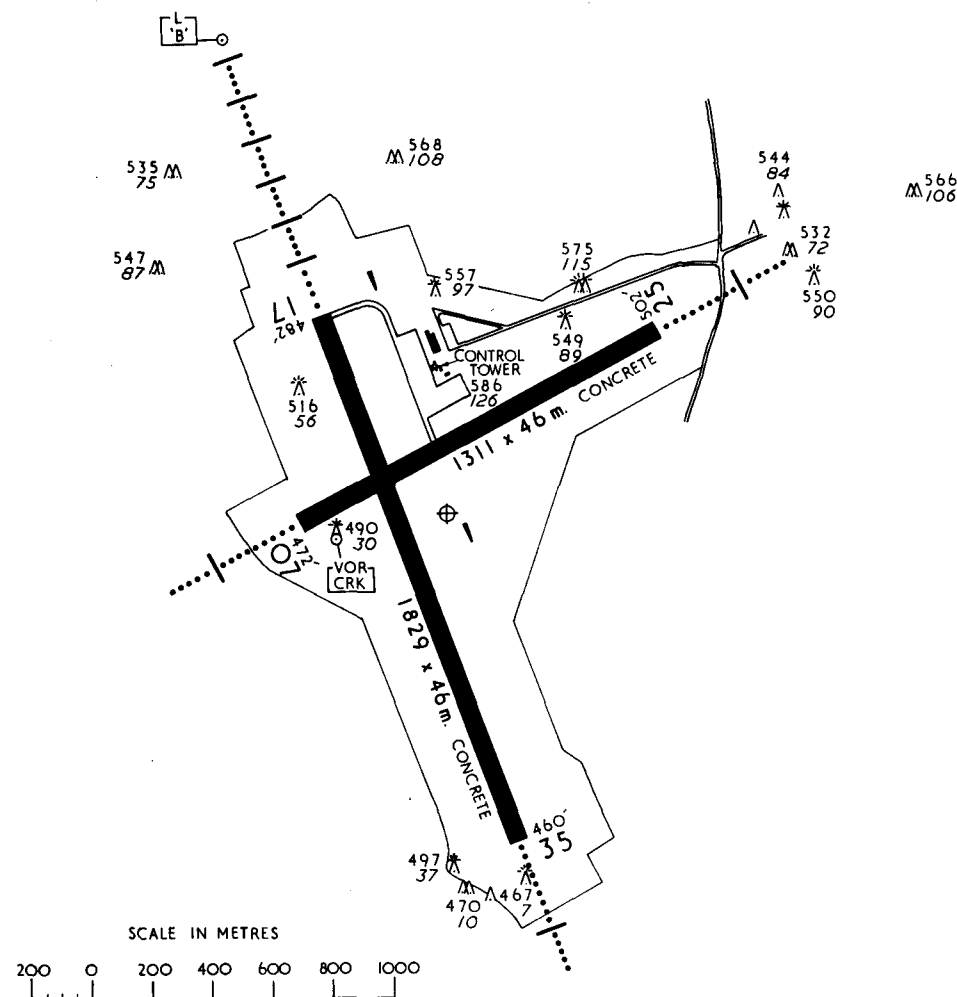
Warning: Exercise extreme caution when taxiing due to extensive work being carried out near taxiways.

SERIAL No 50168/3 DATE 7.7.65

Reason for Re-Issue:

TAXIWAY DESIGNATIONS

TAXIWAYS
BRUSSELS (NATIONAL)



CORK AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
17	WHITE H.I. (VAR) CODED UNI-DIR. C/L & 6 BARS. WHITE L.I. OMNI-DIR. C/L & BAR	
35 07 25	WHITE L.I. OMNI-DIR. C/L & BAR	3 1/2°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
17	GREEN H.I. (VAR) OMNI-DIR. ELEV. 15" WITH WING-BARS. RED UNI-DIR. CONTRAST BAR 60M. BEFORE T/H	WHITE H.I. (VAR) OMNI-DIR. ELEVATED 15". LAST 610 M. YELLOW. DISTANCE BAR 305 M. ALONG R/W. (EXCEPT 25).
35 07 25	GREEN H.I. (VAR) OMNI-DIR. ELEVATED 15"	

OBSTRUCTION, TAXIWAY, ADME BEACON, APRON.

ELEVATIONS (FT) :-

OFFICIAL AERODROME	502
LOWEST THRESHOLD (35)	460
LAT 51° 50' N, LONG 08° 29' W	
VARIATION 12° W	Bearings all magnetic
SERIAL No. 50297/1	DATE : 7 - 5 - 65

Reason for Re-Issue:

VORIDENT / NOTE

AERODROME CHART CORK

SECT. SAFE ALT.
25 NM. 4000'

08° 40' W

30° 08° 20' W

SECT. SAFE ALT.
25 NM. 4000'

CORK

ILS R/W 17

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH CORK APP

119.3 121.5E

TOWER CORK TWR.

118.6 121.8

TRANS ALT 3500 FT.

G/S (KTS)	70	90	110	130	150
LOM TO T/H	3.10	2.28	2.01	1.43	1.29

DESCENT ON G/P (FT/MIN)	330	430	520	620	720
-------------------------	-----	-----	-----	-----	-----

G/S	170	190			
LOM TO T/H	1.18	1.10			

DESCENT ON G/P (FT/MIN)	810	910			
-------------------------	-----	-----	--	--	--

MISSED APPROACH

Climb on 175° to 2500 2040
or as instructed.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	502
LOWEST THRESHOLD	460
THRESHOLD R/W 17	482
VARIATION 12° W	Bearings all magnetic
SERIAL No. 50307/1	DATE : 7.5.65

Reason for Re-issue:

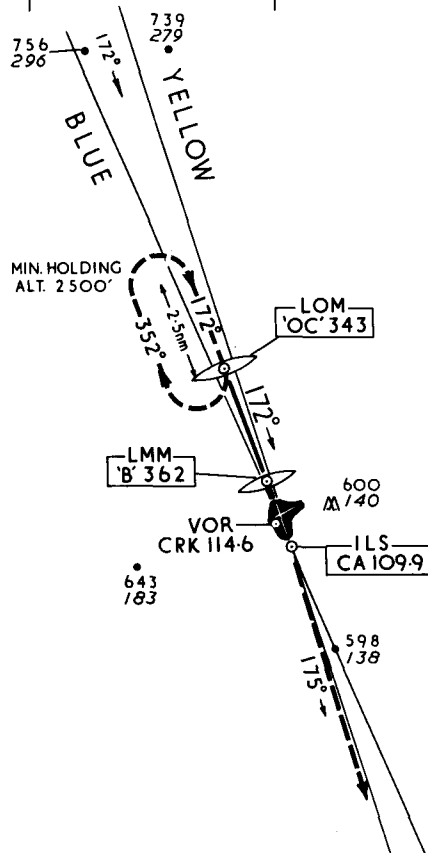
VOR IDENT

ILS R/W 17

CORK

SECT. SAFE ALT.
25 NM. 3000'

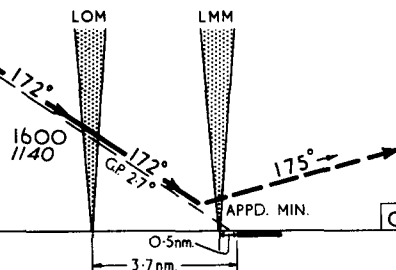
SECT. SAFE ALT.
25 NM. 2000'



INITIAL APPROACH
MIN. EN ROUTE ALT.

SHUTTLE ON
HOLDING PATTERN

2500
2040



G.P. HGT AT LMM 685 225

NW

SE

COVENTRY AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06	LI CENTRE-LINE & BAR	
24	HI. CENTRE-LINE & BAR	3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06	GREEN	ELEVATED H.I. BI-DIRECTIONAL
24	GREEN	(AND LI OMNI-DIRECTIONAL)

OBSTRUCTION, TAXIWAY, IDENT BEACON 'BG'
(GREEN).

ELEVATIONS (FT) a.m.s.l. 320
above lowest T/H 56

OFFICIAL AERODROME 278

LOWEST THRESHOLD (06) 264

LAT 52° 22' N, LONG 01° 29' W

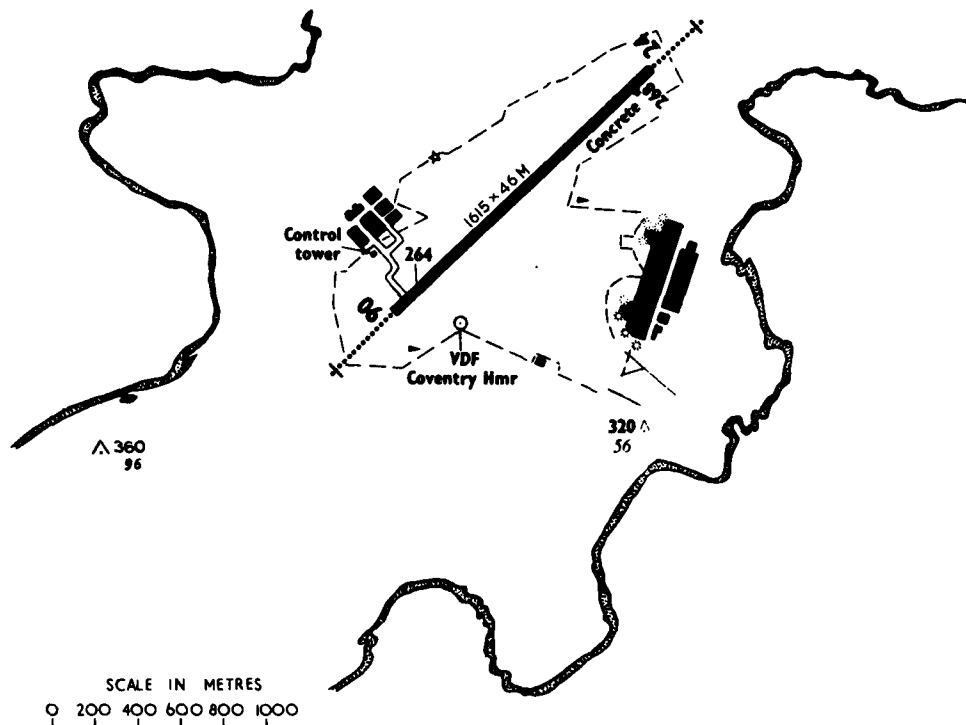
VARIATION 9° W | Bearings all magnetic

SERIAL No. 40073 | DATE: 30.9.64

Reason for Re-issue:

VASI R/W 24 ADDED.

AERODROME CHART COVENTRY



- NOTE: 1. VARIABLE CIRCUITS.
2. GLIDER LAUNCHING MAY TAKE PLACE.
3. JET A/C MAY NOT BE USING LANDING DIRECTION INDICATED

SECT. SAFE ALT
25 N.M. 2500'

SECT. SAFE ALT.
25 N.M. 2300'

COVENTRY

VDF R/W 24

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH COVENTRY APP

122.0

TOWER COVENTRY TOWER

122.7

TRANS ALT 3000 FT.

G/S (KTS)	70	90	110	130	150
VDF TO TURN	6m26s	5m0s	4m06s	3m28s	3m0s
	m s	m s	m s	m s	m s

G/S	170	190	210	230	250
VDF TO TURN	2m39s	2m22s	m s	m s	m s
	m s	m s	m s	m s	m s

DESCENT ON G/P (FT/MIN)					
-------------------------	--	--	--	--	--

CRITICAL HEIGHTS (FT)

TRIDENT AUTO MANUAL

OTHER TYPES WITH G/P NO G/P 500

OVERSHOOT

CLIMB AHEAD TO 764 500
THEN TURN LEFT ONTO 180°
TO 2100 1836

ELEVATIONS (FT) a.m.s.l. 2500
above lowest T/H 2236

OFFICIAL AERODROME 278

LOWEST THRESHOLD 264

THRESHOLD R/W 24 268

VARIATION 9° W Bearings all magnetic

SERIAL No. 40171 DATE 30.9.64

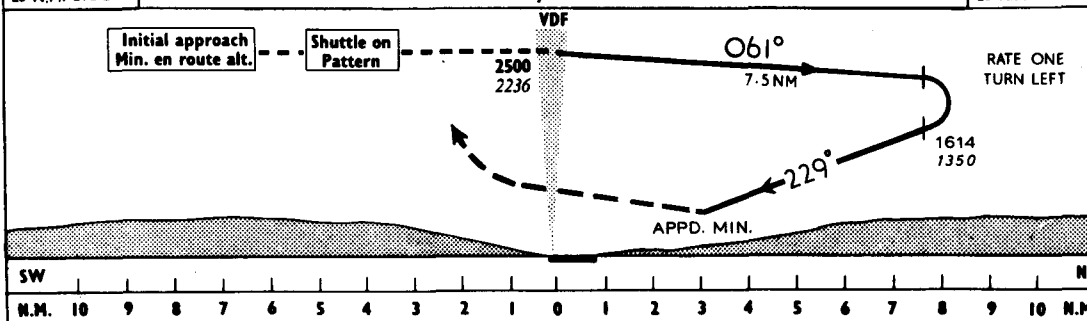
Reason for Re-issue:

APP. 122.7 WITHDRAWN

VDF R/W 24
COVENTRY

SECT. SAFE ALT
25 N.M. 2100'

SECT. SAFE ALT.
25 N.M. 2200'



RUNWAY AVAILABILITY

R/W 30 LANDING 1400M.

VDF
DINARD HMR

220
30

1500 x 80m

NOT IN USE

223
33

VOR
DIN

190
36

2000 x 45m

NOT IN USE

236
46

Displaced
threshold

262
72

Control Tower

299
109

322
132

33 CONCRETE

NOT IN USE

NOT IN USE

NOT IN USE

NOT IN USE

NOT IN USE

SCALE IN METRES

0 200 400 600 800 1000

DINARD

AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
18/36	NIL	
12/30	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
18/36	L.I. GREEN	L.I. WHITE. WHITE WING BARS 300M. UP R/W.
12	L.I. GREEN	L.I. WHITE
30	L.I. GREEN. WING BARS 100M. UP R/W.	L.I. WHITE

NOTE: LIGHTING MUST BE REQUESTED 2 HRS.
BEFORE CLOSING TIME OF AERODROME.

A'DME BEACON 'D'(WHITE) LANDING INDICATOR

ELEVATIONS (FT) a.m.s.l. 299
above lowest T/H 109

OFFICIAL AERODROME 213

LOWEST THRESHOLD (36) 190

LAT 48° 35' N, LONG 02° 05' W

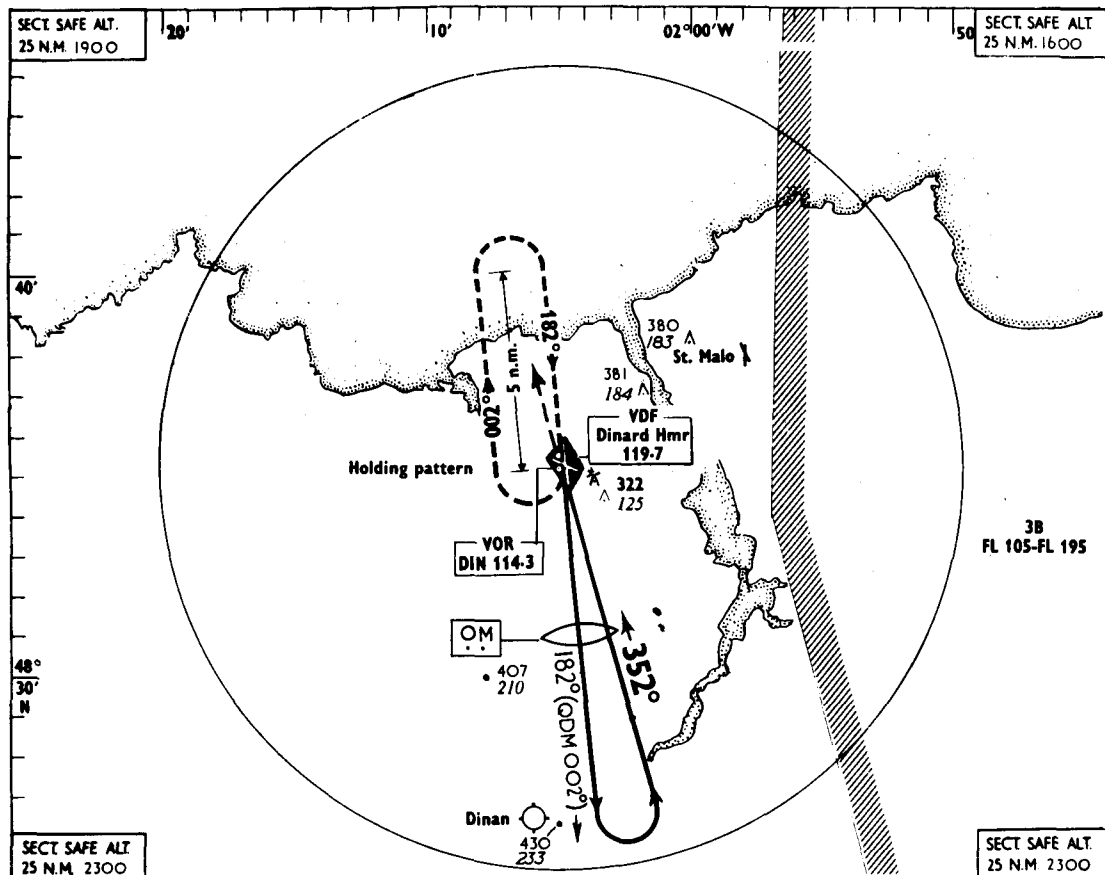
VARIATION 8° W Bearings all magnetic

SERIAL No. 50009/1 DATE 12.4.64

Reason for Re-issue:

NEW TAXIWAY ADDED

AERODROME CHART DINARD



DINARD

VOR OR VDF R/W 36

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH

TOWER

DINARD APT.

118.3 119.7

TRANS ALT

1700

FT.

G/S (KTS)	70	90	110	130	150
FACILITY TO TURN	7 · 30	5 · 50	4 · 46	4 · 02	3 · 30

DESCENT ON G/P (FT/MIN)					
-------------------------	--	--	--	--	--

G/S	170	190	210	230	250
FACILITY TO TURN	3 · 05	2 · 46	2 · 30	2 · 17	2 · 06

DESCENT ON G/P (FT/MIN)					
-------------------------	--	--	--	--	--

OVERSHOOT

CLIMB AHEAD FOLLOW CONTROL INSTRUCTIONS.

ELEVATIONS (FT)	a.m.s.l.	1700
	above lowest T/H	1503

OFFICIAL AERODROME	213
--------------------	-----

LOWEST THRESHOLD	197
------------------	-----

THRESHOLD R/W 36	197
------------------	-----

VARIATION 8°W	Bearings all magnetic
---------------	-----------------------

SERIAL No. 50128/1	DATE 12.4.65
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Reason for Re-issue:

OM ADDED

VOR OR VDF R/W 36
DINARD

Initial Approach
25 n.m 2300 2103

Shuttle on
holding pattern

VOR

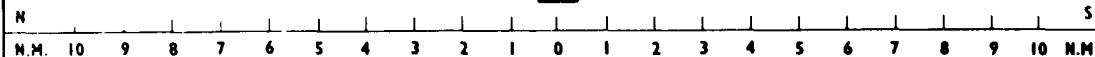
1700
1503

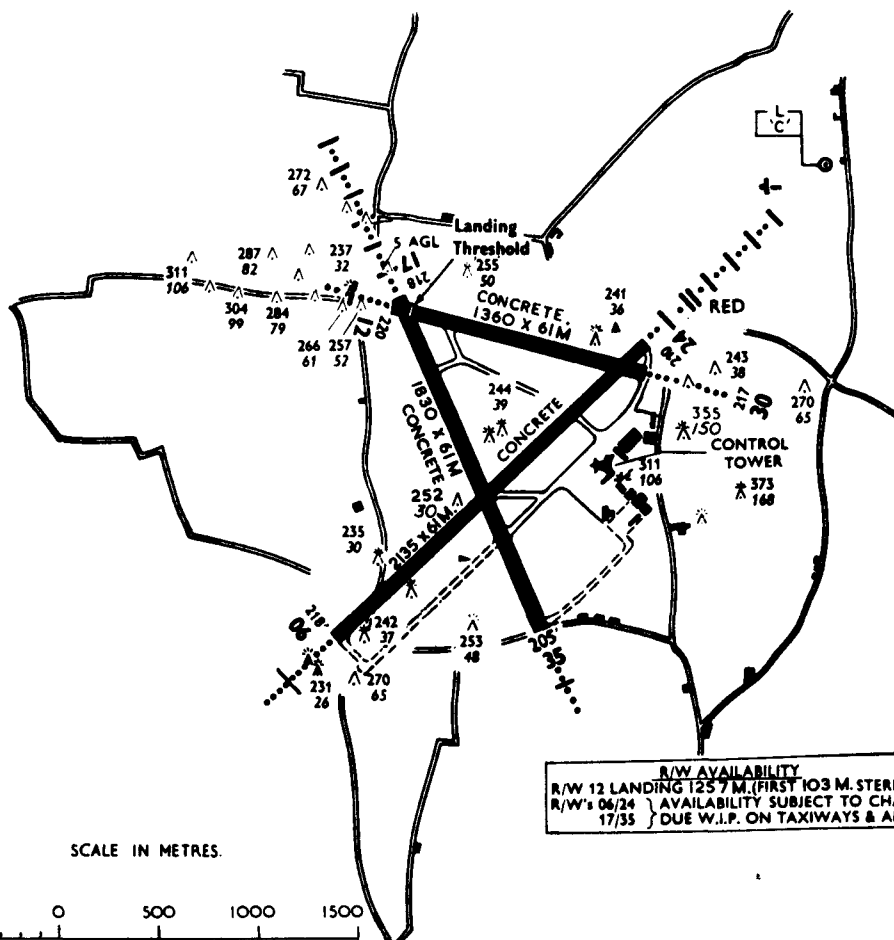
182°
8.75 n.m.
OM

Rate one
turn left

1700
1503

NOTE
Final approach offset
6° from R/W QFU.





R/W AVAILABILITY
 R/W 12 LANDING 125.7 M. (FIRST 103 M. STERILE)
 R/W's 06/24 } AVAILABILITY SUBJECT TO CHANGE
 17/35 } DUE W.I.P. ON TAXIWAYS & APRON

DUBLIN AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
24	WHITE H.I. (VAR.) CENTRE-LINE & BARS WHITE L.I. CENTRE-LINE & BAR	3°
17	WHITE H.I. (VAR.) CENTRE-LINE & BARS WHITE L.I. CENTRE-LINE & BAR	3°
30	WHITE L.I. CENTRE-LINE & BAR	
06	WHITE L.I. CENTRE-LINE & BAR	3°
12	WHITE L.I. CENTRE-LINE & BAR	
35	WHITE L.I. CENTRE-LINE & BAR	3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
24	Green omni-directional var. int. elevated	White omni-directional var. int. third last yellow (elevated)
17	Green omni-directional var. int. elevated with wingbars & red contrast bar 60m. before threshold	White omni-directional var. int. last third yellow
30	Green uni-directional var. intensity elevated 16" AGL.	H.I. white omni-directional last third yellow Elev. 16" wing bars 300m. along R/W
06	Green omni-directional var. int. (elevated)	White omni-directional var. int. last third yellow (elevated)
12	Green uni-directional 105m along R/W elevated	H.I. white omni-directional last third yellow Elev. 16" wing bars 450m. along R/W
35	Green omni-directional elevated	White omni-directional last third yellow
	Obstruction, taxiway, aerodrome beacon, apron.	

ELEVATIONS (FT) :—

OFFICIAL AERODROME	222
LOWEST THRESHOLD (35)	205
LAT 53° 26' N, LONG 06° 15' W	
VARIATION 11° W	Bearings all magnetic
SERIAL No. 40366/2	DATE: 9.2.65

Reason for Re-issue:

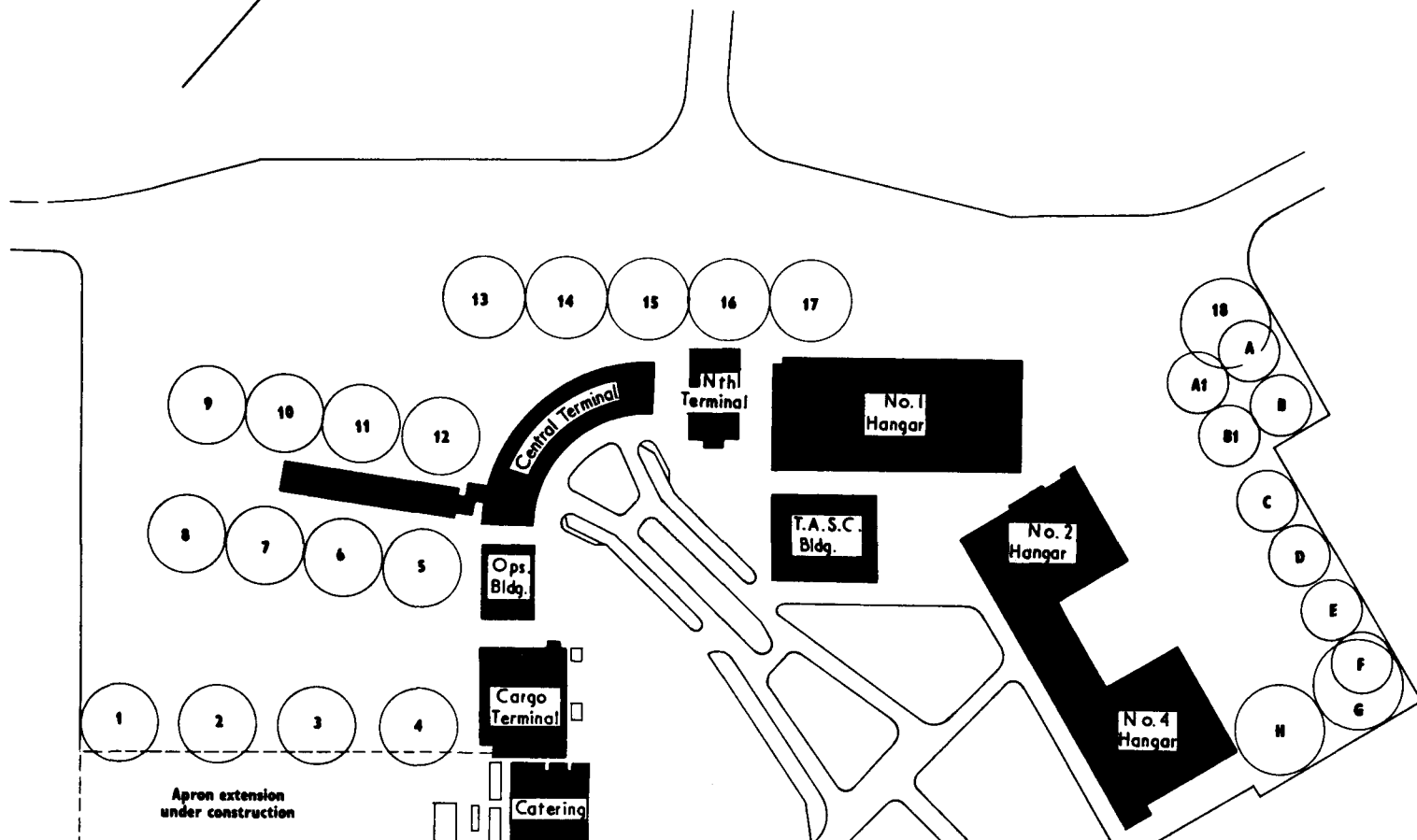
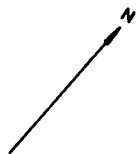
OBSTRUCTION 355' ADDED

AERODROME CHART DUBLIN

BRITISH EUROPEAN AIRWAYS

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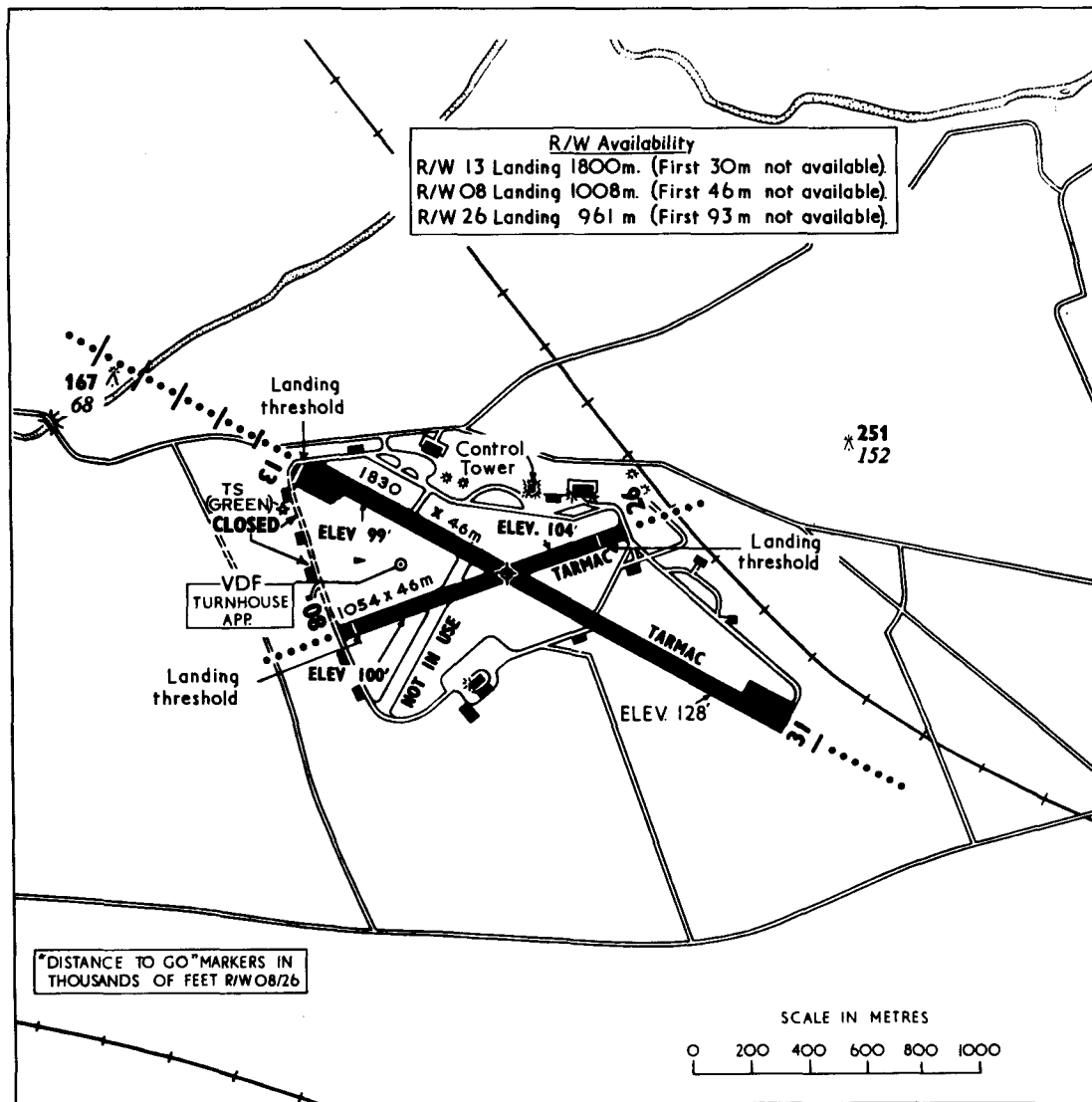


SERIAL No. 46168

DATE: 5.8.64

Reason for Re-issue:

GROUND MOVEMENT
AND PARKING
DUBLIN



EDINBURGH(TURNHOUSE) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
08	SHORT SODIUM CENTRE-LINE	
26		
13	H.I. WHITE CENTRE-LINE & 5 BARS, L.I. RED TEE	3°
31	L.I. RED TEE	3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08	H.I. GREEN WING-BARS	L.I. OMNI-DIRECTIONAL
26		
13	H.I. GREEN	ELEVATED VARIABLE HI DI- DIRECTIONAL. L.I. OMNI-DIRECTIONAL
31		

OBSTRUCTION, TAXIWAY, IDENT. BEACON ('TS')
LANDING INDICATOR,

ELEVATIONS (FT) : —

OFFICIAL AERODROME 135

LOWEST THRESHOLD (13) 99

LAT 55° 57' N, LONG 03° 21' W

VARIATION 11° W Bearings all magnetic

SERIAL No. 50152/2 DATE: 22.3.65

Reason for Re-Issue:
LIGHTING

AERODROME CHART EDINBURGH (TURNHOUSE)

SECT. SAFE ALT.
2.5 NM. 3400'

30'

03° 20' W

10'

SECT. SAFE ALT.
2.5 NM. 2800'

YELLOW

DONIBRISTLE
(ABANDONED)



LOM

NDB
'TRH' 341

ILS
TRH 109.1

NDB
'OE' 331

MM

**HOLDING PATTERN 3000'
LET-DOWN & LANDING STACK 2000'**

Holding
pattern
min. alt.
3000'

KIRKNEWTON
(ABANDONED)

**NO BACK
BEAM**

BLUE

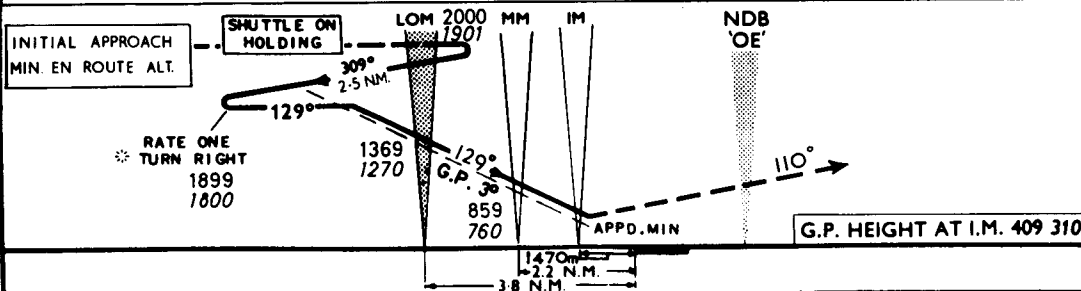
SECT. SAFE ALT.
2.5 NM. 3700'

SECT. SAFE ALT.
2.5 NM. 3700'

NOTE

**LOCALISER NARROW BEAM TYPE & MUST
NOT BE USED OUTSIDE SECTOR 10° EACH
SIDE OF APP. CENTRE LINE OF R/W 13**

**ALTERNATIVELY, FLY 2.5nm. BEYOND LOM.
PROCEDURE TURN RIGHT DESCENDING TO 1699
(1600)**



EDINBURGH

ILS R/W13

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH TURNHOUSE APP.

121.2

TOWER TURNHOUSE TWR.

118.7 121.5

TRANS ALT 3000 FT.

G/S(KTS)	70	90	110	130	150
LOM TO ADME	3.15	2.32	2.04	1.45	1.31

DESCENT ON G/P(FT/MIN)	371	477	583	689	796
------------------------	-----	-----	-----	-----	-----

G/S	170	190	210	230	250
LOM TO ADME	1.20	1.12	1.05	1.00	0.55

DESCENT ON G/P(FT/MIN)	902	1008	1114	1221	1321
------------------------	-----	------	------	------	------

OVERSHOOT

CLIMB AHEAD TO 599(500) TURN LEFT ONTO
110° CLIMBING TO 3000' 2801 AND RETURN
TO NDB 'OE'

ELEVATIONS (FT) :—

OFFICIAL AERODROME 135

LOWEST THRESHOLD 99

THRESHOLD R/W 13 99

VARIAION 11°W Bearings all magnetic

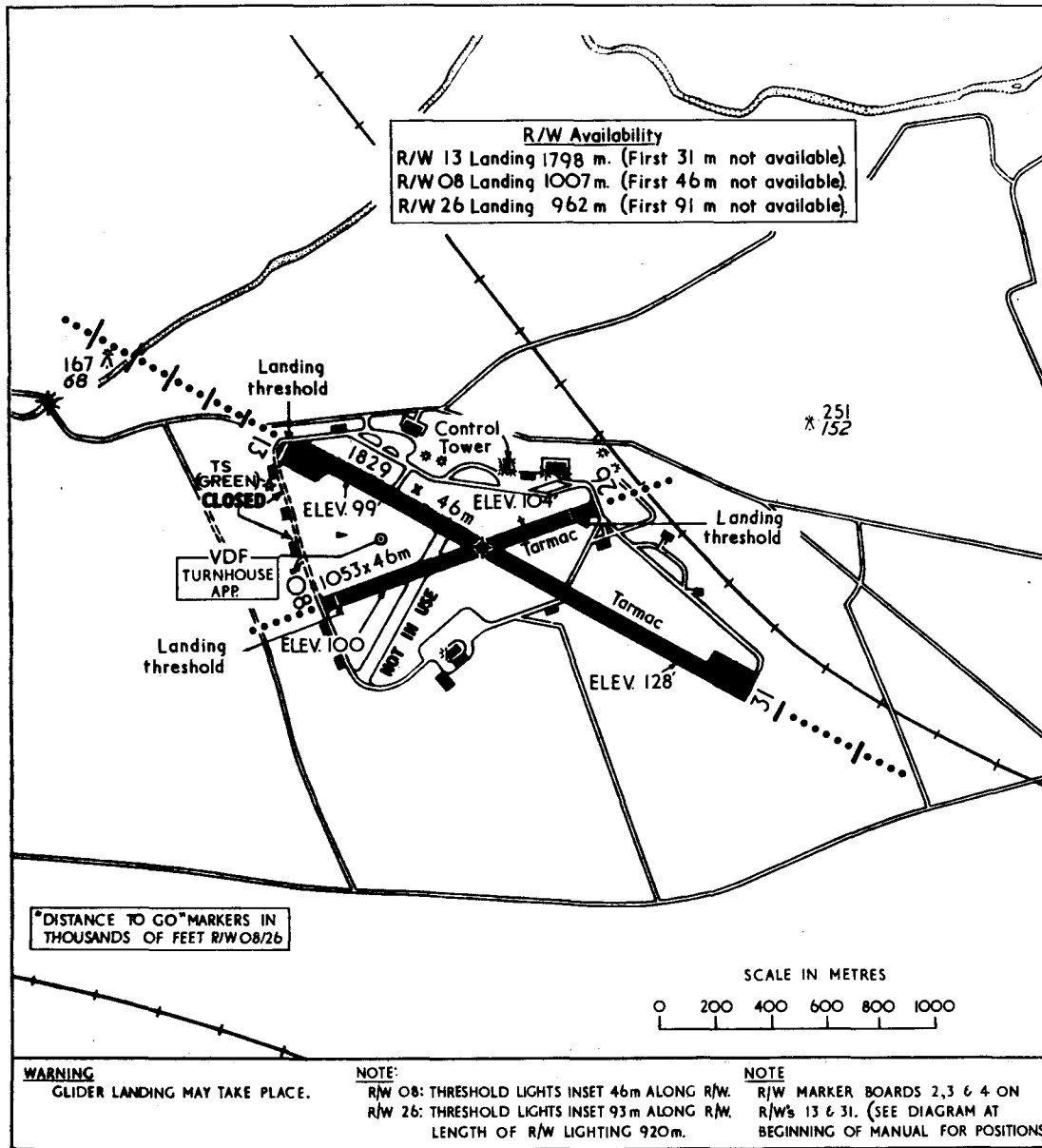
SERIAL No. 50128/3 DATE: 19.3.65

Reason for Re-Issue:

CRIT HEIGHT DELETED

ILS R/W 13

EDINBURGH



EDINBURGH(TURNHOUSE) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
08		
26	SHORT SODIUM CENTRE-LINE	
13	H.I. WHITE CENTRE-LINE & 5 BARS, L.I. RED TEE	3°
31	H.I. CENTRE-LINE (730M) WITH 2 BARS. L.I. RED TEE	3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08	H.I. GREEN WING-BARS	L.I. OMNI-DIRECTIONAL
26		
13 31	H.I. GREEN	ELEVATED VARIABLE HI BI- DIRECTIONAL. L.I. OMNI-DIRECTIONAL H.I. BI-DIRECTIONAL CENTRE-LINE

OBSTRUCTION, TAXIWAY, IDENT. BEACON (TS)
 LANDING INDICATOR.

ELEVATIONS (FT) : —

OFFICIAL AERODROME	135
LOWEST THRESHOLD (13)	99
LAT 55° 57' N. LONG 03° 21' W	
VARIATION 10° W	Bearings all magnetic
SERIAL No. 50627/4	DATE : 30.12.65
Reason for Re-Issue:	
LIGHTING	

AERODROME CHART EDINBURGH (TURNHOUSE)

SECT. SAFE ALT.
25 NM. 3400'

SECT. SAFE ALT.
25 NM. 2800'

Alternatively, fly 2.5 nm. beyond LOM
procedure turn left at 2099, 2000.

• 785
686

DONIBRISTLE
(ABANDONED)

YELLOW

NDB
TRH 341

535
436

LOM

535
436

ILS

TRH 109-1

NDB
'OE' 331

622
523

505
406

823
724

480
381

MM

IM

345
246

520
421

HOLDING PATTERN 4000
LET DOWN & LANDING STACK 3000' A

1017
918

BLUE

NO BACK
BEAM

Kirknewton
(Abandoned)

1898
1799

SECT. SAFE ALT.
25 NM. 3700'

NOTE

LOCALISER NARROW BEAM TYPE & MUST
NOT BE USED OUTSIDE SECTOR 10° EACH
SIDE OF APP. CENTRE LINE OF R/W 13

SECT. SAFE ALT.
25 NM. 3700'

EDINBURGH
ILS R/W 13

BRITISH EUROPEAN AIRWAYS

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APPROACH TURNHOUSE APP.

121.2

TOWER TURNHOUSE TWR.

118.7 121.5

TRANS ALT 4000 FT.

G/S (KTS)	70	90	110	130	150
LOM TO ADME	3.15	2.32	2.04	1.45	1.31
DESCENT ON G/P (FT/MIN)	371	477	583	689	796
G/S	170	190	210	230	250
LOM TO ADME	1.20	1.12	1.05	1.00	0.95
DESCENT ON G/P (FT/MIN)	902	1008	1114	1221	1321

MISSED APPROACH.

CLIMB AHEAD TO 599 500 TURN LEFT ONTO
100° CLIMBING TO 3000 2901 AND RETURN
TO NDB 'OE' AND HOLD

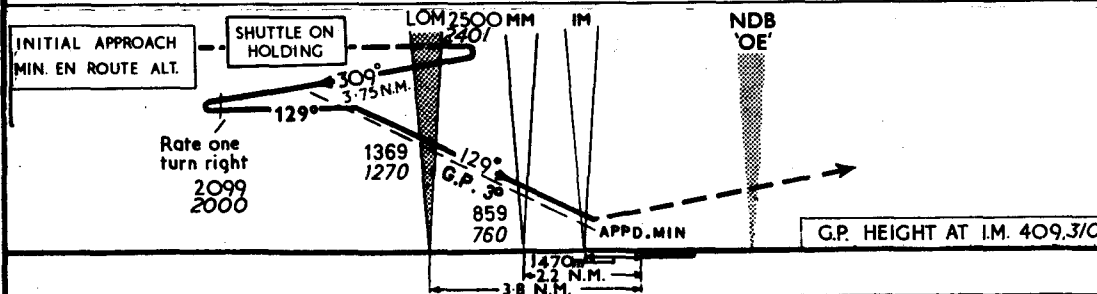
ELEVATIONS (FT) :-

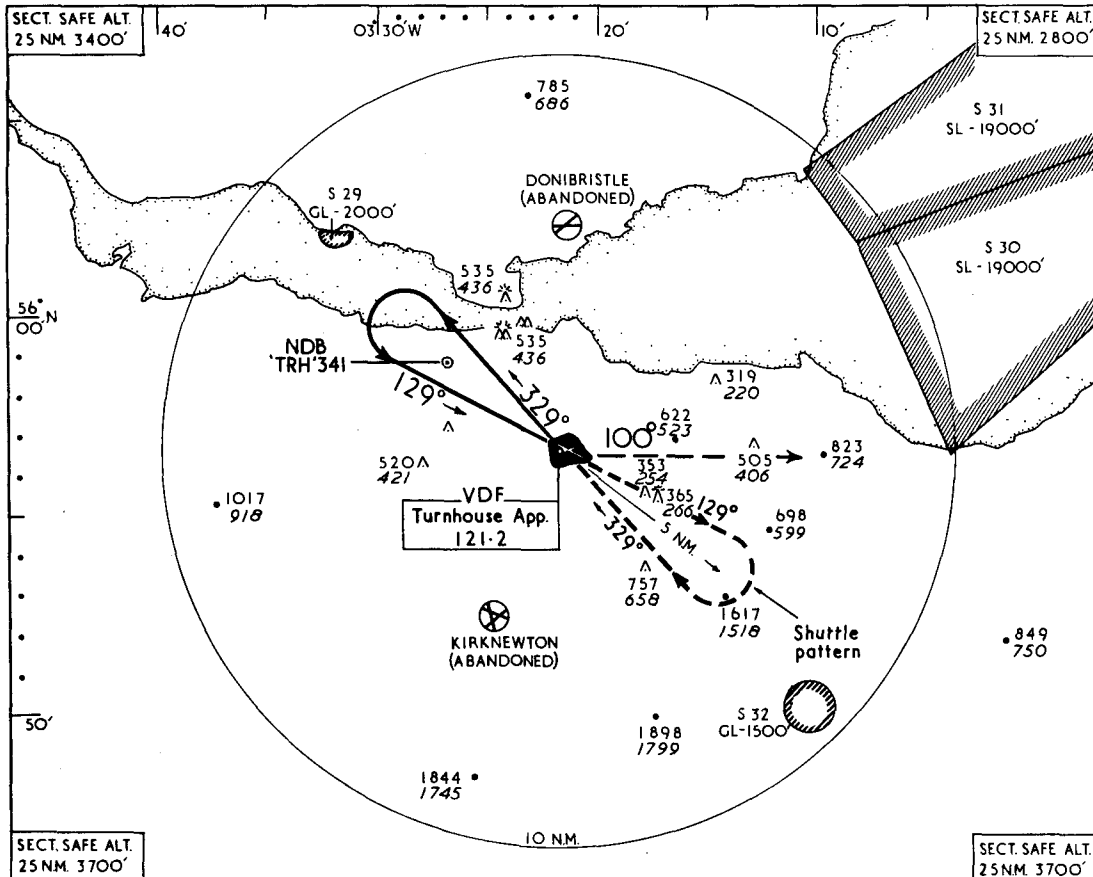
OFFICIAL AERODROME	135
LOWEST THRESHOLD	99
THRESHOLD R/W 13	99
VARIATION 10° W	Bearings all magnetic
SERIAL No. 6320/6	DATE: 21.4.66
Reason for Re-issue: TRANS ALT	

EFFECTIVE 2.5.66

ILS R/W 13

EDINBURGH





EDINBURGH VDF R/W 13

BRITISH EUROPEAN AIRWAYS

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APPROACH TURNHOUSE APP

121.2

TOWER TURNHOUSE TWR.

118.7 121.5E

TRANS ALT 4000 FT.

G/S (KTS)	90	110	130	150
VDF TO TURN	3.20	2.44	2.19	2.00

G/S	170	190	210	230	250
VDF TO TURN	1.46	1.35	1.26	1.18	1.12

MISSED APPROACH

TURN LEFT ONTO 100° & CLIMB TO 3700 3601

ELEVATIONS (FT) :—

OFFICIAL AERODROME 135

LOWEST THRESHOLD 99

THRESHOLD R/W 13 99

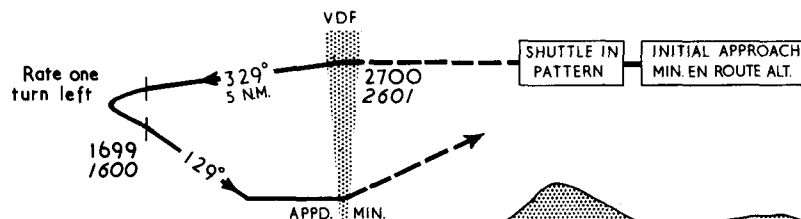
VARIATION 10°W Bearings all magnetic

SERIAL No. 6320/6 DATE: 21.4.66

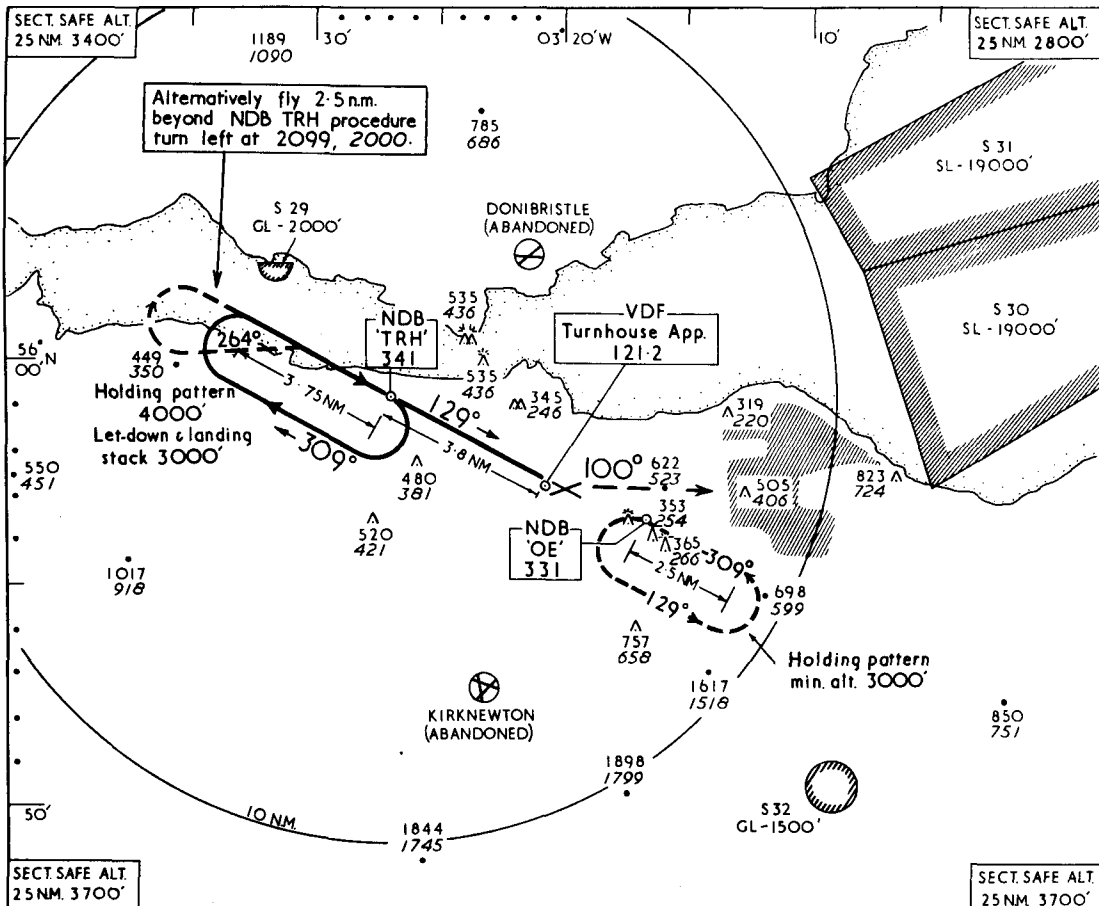
Reason for Re-issue: TRANS ALT.

EFFECTIVE 2.5.66

VDF R/W 13
EDINBURGH



NW 12 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 11 12 SE NM



EDINBURGH NDB/VDF R/W13

BRITISH EUROPEAN AIRWAYS

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APPROACH TURNHOUSE APP

121.2

TOWER TURNHOUSE TWR.

118.7 121.5E

TRANS ALT 4000 FT.

G/S (KTS)		90	110	130	150
ABM NDB TO TURN		2.30	2.03	1.45	1.30
NDB TO ADME		2.30	2.04	1.46	1.31
G/S	170	190	210	230	250
ABM NDB TO TURN	1.20	1.11	1.04	0.59	0.55
NDB TO ADME	1.21	1.12			

MISSED APPROACH.

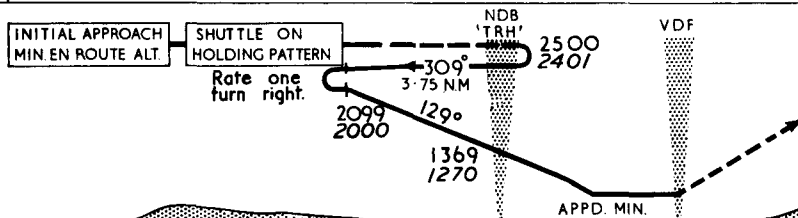
TURN LEFT ONTO
100° CLIMBING TO 3000 290° AND RETURN
TO 'OE' & HOLD

ELEVATIONS (FT) :—

OFFICIAL AERODROME	135
LOWEST THRESHOLD	99
THRESHOLD R/W 13	99
VARIATION 10°W	Bearings all magnetic
SERIAL No. 6320/6	DATE 21.4.66
Reason for Re-issue: TRANS ALT.	

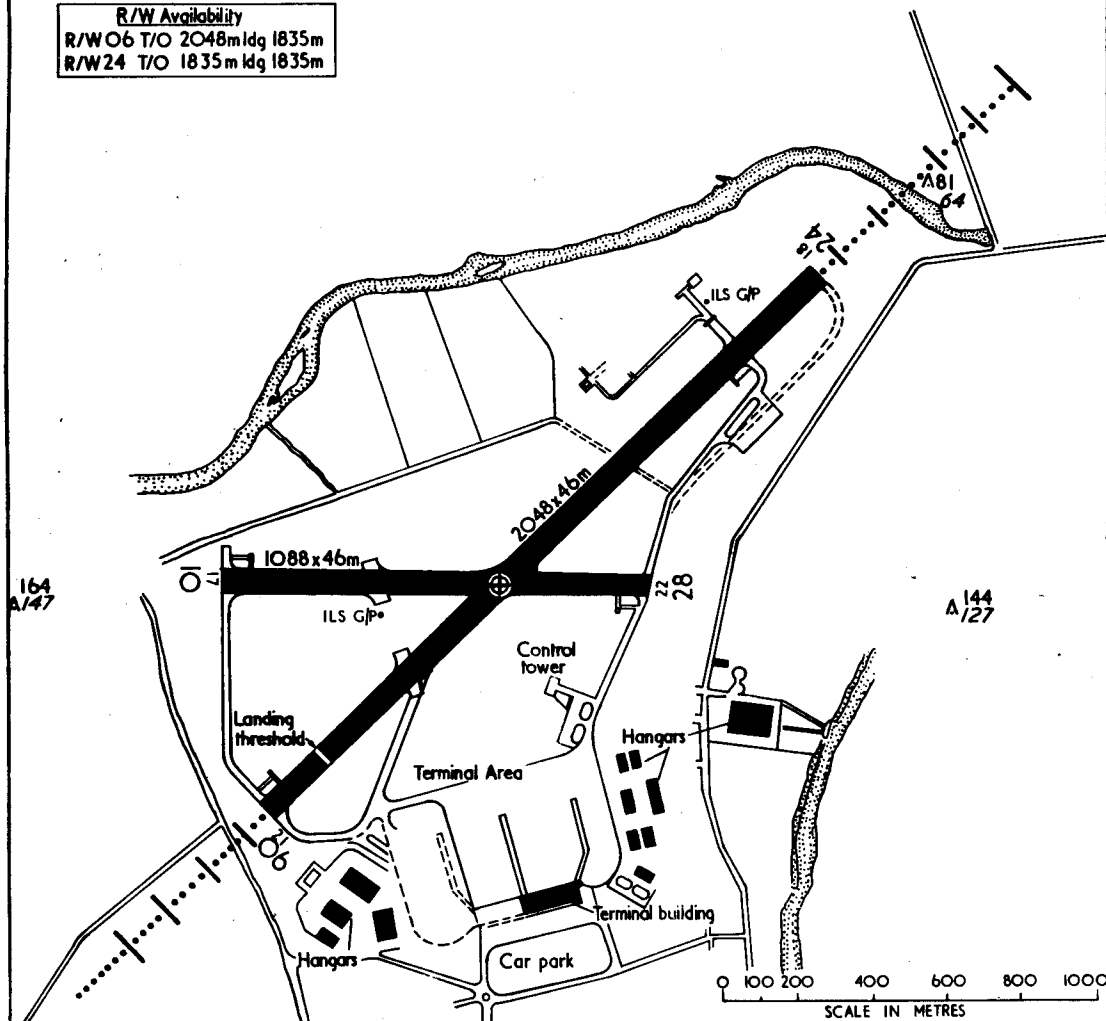
EFFECTIVE 2.5.66

NDB/VDF R/W13
EDINBURGH



WNW NM. 12 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 ESE NM.

R/W Availability
 R/W 06 T/O 2048m ldg 1835m
 R/W 24 T/O 1835m ldg 1835m



Note. Pecked lines indicate future development.

GLASGOW ABBOTSINCH AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06	H.I. CENTRE LINE	3°
24	WITH 5 BARS	3°
10	NIL	
28		

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06	H.I. GREEN WITH ELEVATED WING BARS	ELEV. H.I. BI-DIR EDGE WITH L.I. OMNI-DIR. FLUSH H.I. BI-DIR C/L H.I. UNI-DIR T/DOWN ZONE LIGHTS 870 M.
24		
10	NIL	NIL
28		

OBSTRUCTION, TAXIWAY, ADME BEACON
 WIND INDICATOR. APRON FLOODLIGHTS.

ELEVATIONS (FT.)

OFFICIAL AERODROME 23

LOWEST THRESHOLD (10) 17

LAT 55° 52' N, LONG 04° 26' W

VARIATION 10° W | Bearings all magnetic

SERIAL No. 6208 | DATE 31.3.66

Reason for Re-issue:

OPERATIONAL SERVICE COMMENCES

EFFECTIVE 2.5.66

AERODROME CHART GLASGOW ABBOTSINCH

Sect. safe alt.
25nm. 4400

0430° 20°

Sect. safe alt.
25nm. 4300

GLASGOW ABBOTSINCH

ILS R/W 06

BRITISH EUROPEAN AIRWAYS

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APPROACH ABBOTSINCH APP

119.1

TOWER ABBOTSINCH TWR

118.8

TRANS ALT 4000 FT.

G/S (KTS)	70	90	110	130	150
LOM TO TURN			2.11	1.50	1.36
LOM TO T/H	4.12	3.16	2.40	2.15	1.58
DESCENT ON G/P (FT/MIN)	370	480	580	690	800
G/S	170	190	210	230	250
LOM TO TURN	1.25	1.16	1.09	1.02	0.58
LOM TO T/H	1.43	1.32			
DESCENT ON G/P (FT/MIN)	900	1000			

MISSED APPROACH

Climb straight ahead to 1017 /000 climbing
turn right to return to 'AC' at 3500 3483

ELEVATIONS (FT)

OFFICIAL AERODROME 23

LOWEST THRESHOLD 17

THRESHOLD R/W 06 21

VARIATION 10°W Bearings all magnetic

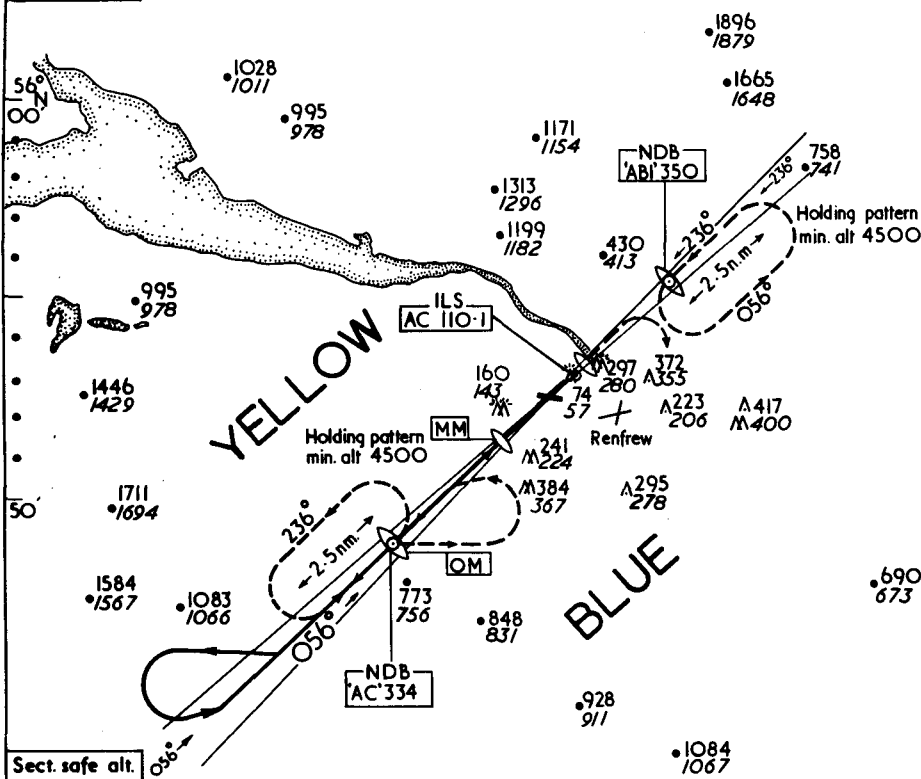
SERIAL No. 6208 DATE: 31.3.66

Reason for Re-Issue:

NEW ISSUE EFFECTIVE 2.5.66

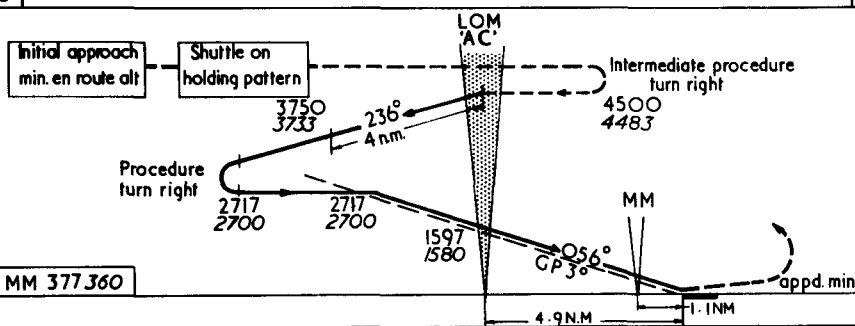
ILS R/W 06

GLASGOW ABBOTSINCH



Sect. safe alt.
25nm. 3900

Sect. safe alt.
25nm. 2800



G.P. height at MM 377360

Sect. safe alt.
25nm. 4400

04°30'W

20

Sect. safe alt.
25nm. 4300

GLASGOW ABBOTSINCH

ILS R/W 24

BRITISH EUROPEAN AIRWAYS

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APPROACH ABBOTSINCH APP.

119.1

TOWER ABBOTSINCH TWR

118.8

TRANS ALT 4000 FT.

G/S (KTS)	70	90	110	130	150
LOM TO TURN			3.15	2.45	2.24
LOM TO T/H	3.12	2.53	2.00	1.43	1.29
DESCENT ON G/P (FT/MIN)	370	480	580	690	800
G/S	170	190	210	230	250
LOM TO TURN	2.06	1.53	1.42	1.33	1.26
LOM TO T/H	1.19	1.11			
DESCENT ON G/P (FT/MIN)	900	1000			

MISSED APPROACH

Climb straight ahead to 1017 / 1000 climbing, turn right to return to 'ABI' at 3500 3483

ELEVATIONS (FT)

OFFICIAL AERODROME	23
LOWEST THRESHOLD	17
THRESHOLD R/W 24	18
VARIAION TO "W"	Bearings all magnetic
SERIAL No. 6208	DATE: 31.3.66
Reason for Re-Issue:	

NEW ISSUE

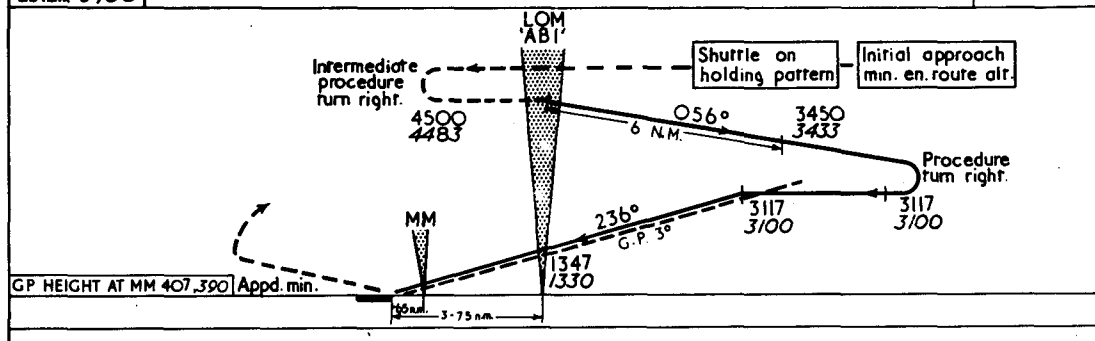
EFFECTIVE 2.5.66

ILS R/W 24

GLASGOW ABBOTSINCH

Sect. safe alt.
25nm. 3900

Sect. safe alt.
25nm. 2800



Sect. safe alt.
25 n.m. 4400

04 30W 20

Sect. safe alt.
25 n.m. 4300

GLASGOW ABBOTSINCH NDB R/W 06

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APPROACH ABBOTSINCH APP

119.1

TOWER ABBOTSINCH TWR

118.8

TRANS ALT 4000 FT.

G/S (KTS)	70	90	110	130	150
AC TO TURN			2.11	1.50	1.36
AC TO ADME	4.12	3.16	2.40	2.15	1.58
RATE OF DESC NDB TO AID	408	525	642	758	875
G/S	170	190	210	230	250
AC TO TURN	1.24	1.16	1.09	1.02	0.58
AC TO ADME	1.43	1.32			
RATE OF DESC NDB TO AID	992	1108			

MISSED APPROACH

Climb straight ahead to 1017,000, climbing
turn right to return to 'AC' at 3500,3483

ELEVATIONS (FT)

OFFICIAL AERODROME	23
LOWEST THRESHOLD	17
THRESHOLD R/W 06	21
VARIATION 10° W	Bearings all magnetic
SERIAL No. 6208	DATE: 31.3.66
Reason for Re-issue:	

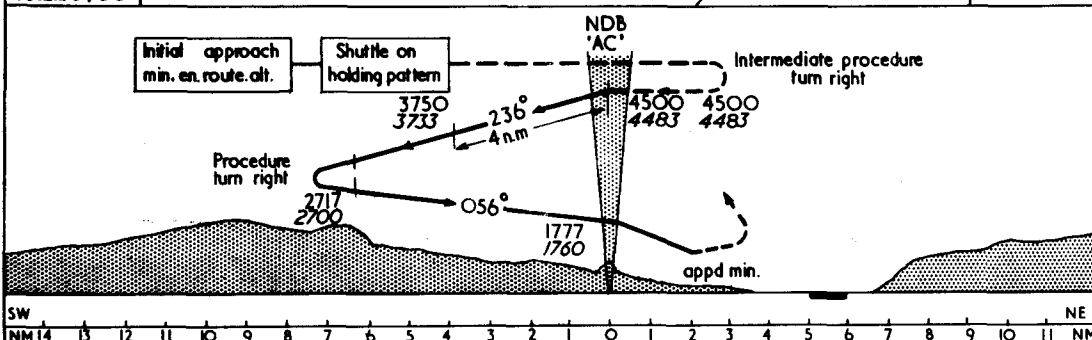
NEW ISSUE EFFECTIVE 2.5.66

NDB R/W 06

GLASGOW ABBOTSINCH

Sect. safe alt.
25 n.m. 3900

Sect. safe alt.
25 n.m. 2800



GUERNSEY AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
10	H.I. WHITE & L.I. RED CENTRE-LINE & BAR	2.75°
28	H.I. WHITE & L.I. RED CENTRE-LINE & BAR	2.75°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
10	H.I. GREEN BI-DIRECTIONAL & H.I. GREEN WING BARS.	ELEVATED H.I. BI-DIRECTIONAL L.I. OMNI-DIRECTIONAL
28	H.I. GREEN BI-DIRECTIONAL & H.I. GREEN WING BARS	ELEVATED H.I. BI-DIRECTIONAL L.I. OMNI-DIRECTIONAL

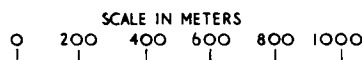
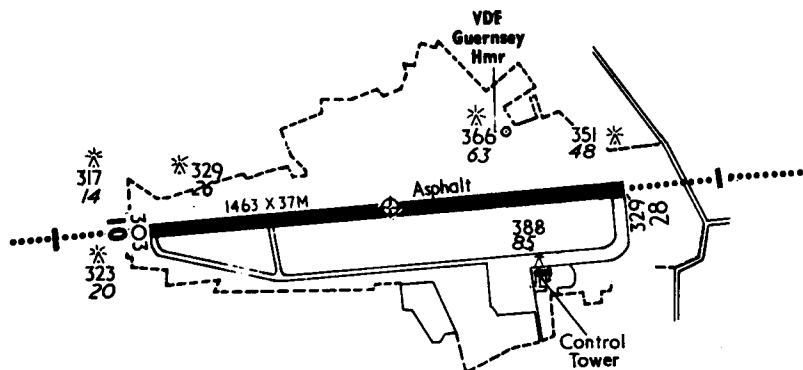
OBSTRUCTION, TAXIWAY,
APRON FLOODLIGHTS, STOPWAY (RED).

ELEVATIONS (FT)	a.m.s.l.	388
	above lowest T/H	85
OFFICIAL AERODROME		336
LOWEST THRESHOLD (O)		303
LAT	49° 26' N.	LONG 02° 36' W
VARIATION	8° W	Bearings all magnetic
SERIAL No.	50191/1	DATE: 17. 6. 65.

Reason for Re-Issue:

VASI R/W 28

AERODROME CHART GUERNSEY

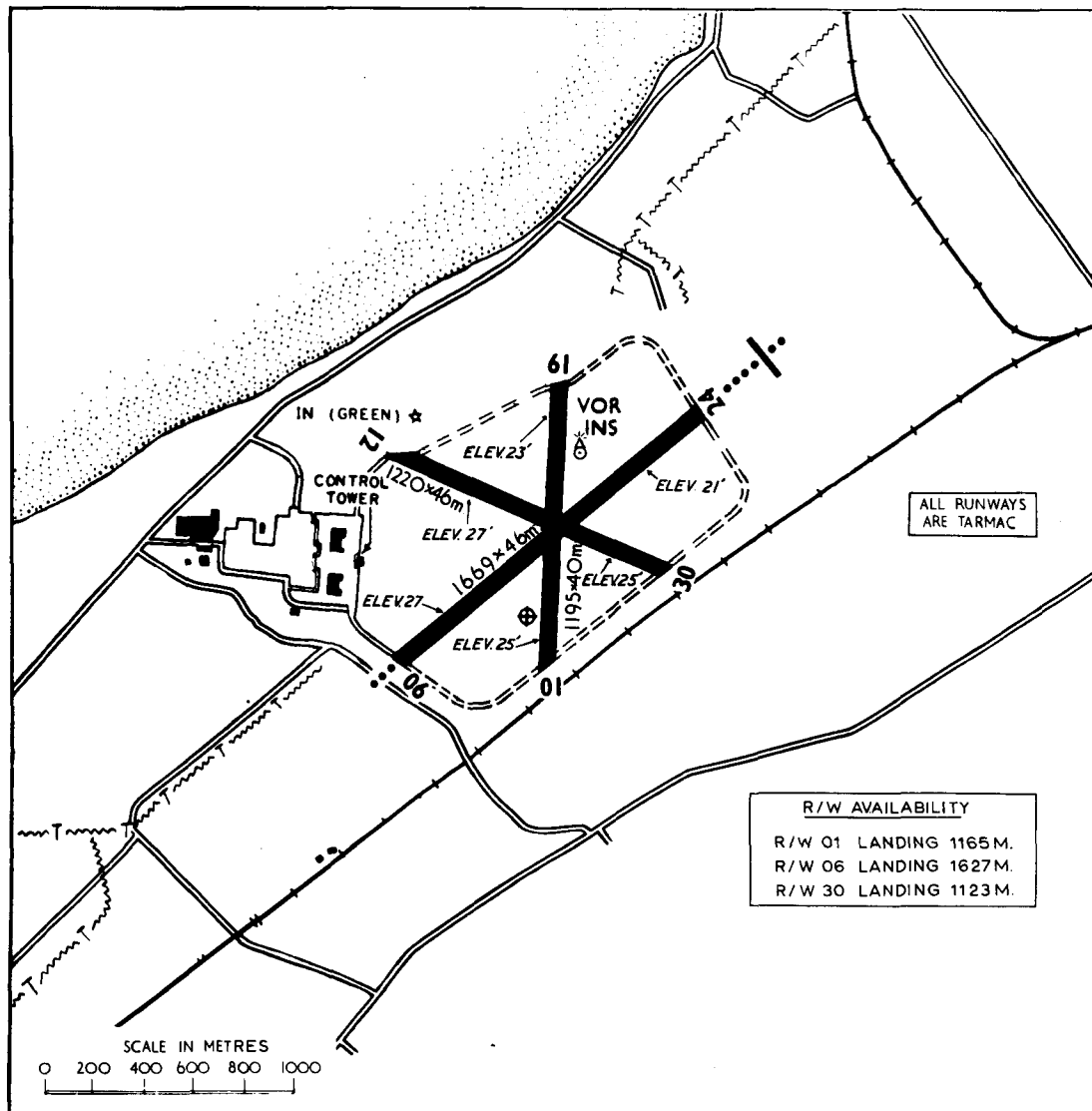


STOPWAYS:

R/W 10 AND 28 — 91m.

WARNING:

GROUND SLOPES STEEPLY 122m. EAST OF R/W 28.
MARKED BY RED LIGHTS.



R/W AVAILABILITY

R/W 01 LANDING 1165M.
R/W 06 LANDING 1627M.
R/W 30 LANDING 1123M.

INVERNESS (DALCROSS) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI (CAO) Type Angle
24	VARIABLE INT WHITE C/L & BAR	3°
06	SHORT SODIUM CENTRE-LINE	
01 19 12 30	NIL	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
24.	SODIUM WING BARS	OMNI-DIRECTIONAL & GOOSENECK FLARES
06.	NIL	
01. 19. 12 30	NIL	NIL

OBSTRUCTION, IDENT. BEACON 'IN' (GREEN) APRON.
FLOODS, TAXIWAY (BETWEEN R/W 06 AND R/W 12
AND APRON ONLY)

ELEVATIONS (FT) :—

OFFICIAL AERODROME 31

LOWEST THRESHOLD (24) 21

LAT 57° 32' N, LONG 04° 03' W

VARIATION 11° W Bearings all magnetic

SERIAL No 50128/1 DATE 15.7.65

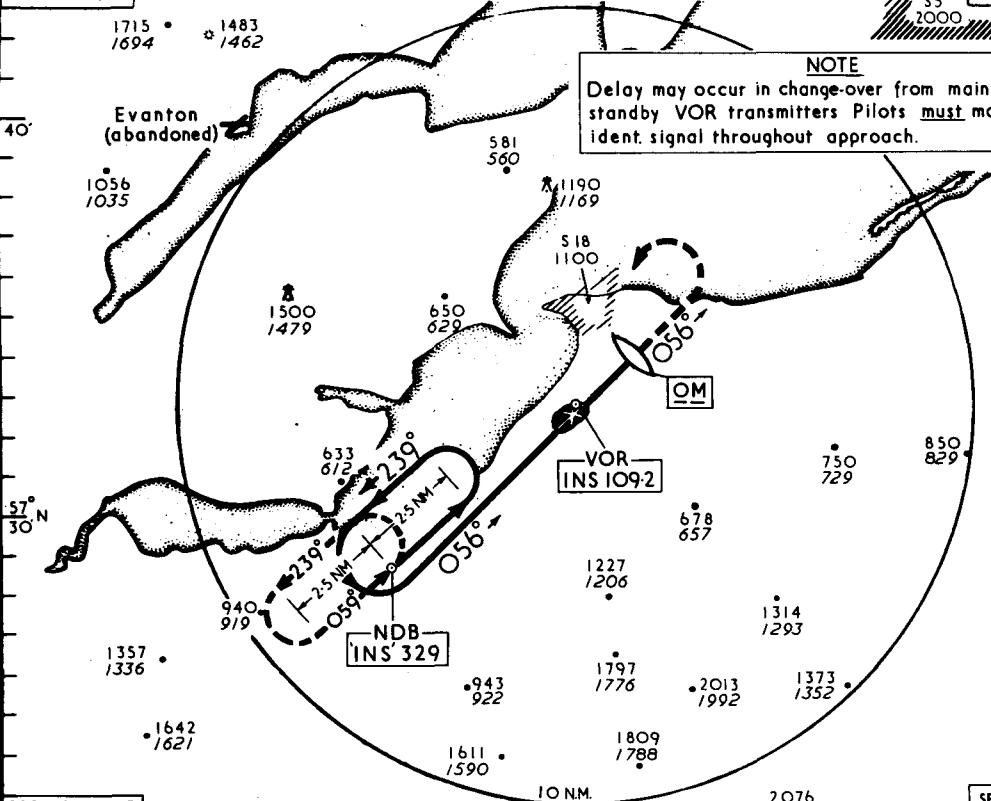
Reason for Re-issue:

LIGHTING

AERODROME CHART
INVERNESS (DALCROSS)

SECT. SAFE ALT.
25 N.M. 4500'

SECT. SAFE ALT.
25 N.M. 3800'



SECT. SAFE ALT.
25 N.M. 4800'

SECT. SAFE ALT.
25 N.M. 5000'

INITIAL APPROACH
MIN. EN ROUTE ALT.
OR AS DIRECTED
BY ATC.

NDB 'INS'
3500
3469
TURN LEFT
2332
2300
TURN LEFT
239°
25 NM
059°
056°

VOR

OM

APPD. MIN.

INVERNESS

NDB/VOR R/W 06

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APPROACH DALLCROSS TWR.

118.3

TOWER DALLCROSS TWR.

118.3

TRANS ALT 3000 FT.

G/S (KTS)	70	90	110	130	150
NDB TO TURN	2.09	1.40	1.22	1.09	1.0
NDB TO ADME	4.43	3.40	3.0	2.32	2.12

G/S	170	190	210	230	250
NDB TO TURN	0.53	0.47	0.43	0.39	0.36
NDB TO ADME	1.58	1.44			

OVERSHOOT

CLIMB AHEAD TO 1000 979 THEN CLIMBING
TURN LEFT TO OVERHEAD NDB AT 2500 2479

ELEVATIONS (FT) :—

OFFICIAL AERODROME 31

LOWEST THRESHOLD 21

THRESHOLD R/W 06 27

VARIATION 11°W Bearings all magnetic

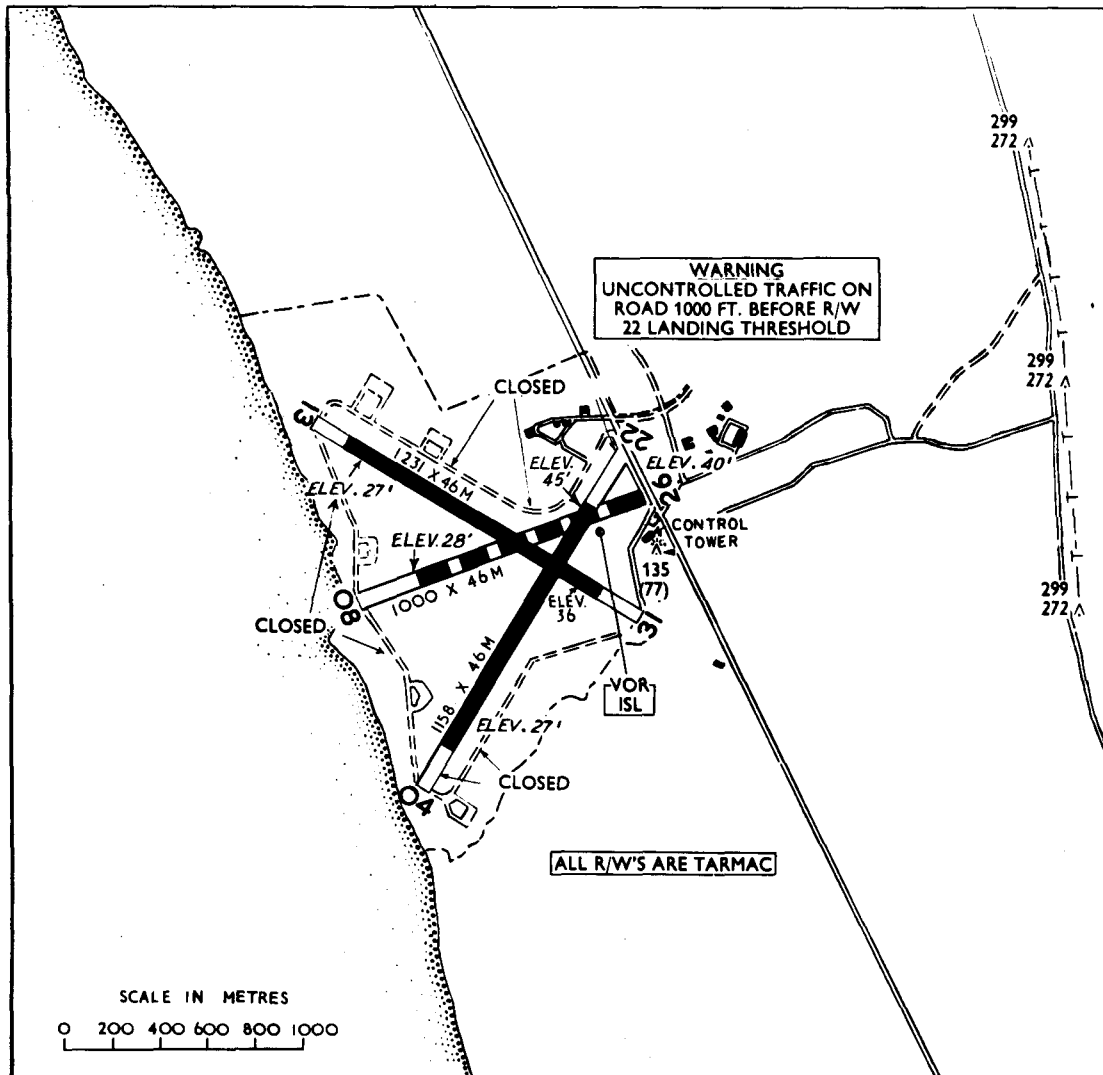
SERIAL No 50128/2 DATE: 15.7.65

Reason for Re-issue:

CRITICAL HT DELETED

NDB/VOR R/W 06

INVERNESS



ISLAY AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
13	NIL	3°
22	SODIUM CLUSTER (4 LIGHTS) 2865M BEFORE THRESHOLD.	
26	NIL	
31	SODIUM CLUSTER (4 LIGHTS) 2286M BEFORE THRESHOLD.	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
04 08 13 22 26 31	NIL	GOOSENECKS (EMERGENCY)

OBSTRUCTION, TAXIWAY,

ELEVATIONS (FT)

OFFICIAL AERODROME 58

LOWEST THRESHOLD (04 & 13) 27

LAT 55° 41' N. LONG 06° 15' W

VARIATION 12° W Bearings all magnetic

SERIAL No. 50388/1 DATE: 28. 6. 65.

Reason for Re-issue:

R/W LENGTHS & STOPWAYS

AERODROME CHART ISLAY

SPECIAL WARNING R/W 08/26

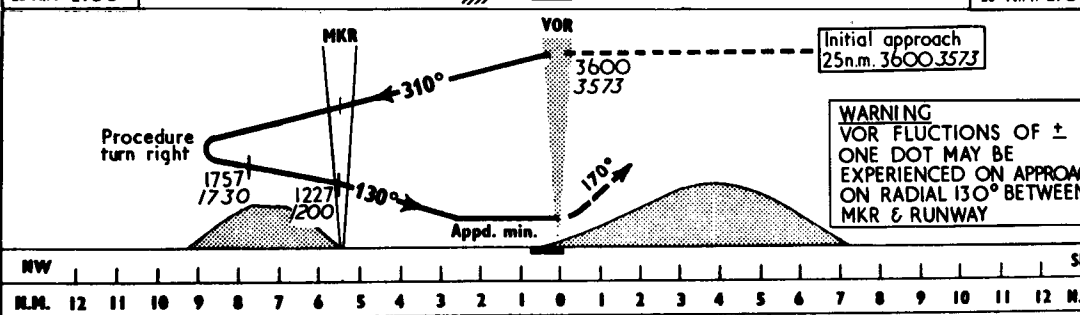
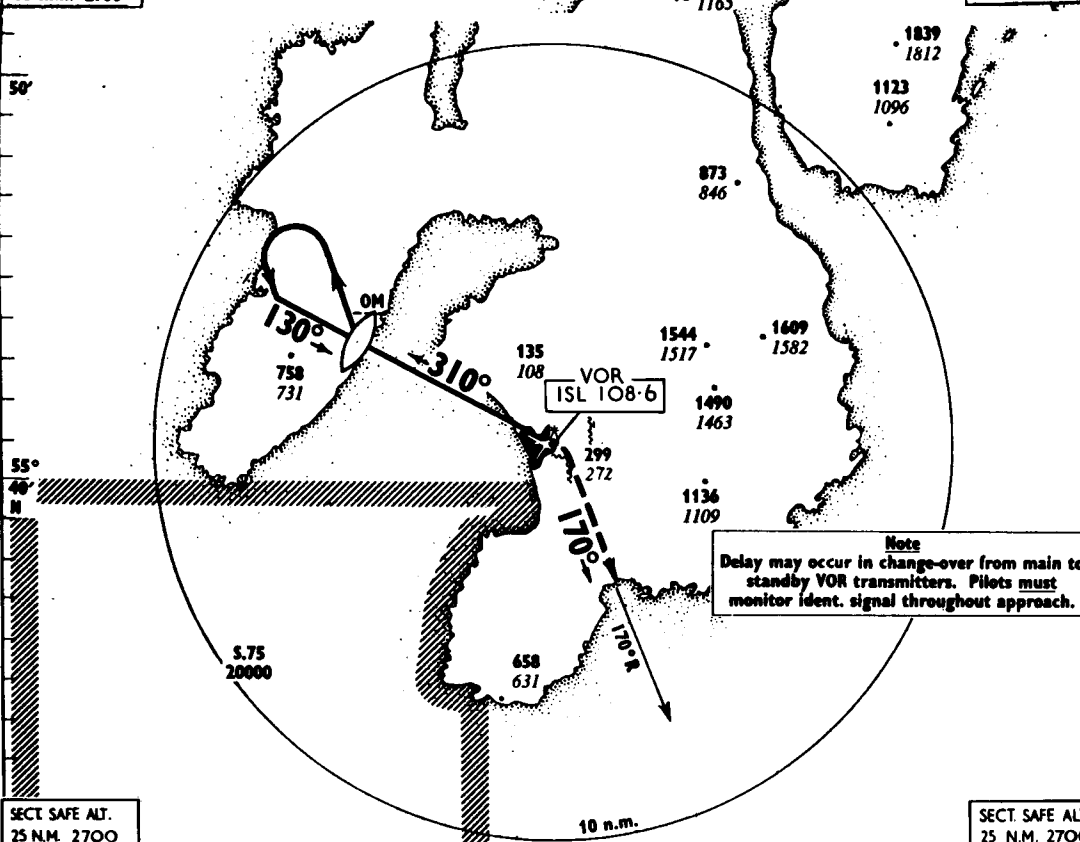
THIS RUNWAY IS OFFICIALLY CLOSED, BUT MAY BE USED AT CAPTAINS DISCRETION AND THEN ONLY BY CAPTAINS WITH RECENT & REGULAR AIRFIELD EXPERIENCE. THIS IS BECAUSE THE RUNWAY IS NO LONGER BEING MAINTAINED, BUT WILL BE INSPECTED FOR LOOSE STONES ETC, IF PILOTS WISH TO USE IT.

STOPWAYS:

R/W'S 04 - 268 M
22 - 76 M
13 - 168 M
31 - 153 M

Sect. Safe Alt.
25 n.m. 2700'

Sect. Safe Alt.
25 n.m. 3600'



ISLAY VOR R/W 13

BRITISH EUROPEAN AIRWAYS

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APPROACH

TOWER

PORT ELLEN

(AFIS ONLY)

118.5

TRANS ALT

3000

FT.

G/S (KTS)	70	90	110	130	150
MKR TO ADME	4.01	3.08	2.34	2.10	1.53

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

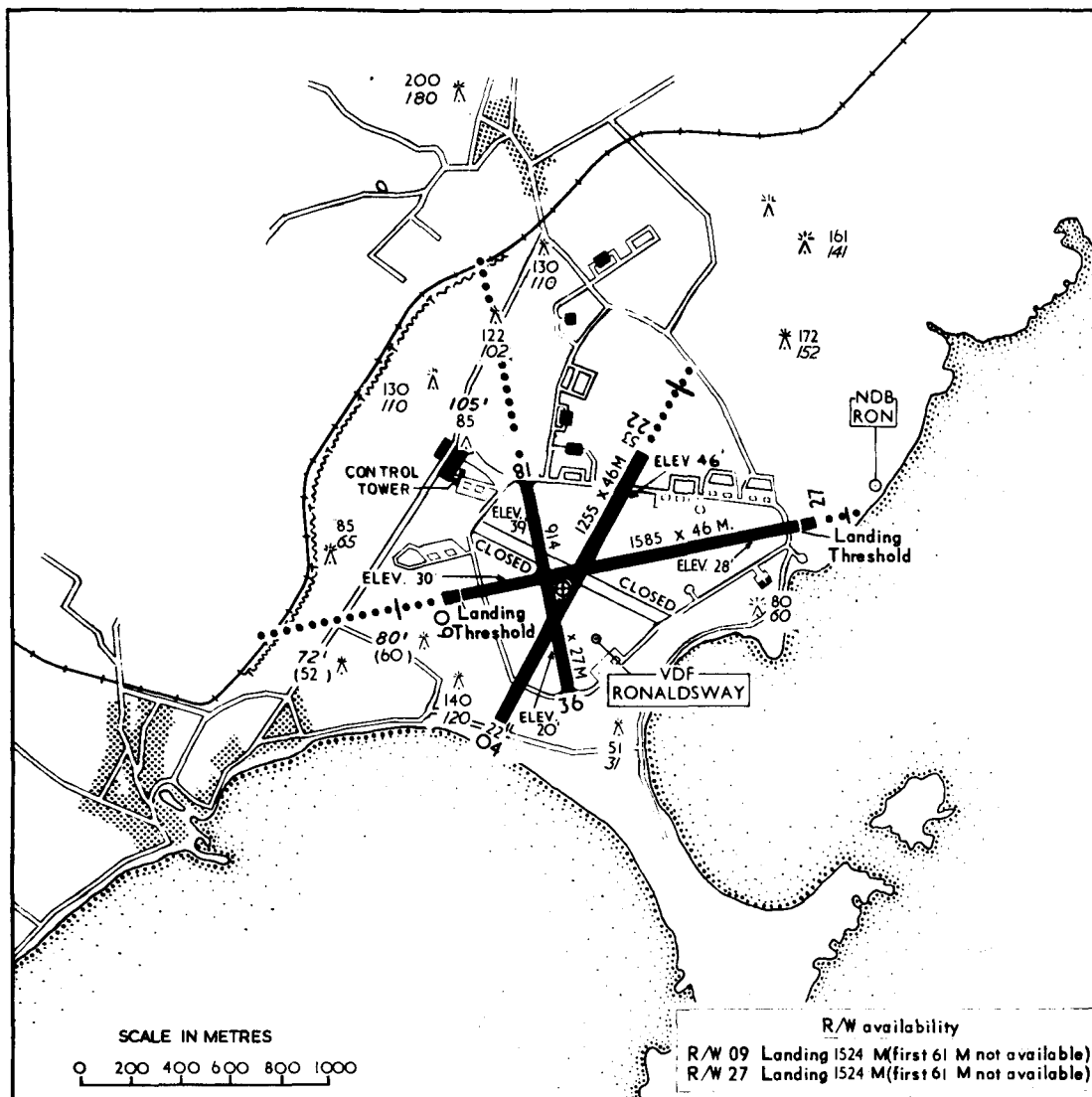
G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			

G/S	170	190	210	230	250
MKR TO ADME	1.40	1.29			



I.O.M. RONALDSWAY AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
04	NIL	
22	L.I. RED CENTRE-LINE & BAR	
18	L.I. RED CENTRE-LINE	
36	SODIUM BOX	
09	SODIUM CENTRE-LINE & BAR L.I. RED CENTRE-LINE & BAR	
27	H.I. CENTRE-LINE & BAR L.I. RED CENTRE-LINE & BAR	25°

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
04	L.I. GREEN WITH SODIUM WINGBARS	L.I. OMNI-DIRECTIONAL
22	L.I. GREEN	
18		
36		
09		
27		

OBSTRUCTION, TAXIWAY, ADME BEACON
LANDING INDICATOR, FLOOD, ANGLE OF APPROACH
(R/W 04)

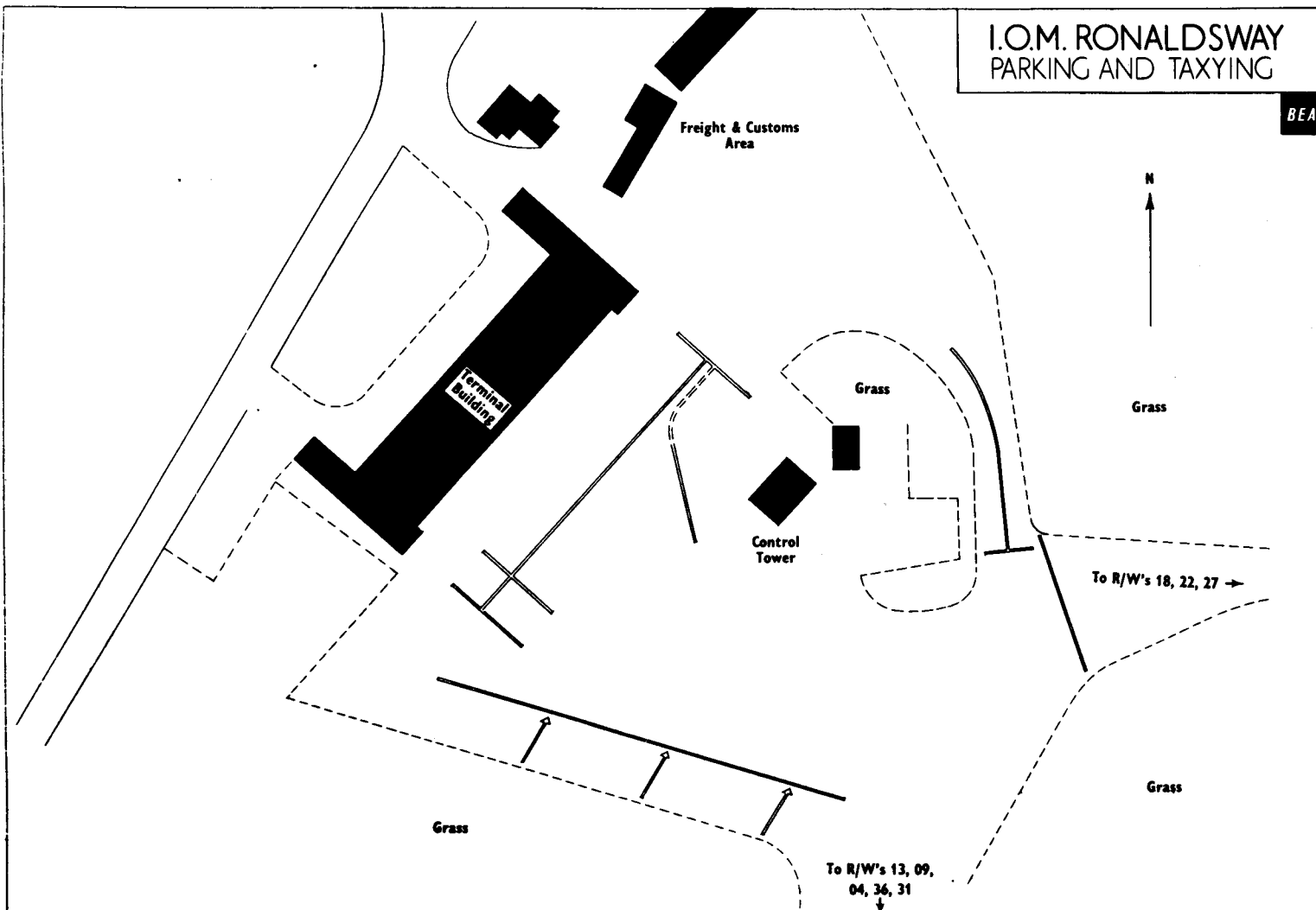
ELEVATIONS (FT)

OFFICIAL AERODROME	55
LOWEST THRESHOLD ()	20
LAT	54° 05' N, LONG 04° 38' W
VARIATION 10° W	Bearings all magnetic
SERIAL No. 50080/1	DATE: 28.6.65
Reason for Re-issue:	R/W LENGTHS & OBSTRUCTIONS

AERODROME CHART I.O.M. RONALDSWAY

I.O.M. RONALDSWAY PARKING AND TAXYING

BEA



NOTE

CENTRE - LINES ON TARMAC PROVIDE WING TIP CLEARANCE OF AT LEAST 20 FT. BETWEEN A/C PARKED ON STOP LINES AND BUILDINGS.

SERIAL No 21297

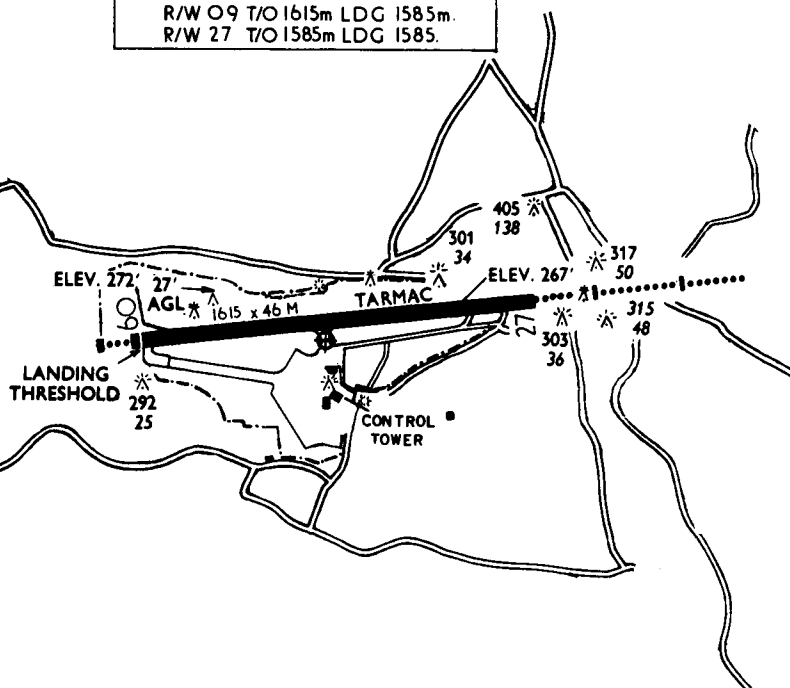
DATE 5.8.64

Reason for Re-issue:

PARKING AND TAXYING
I.O.M. RONALDSWAY

RUNWAY AVAILABILITY
 First 30m. at west end lit by red lights & is available for take-off only in the 09 direction.
 R/W 09 T/O 1615m LDG 1585m.
 R/W 27 T/O 1585m LDG 1585.

NDB
JW



SCALE IN METRES
 0 200 400 600 800 1000

JERSEY AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
CAO
Type
Angle

R/W	APPROACH	
09	HIGH & LOW INT. RED CENTRE LINE & BAR	3°
27	HIGH & LOW INT RED CENTRE LINE & 2 BARS	3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
09	GREEN WITH WINGBAR	H.I. BI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL (BOTH ELEVATED)
27	HIGH INT GREEN WITH WINGBAR	

OBSTRUCTION, TAXIWAY,

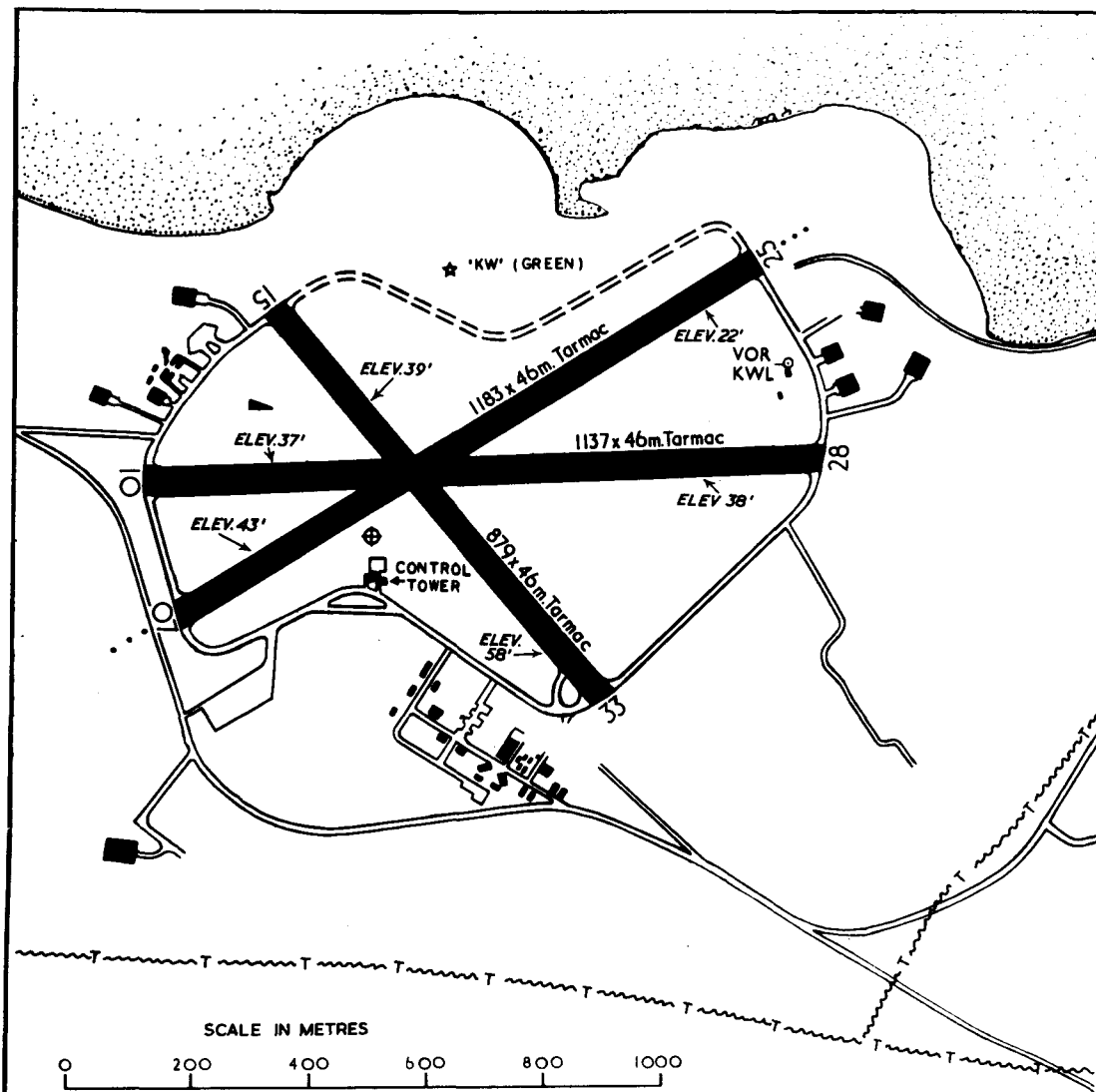
ELEVATIONS (FT) :-

OFFICIAL AERODROME	276
LOWEST THRESHOLD (27)	267
LAT 49° 13' N, LONG 02° 11' W	
VARIATION 8° W Bearings all magnetic	
SERIAL No. 50220/1	DATE : 13.4.65

Reason for Re-issue:

R/W LENGTH

AERODROME CHART JERSEY



NOTES:
R/W 15/33 HAS DOWN GRADIENT OF 1:50 ON SECTION 550m
LONG COMMENCING AT S.E. END OF R/W.
TAXIWAYS ON N.W. & S.E. SIDES OF A/D ARE UNSUITABLE FOR A/C WITH A
WINGSPAN GREATER THAN 40FT.

WARNING
UNCONTROLLED TRAFFIC ON ROAD 60M. BEFORE
R/W 07 AND 92M. BEFORE R/W 10 LANDING
THRESHOLDS.

KIRKWALL AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
07	3 SODIUM LIGHTS	
25	3 SODIUM GUIDE LIGHTS EACH SIDE BEAMED AT RIGHT ANGLES TO C.L.	3°

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
07	NIL	GOOSENECKS
25	NIL	

OBSTRUCTION, IDENT BEACON 'KW' (GREEN),
WIND INDICATOR

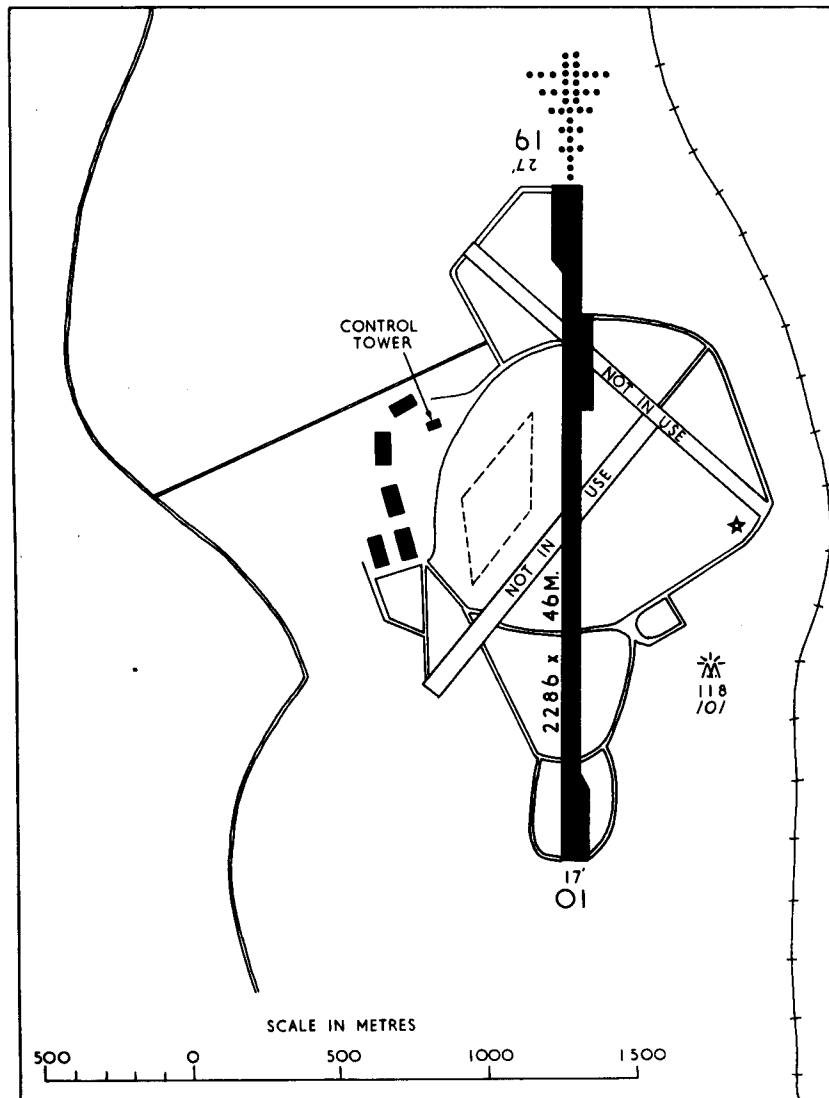
ELEVATIONS (FT)

OFFICIAL AERODROME	69
LOWEST THRESHOLD (25)	22
LAT 58° 57' N, LONG 02° 54' W	
VARIATION 10° W	Bearings all magnetic
SERIAL No 50055/1	DATE 15.7.65.

Reason for Re-Issue:

LIGHTING & VARIATION

AERODROME CHART KIRKWALL



LECONFIELD AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
01	NIL	
19	H.I. CENTRE LINE & BARS	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
01	} NIL	} H.I. WHITE
19		

OBSTRUCTION, TAXIWAY, ADME BEACON

ELEVATIONS (FT) :—

OFFICIAL AERODROME	29
LOWEST THRESHOLD (01)	17
LAT 53° 53' N, LONG 00° 26' W	
VARIATION 8° W	Bearings all magnetic
SERIAL No. 40151	DATE: 4.12.64

Reason for Re-issue:

NEW ISSUE

AERODROME CHART
LECONFIELD

Sect Safe Alt
25nm 2100

• 807
790

53°
50' N

WARNING
Localizer offset

Localizer Beam	183°
Runway Centreline	187°

BLUE

YELLOW

OM

MM

119 A 102

190°

A 231 214

ILS LCII01

119 102

003°

603°

Sect Safe Alt
25nm 1800

N 64
2000

Sect Safe Alt
25nm 1800

G/S (KTS)	70	90	110	130	150
LLZ TOTURN			4 . 22	3 . 42	3 . 12
OM TO T/H	4 . 17	3 . 20	2 . 44	2 . 18	2 . 0
DESCENT ON G/P (FT/MIN)	370	480	580	690	800
G/S	170	190	210	230	250
LLZ TO TURN	2 . 50	2 . 32	2 . 17	2 . 06	1 . 56
OM TO T/H	1 . 46	1 . 35			
DESCENT ON G/P (FT/MIN)	900	1010			

Climb on 190° to 26172600
or as instructions.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	29'
--------------------	-----

LOWEST THRESHOLD	17'
------------------	-----

THRESHOLD R/W	9	27'
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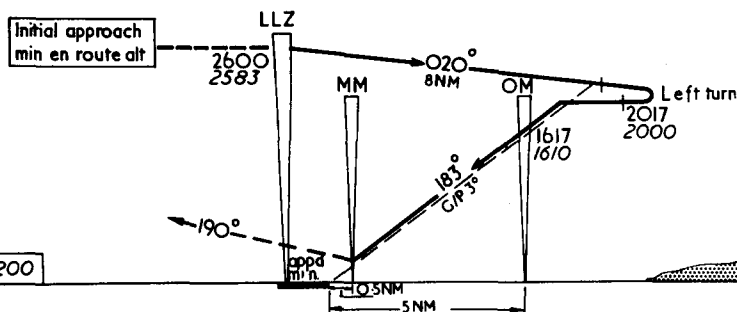
VARIATION 8 °W	Bearings all magnetic
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SERIAL No. 50356	DATE : 31 5 65
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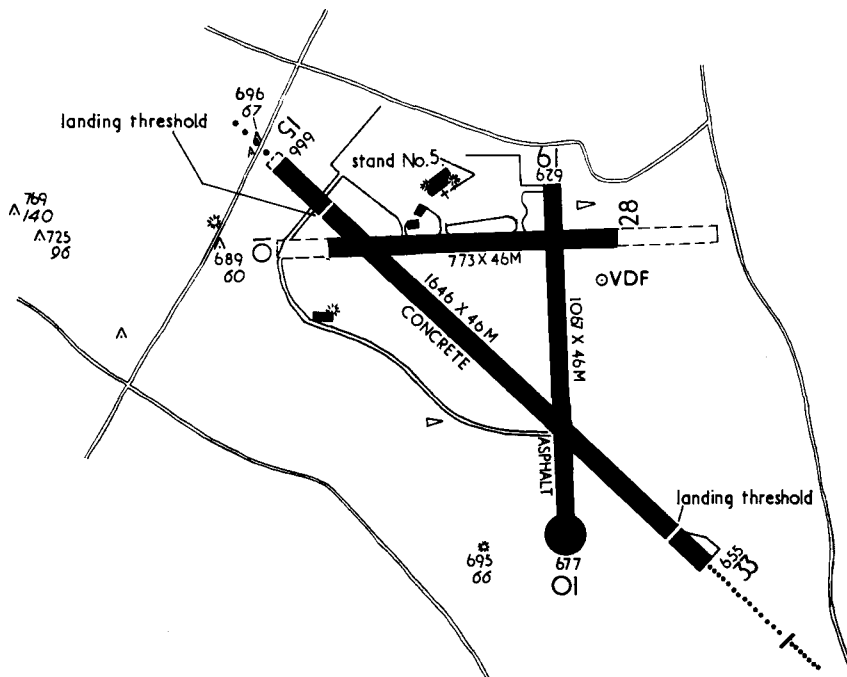
Reason for Re-Issue:

NEW ISSUE

ILS R/W 19
LECONFIELD



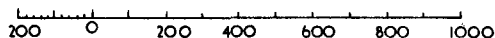
GP HGT at MM 217200



Runway Availability

R/W 15 T/O Dist. 1600m Idgdist 1419m.
R/W 33 T/O Dist. 1524m Idgdist 1524m.

SCALE IN METRES



WARNING. Ground falls away sharply at SE end R/W 15.

NOTE 1. Taxi only on runways.

2. No marshalling on apron. BEA aircraft will normally be allocated stand No. 5

LEEDS & BRADFORD AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
01/19	NIL	
33	H1 & LI CENTRE LINE 427M WITH ONE CROSSBAR AT 305 M	3°
15	SHORT LEAD IN LIGHTS	3°
10/28	NIL	3° R/W 28

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
01/19	GREEN LI	LI OMNI-DIRECTIONAL
15	GREEN WING BARS	41 BI-DIRECTIONAL WITH LI OMNI-DIRECTIONAL COMPONENT
33	GREEN	
10/28	NIL	NIL

OBSTRUCTION, TAXIWAY, ADME BEACON,
APRON FLOOD, LANDING DIRECTION
INDICATOR.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	678
LOWEST THRESHOLD (19)	629
LAT 53° 52' N, LONG 01° 39' W	
VARIATION 9° W	Bearings all magnetic
SERIAL No. 50323 /1	DATE 11.5.65

Reason for Re-Issue:

PARKING STAND

AERODROME CHART LEEDS & BRADFORD

Sect Safe Alt
25nm 3500

50°
1500
871

01° 40' W 30'

Sect Safe Alt
25nm 3000

LEEDS & BRADFORD

NDB (BCP)

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APPROACH LEEDS TWR

122.3

TOWER LEEDS TWR

122.3

TRANS ALT 3000 FT.

G/S (KTS)	70	90	110	130	150
NDB TO TURN	2.09	1.40	1.22	1.09	1.0

G/S	170	190	210	230	250
NDB TO TURN	0.53	0.47			

MISSED APPROACH

Turn left onto 260° climbing to 1629/000,
then climbing turn left returning to NDB
at 2479 /850

ELEVATIONS (FT) :—

OFFICIAL AERODROME 678

LOWEST THRESHOLD 629

VARIATION 9° W Bearings all magnetic

SERIAL No 50309 DATE 10.5.65

Reason for Re-Issue:

NEW PROCEDURE

NDB (BCP)

LEEDS & BRADFORD

54°
00'

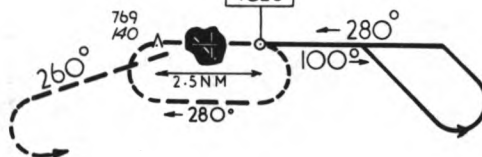
1341
712

Δ 1250
621

1321°
692

NDB
LBA
4025

769
140



30'

1465
836

1320
691

10 NM

Sect Safe Alt
25nm 3500

Sect Safe Alt
25nm 3000

Initial Approach
min en route alt

Shuttle on
holding

NDB
3500
2871

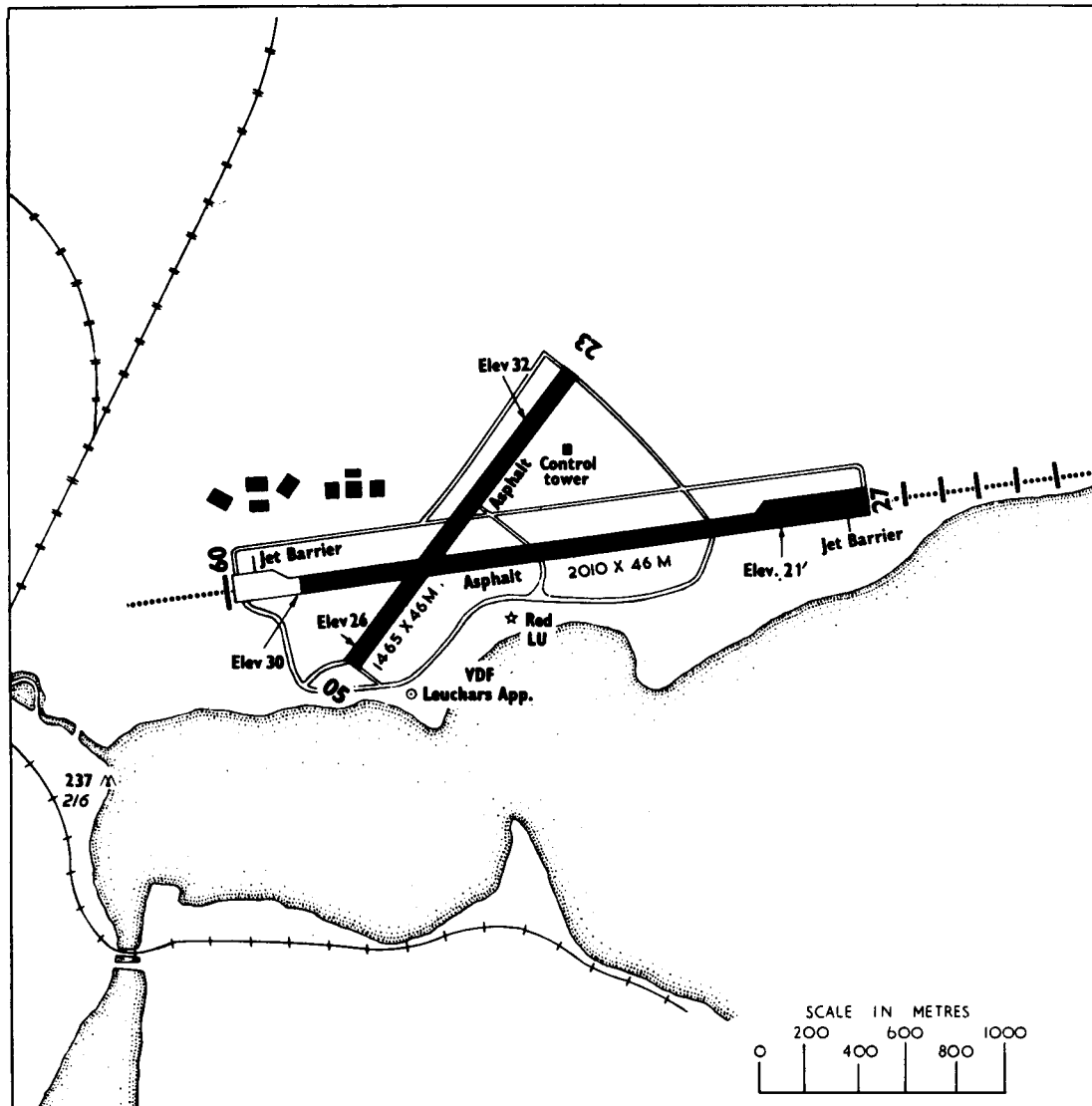
100°
2.5 NM

Procedure turn right

2029
1400

appd min

W NM 14 13 12 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 NM E



LEUCHARS AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
27	H.I. CENTRE-LINE & BARS	X
09	L.I. RED 'TEE'	X
05, 23	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
27	SODIUM WINGS	H.I. UNI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL SODIUMS
09	NIL	L.I. OMNI-DIRECTIONAL
05		
23		

OBSTRUCTION, TAXIWAY, IDENT BEACON, LU (RED)

ELEVATIONS (FT)	a.m.s.l.	237
	above lowest T/H	216
OFFICIAL AERODROME		38
LOWEST THRESHOLD (27)		21
LAT 56° 22' N, LONG 02° 52' W		
VARIATION 10° W	Bearings all magnetic	
SERIAL No. 30191	DATE: 5.8.64	
Reason for Re-Issue:		

OTHER LIGHTING:

CIRCUITS: R/Ws 23 & 27 RIGHT HAND. AVOID SAFETY LANE
070° M - 110° M WHEN JOINING CIRCUIT.

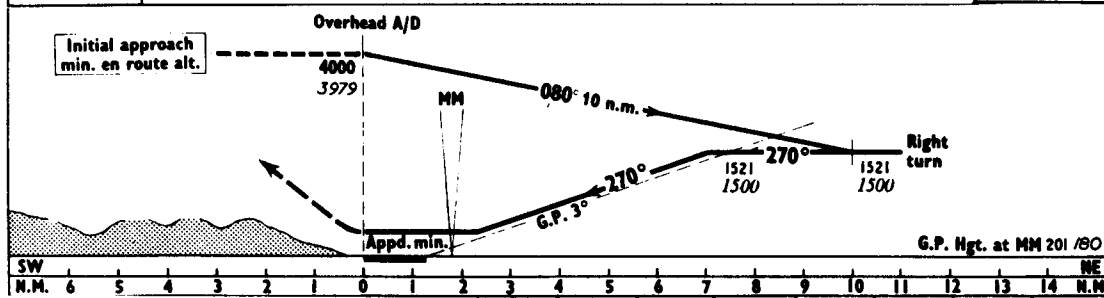
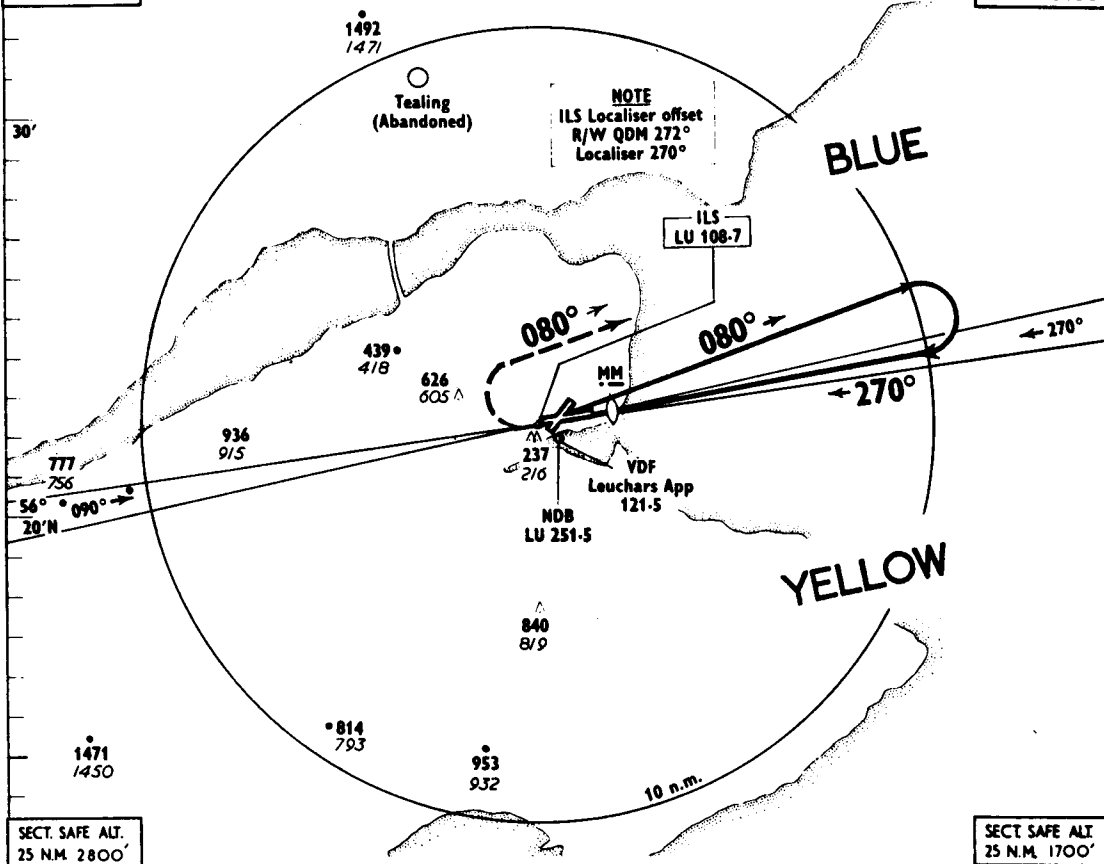
JET BARRIERS: R/Ws 09 & 27

AERODROME CHART LEUCHARS

SECT. SAFE ALT.
25 N.M. 3400

10' 03° 00' W 02° 50' W 40'

SECT. SAFE ALT.
25 N.M. 3400



LEUCHARS ILS R/W 27

BRITISH EUROPEAN AIRWAYS

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APPROACH	LEUCHARS APP.
	121.5 130.7
TOWER	LEUCHARS TWR.
	121.5
TRANS ALT	3000 FT.

G/S (KTS)	70	90	110	130	150
ADME TO TURN	8 m34s	6 m40s	5 m27s	4 m37s	4 m0s
	m s	m s	m s	m s	m s
DESCENT ON G/P (FT/MIN)	371	477	583	689	796
G/S	170	190	210	230	250
ADME TO TURN	3 m32s	3 m09s	2 m51s	2 m37s	2 m24s
	m s	m s	m s	m s	m s
DESCENT ON G/P (FT/MIN)	902	1008	1114	1221	1326

CRITICAL HEIGHTS (FT)

TRIDENT	AUTO	350	MANUAL	400
OTHER TYPES	WITH G/P	350	NO G/P	350

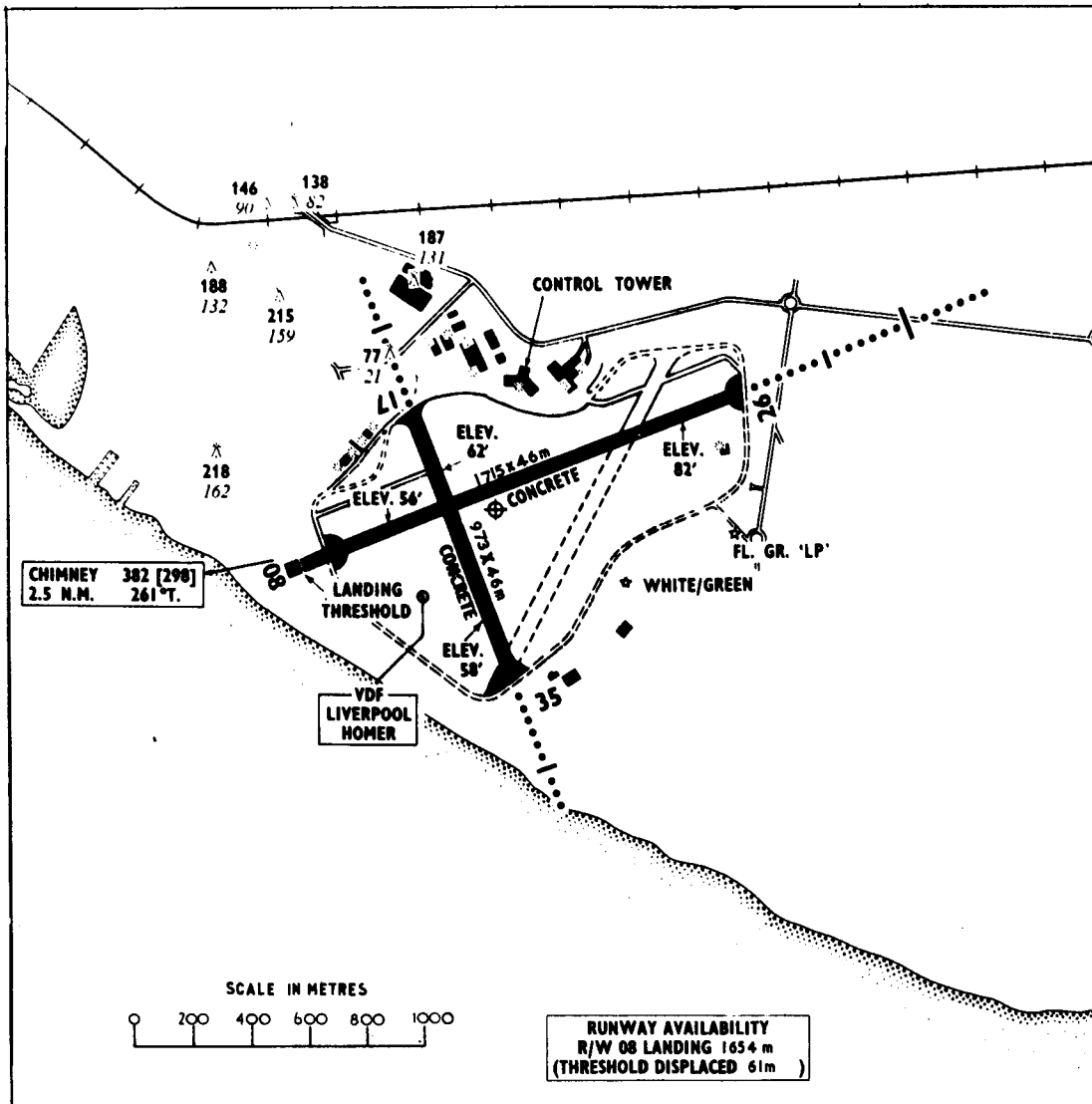
OVERSHOOT

CLIMB AHEAD TO 521 500
THEN CLIMBING TURN RIGHT ONTO 080° TO
4000 3979 OR AS DIRECTED BY ATC.

ELEVATIONS (FT)	a.m.s.l. 1521 above lowest T/H 1500
-----------------	--

OFFICIAL AERODROME	38
LOWEST THRESHOLD	21
THRESHOLD R/W 27	21
VARIATION 10° W	Bearings all magnetic
SERIAL No. 48m	DATE: 5.8.64
Reason for Re-issue:	

ILS R/W 27 LEUCHARS



LIVERPOOL AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

Prepared and Issued by Flight Operations
Dept. as part of the Operations Manual

LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
08	NIL	3°
26	H.I. CENTRE-LINE & SODIUM BARS L.I. RED CENTRE-LINE & BARS	
17/35	H.I. WHITE & L.I. RED CENTRE-LINE & BAR	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08	L.I. & H.I. GREEN ELEVATED WITH WINGS	H.I. BI-DIRECTIONAL & L.I. ELEVATED OMNI-DIRECTIONAL
26	(R/W 08 61M UP R/W)	
17/35	L.I. & H.I. GREEN ELEVATED	H.I. BI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL ELEVATED

OBSTRUCTION, TAXIWAY, ADME BEACON
APRON FLOODS, IDENT BEACON 'LP' (GREEN)

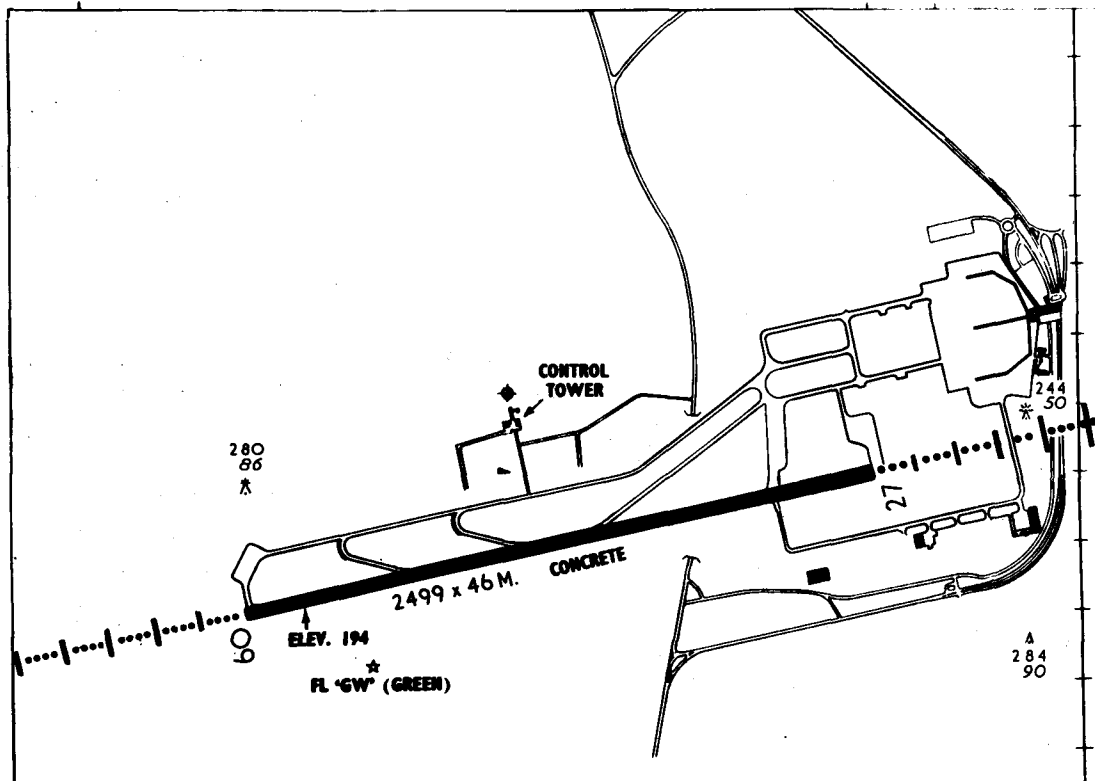
ELEVATIONS (FT)	a.m.s.l.	187
	above lowest T/H	131
OFFICIAL AERODROME		84
LOWEST THRESHOLD (08)		56
LAT 53° 21' N. LONG 02° 53' W		
VARIATION 9° W	Bearings all magnetic	
SERIAL No. 32098	DATE: 28.7.64	
Reason for Re-issue:		

AERODROME CHART LIVERPOOL

STOPWAY: R/W 08 46m.

WARNING: WHEN R/W 17 IN USE AT NIGHT, THE NORTH FACE OF ROAD TRAFFIC SCREEN IS FLOODLIT.

NOTE: VARIABLE CIRCUITS AT THE DISCRETION OF ATC.



LONDON (GATWICK) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
09/ 27	H.I. WHITE & 5 CROSSBARS L.I. RED CENTRE-LINE & 2 CROSSBARS	3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
09	H.I. & L.I. GREEN H.I. GREEN WINGBARS	H.I. ELEVATED (LAST 610M YELLOW) WHITE TOUCHDOWN ZONE LIGHTS (FLUSH FOR FIRST 914 M. AT EACH END)
27		

OBSTRUCTION, IDENT. BEACON 'GW', APRON FLOOD
TAXIWAY : GREEN C/L ON MAIN TAXIWAY WITH
REFLECTIVE EDGE MARKERS. BLUE EDGE MARKERS
ELSEWHERE.

ELEVATIONS (FT) :—

OFFICIAL AERODROME 194

LAT 51° 09' N, LONG 00° 11' W

VARIATION 8° W Bearings all magnetic

SERIAL No. 50403/4 DATE : 17.6.65

Reason for Re-issue:

LIGHTING, TAXIWAYS & S. PIER.

AERODROME CHART LONDON (GATWICK)

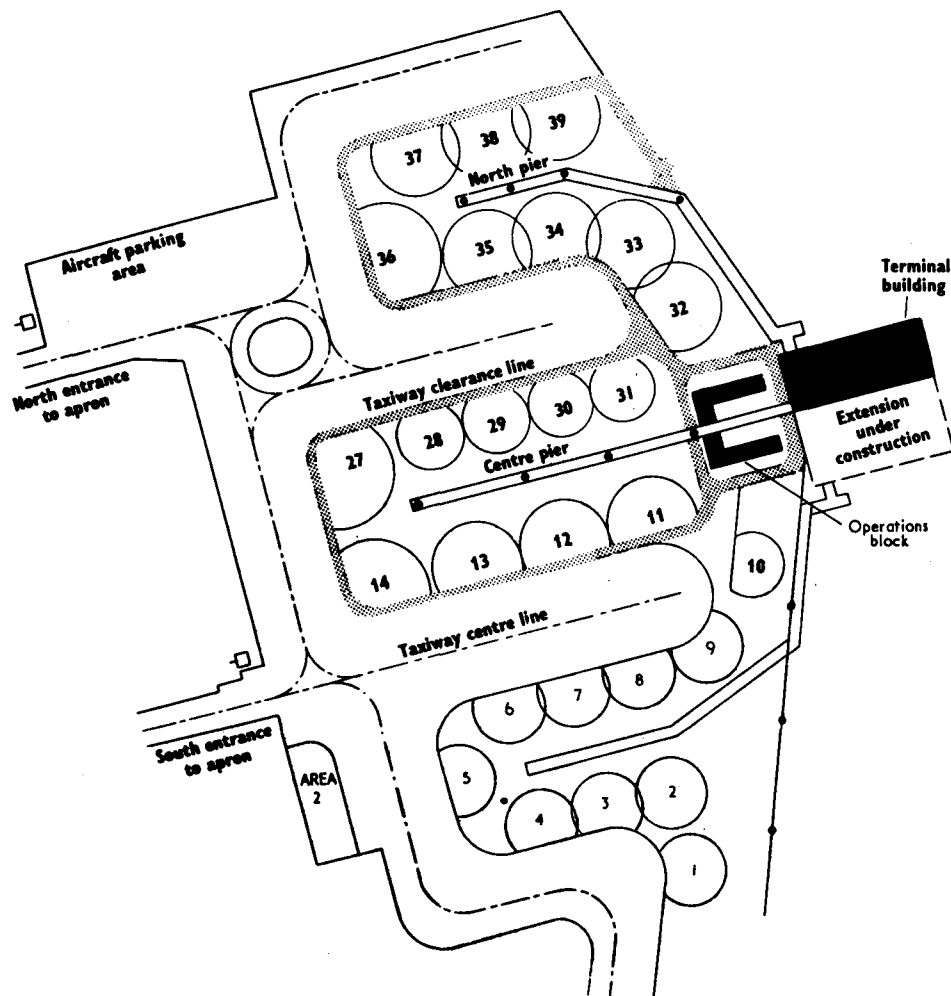
RUNNING UP OF A/C ENGINES IN S.W. CORNER OF R/W 09 RUN-UP AREA (APPROX. 46M. X 46M.) PROHIBITED. RESTRICTED AREA
MARKED BY WHITE LINE AND DIAGONALS.

CAUTION: IN POOR VISIBILITY AT NIGHT DO NOT CONFUSE APRON LIGHTING WITH APPROACH LIGHTS ON APPROACH TO R/W 27.

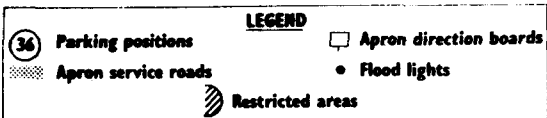
STOPWAY: R/W 27 - 61M.

R/W 09 - 61M

NOTE : "DISTANCE TO GO" MARKERS IN THOUSANDS OF FEET R/W 27



SCALE IN METERS
0 200 400 600 800 1000



LONDON GATWICK APRON AND PARKING POSITIONS

BRITISH EUROPEAN AIRWAYS

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AIRCRAFT SHOULD USE THE PARKING
POSITIONS AS LISTED BELOW:-

POSN. NO.	AIRCRAFT TYPE
1-4	FREIGHT AIRCRAFT
11-13, 27-31	UP TO CONVAIR
10, 32	UP TO SUPER CONSTELLATION
14, 36	UP TO BOEING 707
33	UP TO CONSTELLATION
34-35, 37-39	UP TO DC6B

MARSHALLING IS NOT NORMALLY PROVIDED
EXCEPT FOR LARGE A/C AT POSN'S 1-4.

SERIAL No 50403 DATE 16.6.65

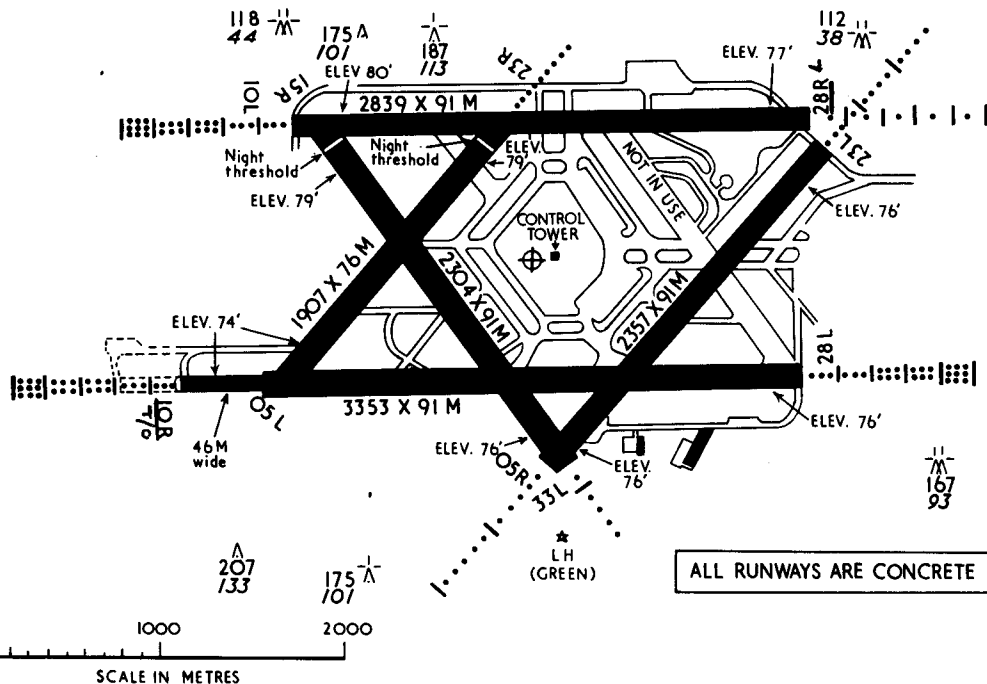
Reason for Re-issue:

SOUTHERN PIER & STANDS 5-9

APRON AND PARKING
POSITIONS
LONDON GATWICK

RUNWAY AVAILABILITY

- 05 L T/O at ATC discretion. Day only landing at ATC discretion.
T/O & landing by night 1761 M. (Last 147 M not available)
- 10 L Normally restricted to west T/O & east landing.
- 10 R Short T/O from BLOCK 79 2919 M.
Normal landing 3225 M (First 128 M not available)
- 15 R T/O & landing by night 2195 M (First 109 M not available)
- 23 R Landing at ATC discretion. T/O all conditions.
- 28 L Landing distance 3225 M (Last 128 M not available.)
- 33 L T/O & landing by night 2195 (Last 109 M not available)



STOPWAY: R/W 28L - 61 M

- NOTE. 1. CENTRE-LINE LIGHTING SHOULD BE STRADDLED (PAVED AREA OF AT LEAST 15 M.) EITHER SIDE OF LIGHTS
2. "DISTANCE TO GO" MARKERS IN THOUSANDS OF FEET R/W'S 10R/28L, 10L/28R, 10R/23L PAINTED ON R/W'S LEFT OF C/L'S

VISUAL APPROACH TO AVOID CONFUSION BETWEEN HEATHROW & NORTHOLT, THE LETTER 'LH' AND AN ARROW POINTING IN THE DIRECTION OF R/W 23L IS PAINTED IN WHITE ON THE NE. SIDE OF THE GASHOLDER SITUATED 045°(T) 3.3 N.M. FROM HEATHROW.

LONDON (HEATHROW) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
10L 28R	H.I. CODED CENTRE-LINE & 5 BARS L.I. RED CENTRE-LINE & 2 BARS	ALL 3°
10R	H.I. CODED CENTRE-LINE & 5 BARS L.I. RED CENTRE-LINE & 2 BARS	3°
15R	H.I. CODED CENTRE-LINE & 5 BARS L.I. RED CENTRE-LINE & 2 BARS	—
23L	H.I. CENTRE-LINE & 1 BAR L.I. RED CENTRE-LINE & 2 BARS	3-5°
33L	H.I. CENTRE-LINE & 1 BAR L.I. RED CENTRE-LINE & 1 BAR	—
05R	L.I. RED CENTRE-LINE & 2 BARS	3°
23R	SODIUM CENTRE-LINE	—
05L NIL		

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
10L/28R	H.I. & L.I. GREEN. H.I. WING BARS	H.I. BI-DIRECT. AT R/W EDGE 46 M. GAUGE LAST 609 M. YELLOW. H.I. BI-DIRECT. WHITE C/L. YELLOW TOUCH- DOWN ZONE 914 M. ELEVATED SNOW SODIUM LIGHTING (91 GAUGE)
10R	H.I. GREEN WITH H.I. WING BARS	
28L	L.I. GREEN WITH H.I. WING BARS	
05L 05R	L.I. GREEN	
23L	H.I. & L.I. GREEN	
23R	SODIUM WING BARS	H.I. BI-DIRECTIONAL EDGE (46 M. GAUGE)
15R 33L	L.I. GREEN	

OBSTRUCTION, IDENT BEACON 'LH' (GREEN)
APRON FLOODLIGHTS, BLUE ON EITHER SIDE OF APRON
FLOODLIGHTS, TAXIWAYS, BLUE ON EITHER SIDE OF E-W
TAXIWAY N OF R/W 10L/28R & IN MAINTENANCE AREAS;
GREEN C/L ON ALL OTHER TAXIWAYS & R/W'S USED FOR
TAXI-ING RUNUP AREAS MARKED BY ILLUMINATED SIGN BOARDS

ELEVATIONS (FT)

OFFICIAL AERODROME	80
LOWEST T/H (05L & 10R)	74
LAT 51° 28' N, LONG 00° 27'	
VARIATION 8° W	Bearings all magnetic
SERIAL No. 50384/2	DATE: 11. 6. 65.

Reason for Re-issue:

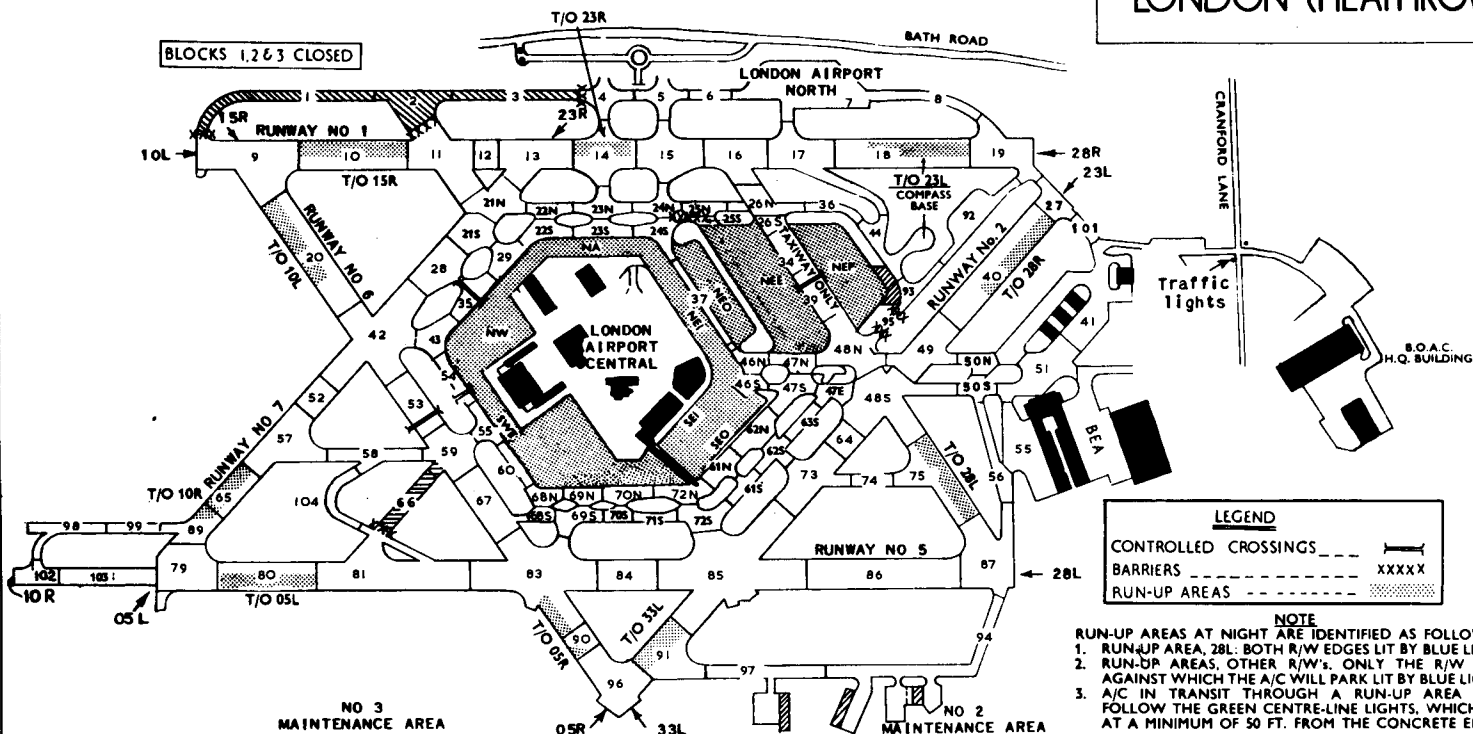
LIGHTING & R/W AVAILABILITY

AERODROME CHART LONDON (HEATHROW)

LOCATION OF TRAFFIC BLOCKS

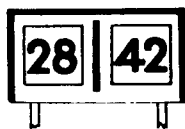
LONDON (HEATHROW)

BEA



LEGEND
 CONTROLLED CROSSINGS ---
 BARRIERS - - - - - XXXXX
 RUN-UP AREAS - - - - -

NOTE
 RUN-UP AREAS AT NIGHT ARE IDENTIFIED AS FOLLOWS:—
 1. RUN-UP AREA, 28L: BOTH R/W EDGES LIT BY BLUE LIGHTS
 2. RUN-UP AREAS, OTHER R/W's. ONLY THE R/W EDGE AGAINST WHICH THE A/C WILL PARK LIT BY BLUE LIGHTS.
 3. A/C IN TRANSIT THROUGH A RUN-UP AREA MUST FOLLOW THE GREEN CENTRE-LINE LIGHTS, WHICH ARE AT A MINIMUM OF 50 FT. FROM THE CONCRETE EDGES.



POSITION INDICATOR BOARDS

1. INDICATOR BOARDS ARE POSITIONED ON PORT SIDE AT TRAFFIC STOP-BARS, AND INDICATE THE TWO TRAFFIC BLOCKS WHICH THE STOP-BAR DIVIDES.
2. WHEN REQUESTED TO 'CHECK POSITION', REPORT AS A FOUR FIGURE GROUP, e.g. 2842, WHICH INDICATES THAT A/C IS IN BLOCK 28, PROCEEDING TO BLOCK 42.

RED TRAFFIC STOP-BARS

TRAFFIC STOPBARS CONSIST OF TRANSVERSE LINES OF RED LIGHTS ACROSS RUNWAYS OR TAXIWAYS. THEY MARK THE DIVISION BETWEEN TRAFFIC BLOCKS AND BETWEEN PARALLEL TAXIWAYS. STOP-BARS ON PURE TAXIWAYS EXTEND THE FULL WIDTH OF THE TAXIWAYS AND WHERE RUNWAYS ARE USED AS TAXIWAYS, EXTEND FROM THE CONCRETE EDGES TO THE RUNWAY LIGHTS, EXCEPT WHERE STOP-BARS GUARD OTHER RUNWAYS WHERE THEY EXTEND THE FULL WIDTH. RED STOP-BARS ARE NOT PROVIDED BETWEEN BLOCKS 21 N & S, 22 N & S, 23 N & S, 24 N & S, 25 N & S, 26 N & S, AND AIRCRAFT ARE ON NO ACCOUNT TO CROSS OVER AN UNLIT AREA FROM THE GREEN ROUTES ILLUMINATED FOR THEM TO ANOTHER ROUTE WHICH CAN BE SEEN.

DAYLIGHT ROUTE INDICATOR BOARDS

THESE ARE PLACED ON THE LEFT SIDE OF TAXIWAYS AND CONSIST OF WHITE DIAGRAMS ON BLACK BACKGROUND AND PORTRAY THE TAXIWAY JUNCTION AHEAD. TOGETHER WITH A WHITE LIGHT WHICH INDICATES THE ROUTE TO BE FOLLOWED.

SERIAL No. 50106/2 DATE: 2.7.65

Reason for Re-Issue:
 BLOCK 82 WITHDRAWN & BLOCKS 1,2,3 CLOSED.

NOTE

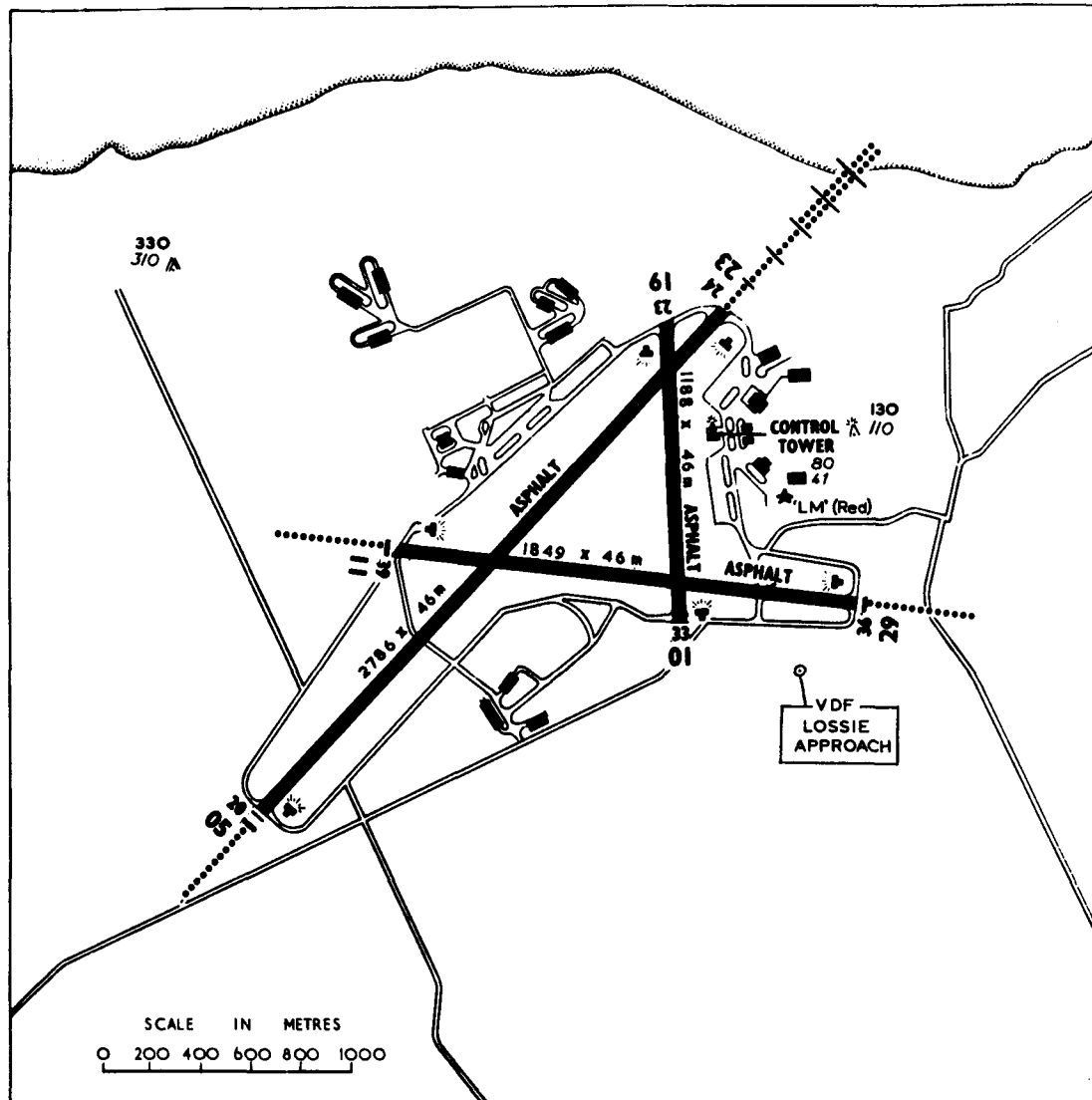
FOR DETAILS OF PARKING POSITION AT CENTRAL AREA, NORTH EAST PARK & NORTH SIDE APRON SEE SEPARATE PAGE.

KEY TO AIRCRAFT PARKING AREAS

- NA — NORTH APEX APRON
- NEE — NORTH EAST EXTENSION APRON
- NEI — NORTH EAST INNER APRON
- NEO — NORTH EAST OUTER APRON
- NEP — NORTH EAST APRON PARK
- NW — NORTH WEST APRON
- SEI — SOUTH EAST INNER APRON
- SEO — SOUTH EAST OUTER APRON
- SW — SOUTH WEST APRON
- SWE — SOUTH WEST EXTENSION APRON

GROUND MOVEMENT CONTROL

LONDON (HEATHROW)



LOSSIEMOUTH AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
23	H.I. WHITE CENTRE-LINE & 5 BARS	
05 11/29	L.I. RED CENTRE-LINE & BAR	
01/19	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
23	GREEN	HIGH INT. UNI-DIRECTIONAL
05 11/29	GREEN	L.I. CONTACT LIGHTS
01/19	GREEN	L.I. CONTACT LIGHTS

OBSTRUCTION, TAXIWAY, IDENT BEACON 'LM' (RED) AMBER CAUTION BARS, RED STOP LIGHTS.

ELEVATIONS (FT) a.m.s.l. 130
above lowest T/H 110

OFFICIAL AERODROME 39

LOWEST THRESHOLD (05) 20

LAT 57° 43' N, LONG 03° 20' W

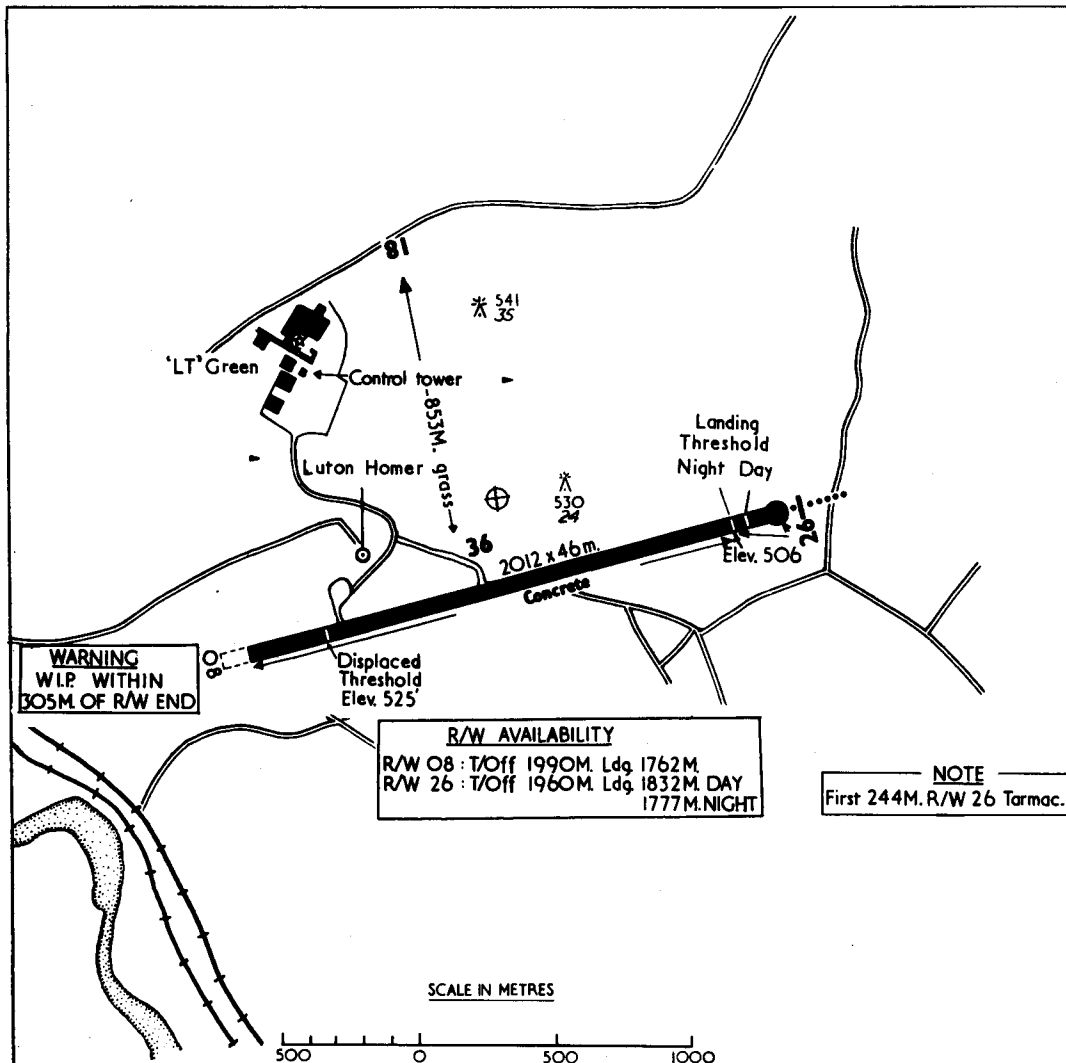
VARIATION 10° W Bearings all magnetic

SERIAL No. 48138 DATE: 5.8.64

Reason for Re-issue:

WARNING
R/W 01/19 RESTRICTED TO PISTON ENGINED AIRCRAFT IN EMERGENCY.

**AERODROME CHART
LOSSIEMOUTH**



LUTON AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI Type Angle
08	NIL	3°
26	H.I. & L.I. CENTRE-LINE & BAR.	3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08	H.I. GREEN WING BARS (92M. ALONG R/W)	H.I. WHITE ELEVATED 9"
26	H.I. GREEN WING BARS (213M. ALONG R/W)	

OBSTRUCTION, TAXIWAY, IDENT. BEACON
LT' (GREEN) WIND INDICATOR.

ELEVATIONS (FT) : —

OFFICIAL AERODROME	525
LOWEST THRESHOLD (26)	506
LAT 51° 52' N, LONG 00° 22' W	
VARIATION 8° W	Bearings all magnetic
SERIAL No. 50449/2	DATE : 14.7.65

Reason for Re-issue:

R/W LENGTH & OBSTRUCTIONS.

AERODROME CHART LUTON

NOTE. TAXIWAY LIGHTS ARE GREEN C/L. BLUE EDGE LIGHTS ON HOLDING AREA.

NOISE ABATEMENT: R/W 26:— UNLESS OTHERWISE INSTRUCTED, ALL A/C TAKING-OFF R/W 26 SHOULD TURN LEFT THROUGH 20° AS SOON AS POSSIBLE AFTER PASSING END OF R/W, AND CLIMB ON THIS HEADING UNTIL CROSSING THE M.I. MOTORWAY.
STOPWAY : R/W 26 92M

SECT. SAFE ALT
25 N.M. 2000'

00° 30' W

20'

10'

SECT. SAFE ALT
25 N.M. 2000'

LUTON
VDF R/W 26

BRITISH EUROPEAN AIRWAYS

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APPROACH LUTON APP.

130-35

TOWER LUTON TWR.

120-2

TRANS ALT

4000

FT.

G/S (KTS)	70	90	110	130	150
VDF TO TURN	4-17	3-20	2-44	2-19	2-0

G/S	170	190
VDF TO TURN	1-46	1-35

OVERSHOOT

CLIMB AHEAD TO 1506 / 1000 RIGHT TURN ONTO
060° CLIMBING TO 2000 / 1494

ELEVATIONS (FT) : —

OFFICIAL AERODROME 525

LOWEST THRESHOLD 506

THRESHOLD R/W 26 506

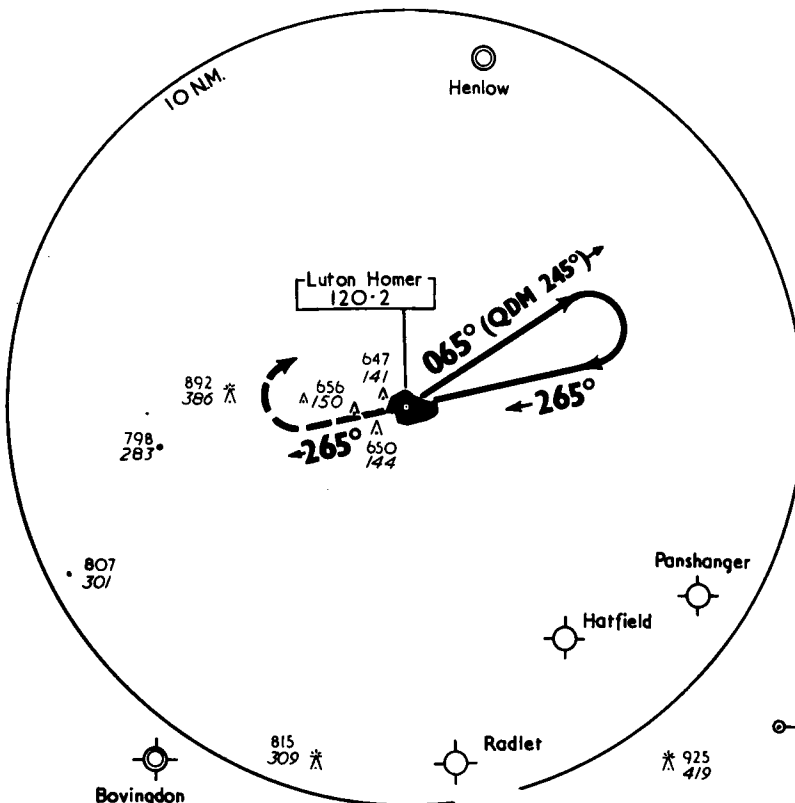
VARIATION 8° W Bearings all magnetic

SERIAL No. 50128/3 DATE: 23-6-65

Reason for Re-issue:

OBSTRUCTIONS

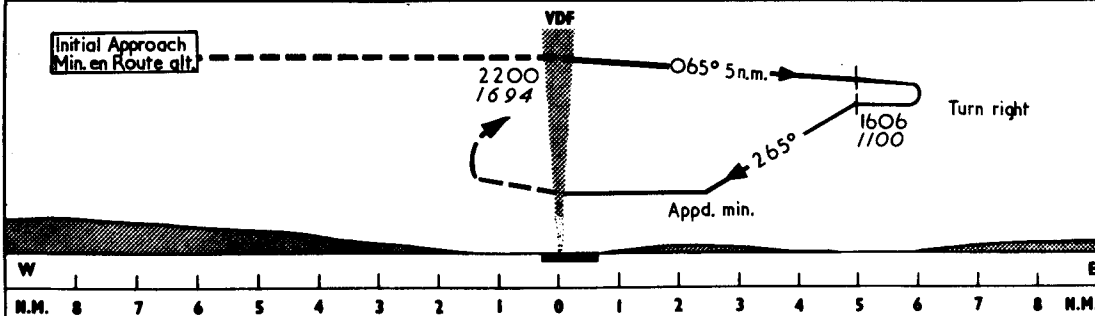
VDF R/W 26
LUTON



SECT. SAFE ALT
25 N.M. 2200'

SECT. SAFE ALT
25 N.M. 2000'

Initial Approach
Min. en Route alt.



R/W Availability
R/W 25: Landing 2195 M.

VDF
Lyneham App.

'LA' ★
Red

NDB
'LA'

2377 X 46 M. Tarmac

Control tower

Landing threshold

Terminal Building

1829 X 46 M. Tarmac

440
36

SCALE IN METRES
0 200 400 600 800 1000

GROUND RISES STEEPLY FROM 340 FT. BELOW A/D LEVEL TO R/W 25.
CAUTION: APPROACHES FOR HULLAVINGTON CROSS APPROACH TO R/W 15.

LYNEHAM AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
25	H.I. WHITE CENTRE-LINE & BARS	2.5
17	RED BAR	2.5
18/36	RED 'T' WITH SODIUMS	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
25 07	GREEN	OMNI & UNI-DIRECTIONAL
18/36	GREEN	OMNI-DIRECTIONAL

OBSTRUCTION, TAXIWAY, IDENT BEACON 'LA'

ELEVATIONS (FT)

OFFICIAL AERODROME 513

LOWEST THRESHOLD (36) 440

LAT 51°30' N, LONG 01°59' W

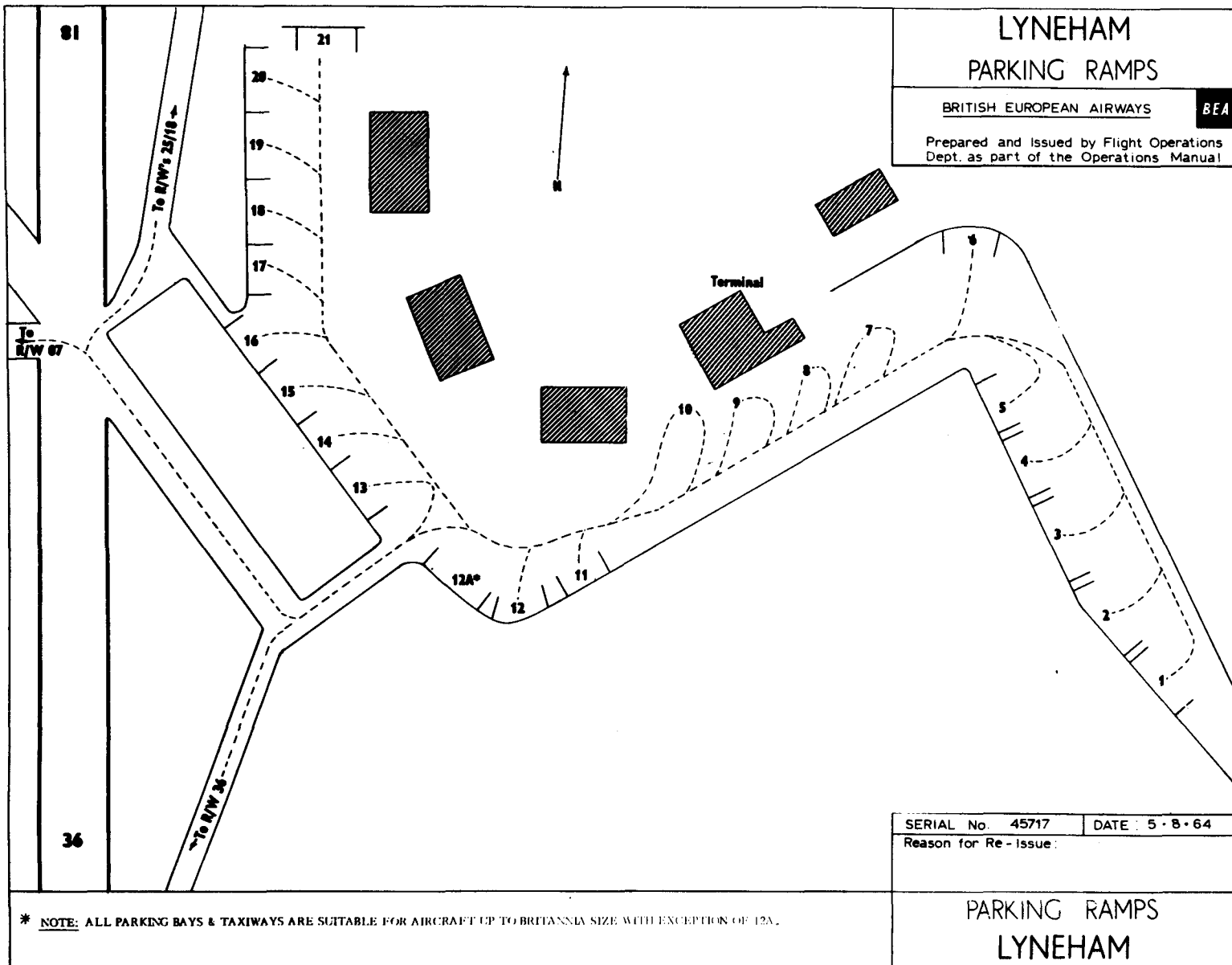
VARIATION *W | Bearings all magnetic

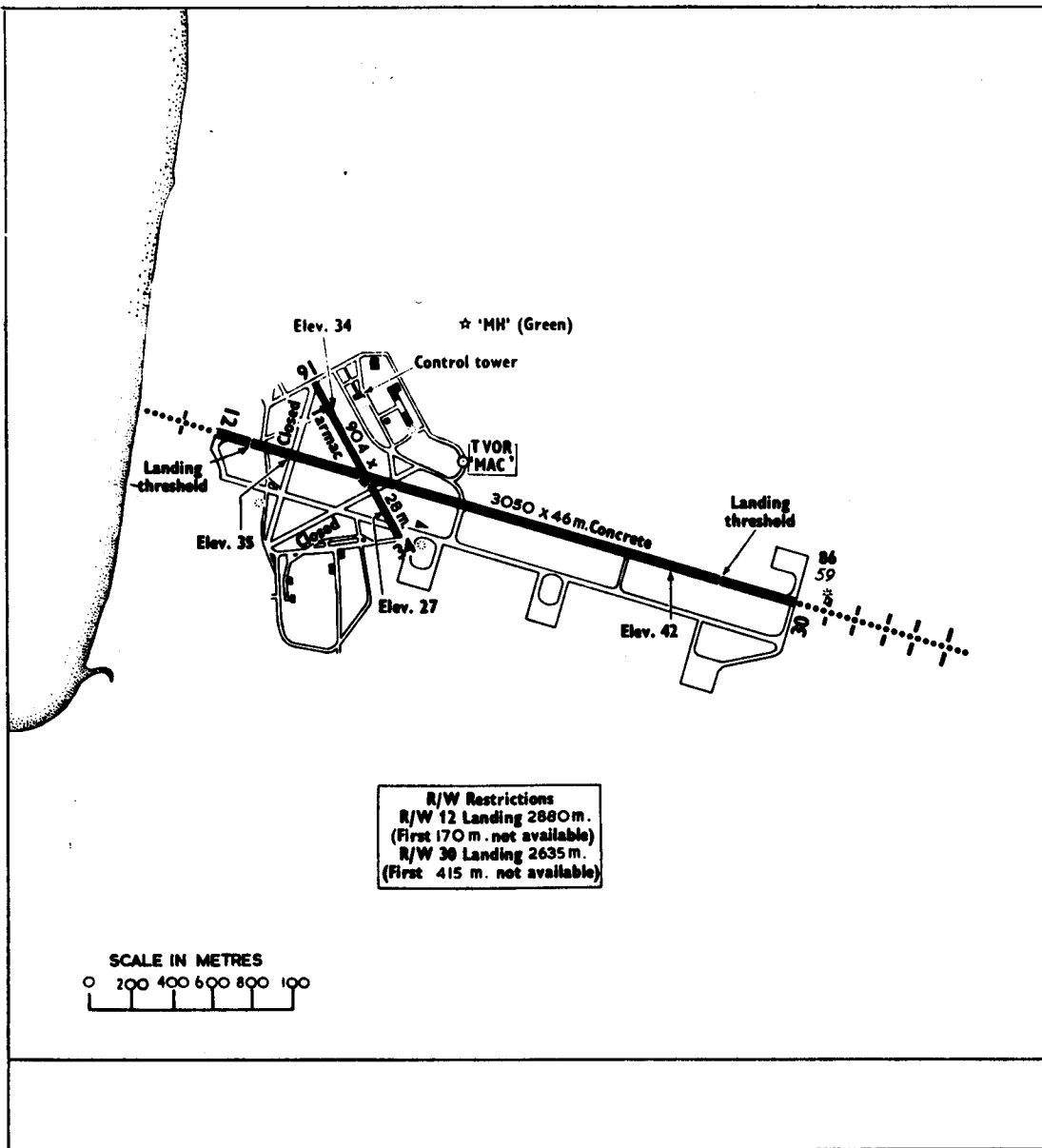
SERIAL No. 40394/1 | DATE 18 6 65

Reason for Re-Issue:

VASI G/P ANGLE

AERODROME CHART LYNEHAM





MACHRIHANISH AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH
12	H I WHITE C/LINE & BAR L I AMBER C/LINE & BAR
30	H I WHITE C/LINE & BAR L I AMBER C/LINE (1200') & BAR
16	NIL
34	NIL

N.B. NO ENTRY UNDER VASI: ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
12	GOOSENECK WINGBARS	ELEVATED HIGH INTENSITY UNI-DIRECTIONAL
30	GREEN WING BARS	LOW INTENSITY OMNI- DIRECTIONAL
16	SODIUM WINGBARS	NIL
34	NIL	NIL

OBSTRUCTION, TAXIWAY, IDENT. BEACON,
'MH' (GREEN).

ELEVATIONS (FT)	a.m.s.l.	86
	above lowest T/H	59
OFFICIAL AERODROME		35
LOWEST THRESHOLD (34)		27
LAT	55° 26' N,	LONG 05° 42' W
VARIATION	11° W	Bearings all magnetic
SERIAL No.	46289	DATE: 28.7.64
Reason for Re-issue:		

AERODROME CHART
MACHRIHANISH

SECT. SAFE ALT.
25 N.M. 3600'

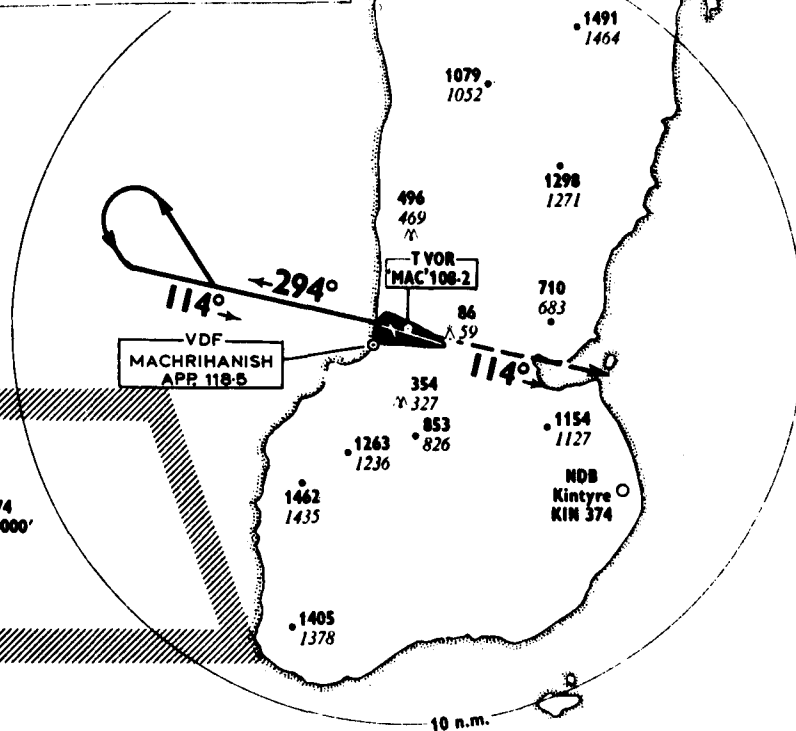
50'

40'

05°30'W

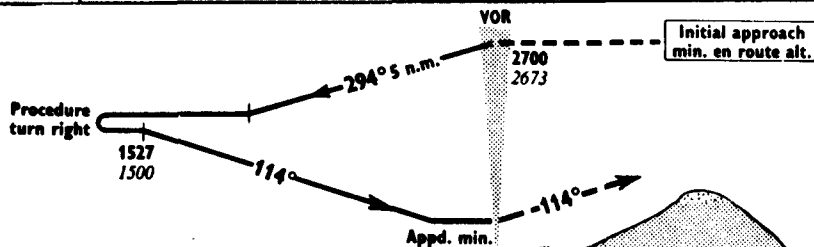
SECT. SAFE ALT.
25 N.M. 3900'

Note
Delay may occur in change-over from main to standby VOR transmitters. Pilots must monitor ident. signal throughout approach



SECT. SAFE ALT.
25 N.M. 2700'

SECT. SAFE ALT.
25 N.M. 2700'



MACHRIHANISH VOR R/W12

BRITISH EUROPEAN AIRWAYS

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APPROACH MACHRIHANISH APP

118-5 130-7

TOWER MACHRIHANISH TWR.

118-5

TRANS ALT 3000 FT.

G/S (KTS) 70 90 110 130 150

VOR TO TURN 4m17s 3m20s 2m44s 2m19s 2m05s
m s m s m s m s m s

DESCENT ON G/P (FT/MIN)

G/S 170 190 210 230 250

VOR TO TURN 1m46s 1m35s 1m26s 1m18s 1m12s
m s m s m s m s m s

DESCENT ON G/P (FT/MIN)

CRITICAL HEIGHTS (FT)

TRIDENT AUTO MANUAL

OTHER TYPES WITH G/P NO G/P 650

OVERSHOOT

CLIMB AHEAD ON 114° TO 2700 2673 & RETURN TO VOR.

ELEVATIONS (FT) a.m.s.l. 2700
above lowest T/H 2673

OFFICIAL AERODROME 35

LOWEST THRESHOLD 27

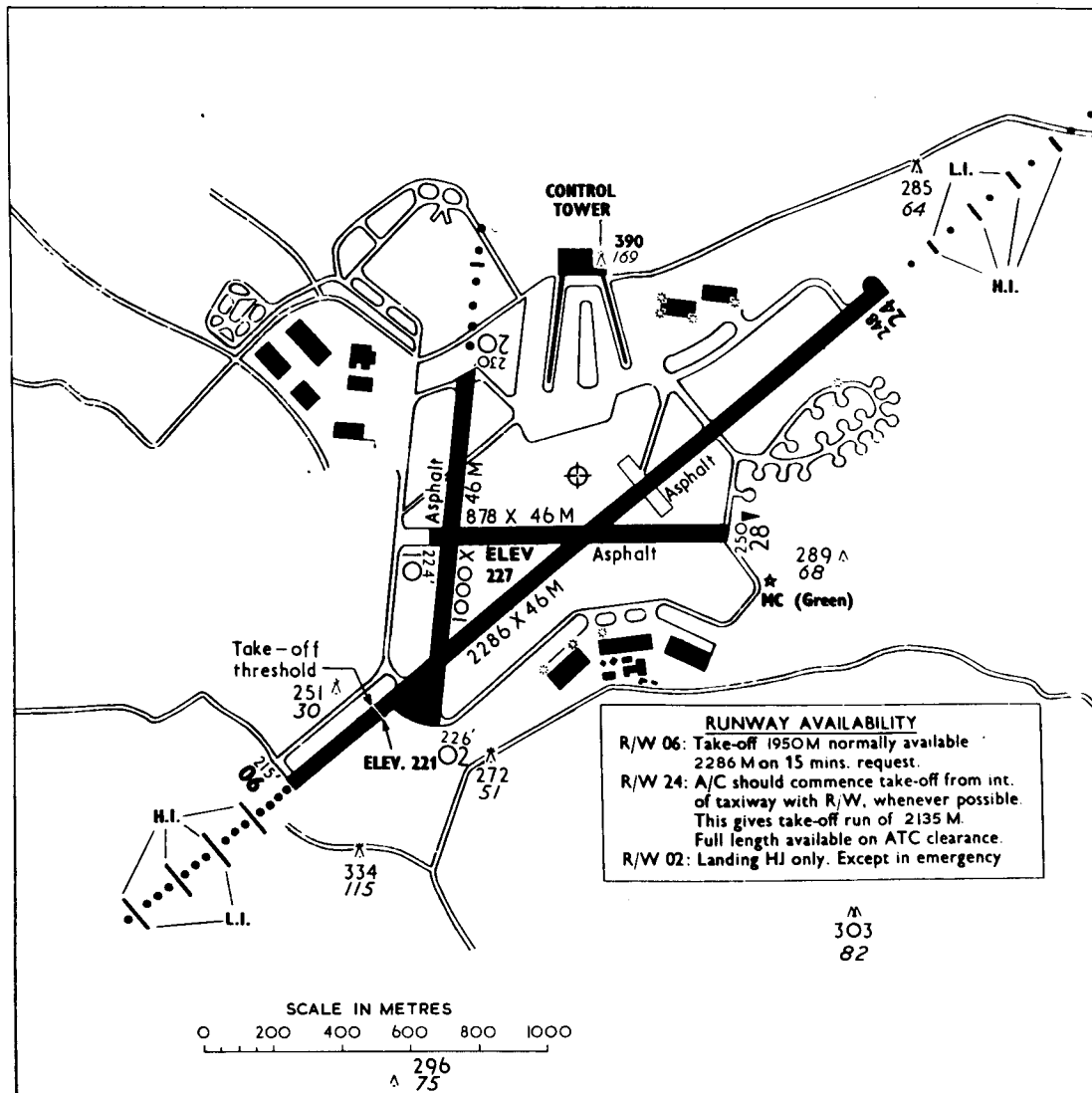
THRESHOLD R/W 12 35

VARIATION 11°W Bearings all magnetic

SERIAL No. 48358 **DATE** 5-8-64

Reason for Re-issue:

VOR R/W 12
MACHRIHANISH



MANCHESTER AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06	610M. L.I. RED CENTRE-LINE WITH 2 BARS. 610M. H.I. CENTRE-LINE WITH 4 BARS.	3°
24	H.I. WHITE CENTRE-LINE & 4 BARS. L.I. RED CENTRE-LINE & 2 BARS.	3°
02	NIL	
20	L.I. RED CENTRE-LINE & BAR.	
10 28	NIL	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06	H.I. & L.I. GREEN H.I. & L.I. GREEN ELEVATED WINGBARS	ELEVATED H.I. & L.I.
24	H.I. & L.I. GREEN H.I. & L.I. GREEN ELEVATED WINGBARS	
02 20	L.I. GREEN	L.I. ELEVATED.

OBSTRUCTION, TAXIWAY, IDENT BEACON 'MC'

ELEVATIONS (FT)

OFFICIAL AERODROME	256
LOWEST THRESHOLD (06)	221
LAT 53° 21' N, LONG 02° 16' W	
VARIATION 9° W	Bearings all magnetic
SERIAL No 50042/1	DATE: 22.7.65.
Reason for Re-issue:	

CAUTION NOTE

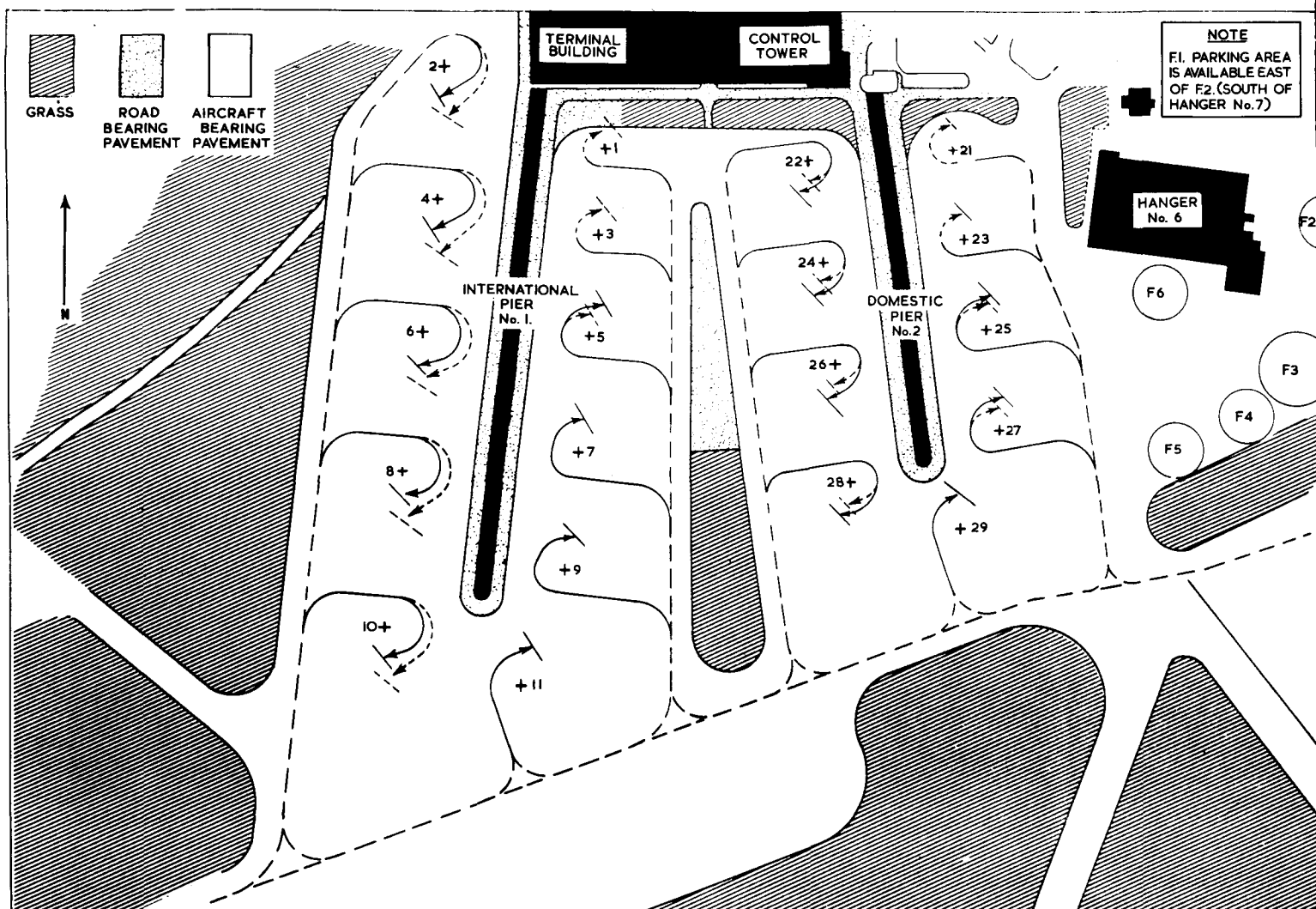
AERODROME CHART MANCHESTER

CAUTION: R/W 06/24 SLIPPERY WHEN WHEN.

STOPWAYS: R/W 06 - 120M. R/W 24 - 60M.

SPECIAL INFORMATION: CIVIL MARSHALLERS' INSTRUCTIONS SHOULD BE ADHERED TO DUE TO RESTRICTIONS
ON THE USE OF THE MILITARY SECTION OF THE APRON.

NOTE: DISTANCE TO GO MARKERS IN THOUSANDS OF FEET R/W 06/24



SEE EXPLANATORY NOTES ON FACING PAGE

BRITISH EUROPEAN AIRWAYS

BEA

SERIAL No 47184

DATE 5-8-64

Reason for Re-issue:

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PARKING AREAS
MANCHESTER

MANSTON AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

Prepared and Issued by Flight Operations
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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
11 & 29	WHITE HIGH INT. UNI-DIRECTIONAL CENTRE-LINE & BARS, LOW INT. RED TEE; SODIUM FLARE PATH.	3° 3°

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
11 & 29	GREEN	HIGH INT. & LOW INT. WHITE BI-DIRECTIONAL.

OBSTRUCTION, TAXIWAY, IDENT. BEACON 'MN'(RED)

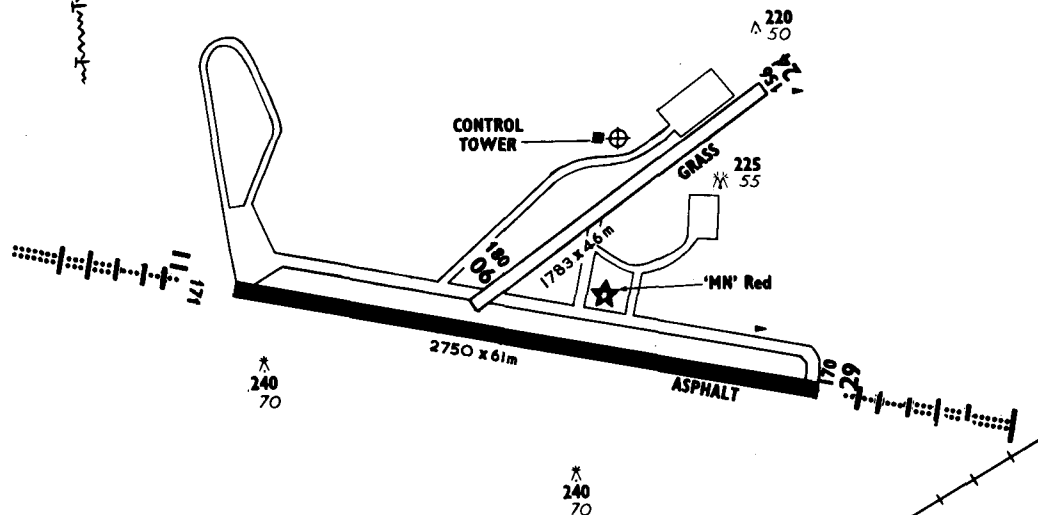
ELEVATIONS (FT) :—

OFFICIAL AERODROME	180
LOWEST THRESHOLD (29)	170
LAT 51° 21' N, LONG 01° 21' E	
VARIATION 7° W	Bearings all magnetic
SERIAL No. 50051/1	DATE: 22.1.65

Reason for Re-Issue:

TAXIWAYS & APRONS.

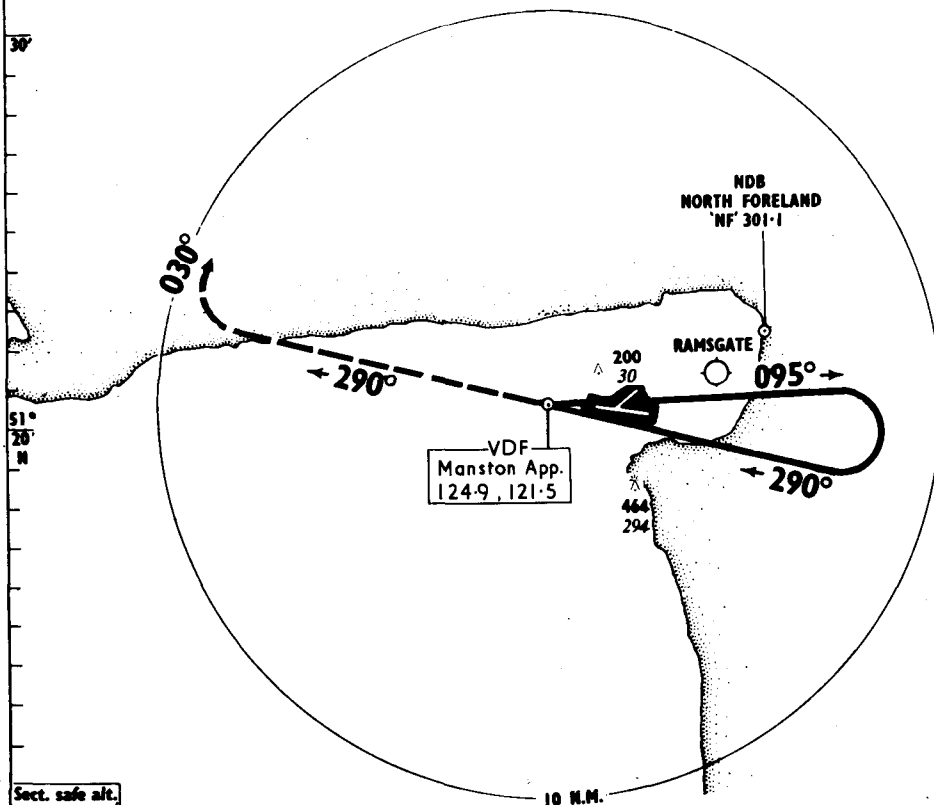
AERODROME CHART MANSTON



NOTE: FOAM CARPET AVAILABLE IN EMERGENCY. (45 MINS. NOTICE REQUIRED.)

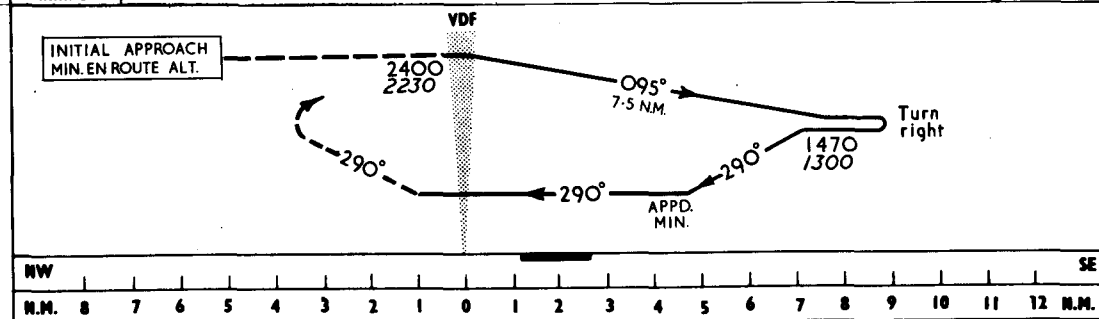
Sect. safe alt.
25 n.m. 1700

Sect. safe alt.
25 n.m. 1500



Sect. safe alt.
25 n.m. 2200

Sect. safe alt.
25 n.m. 2200



MANSTON

VDF R/W 29

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH MANSTON APP.

124.9 121.5

TOWER MANSTON TWR.

124.9 121.5

TRANS ALT 4500 FT.

G/S (KTS)	70	90	110	130	150
VDF TO TURN	6.26	5.0	4.06	3.28	3.0

G/S	170	190	210	230	250
VDF TO TURN	2.39	2.22			

CRITICAL HEIGHT (FT)

700

OVERSHOOT

CLIMB AHEAD TO 3000 2830 WITHIN 10 N.M.
OF A/D. THEN TURN RIGHT ONTO 030°, THEN
AS DIRECTED.

ELEVATIONS (FT) :—

OFFICIAL AERODROME 180

LOWEST THRESHOLD 170

THRESHOLD R/W 29 170

VARIATION 7°W Bearings all magnetic

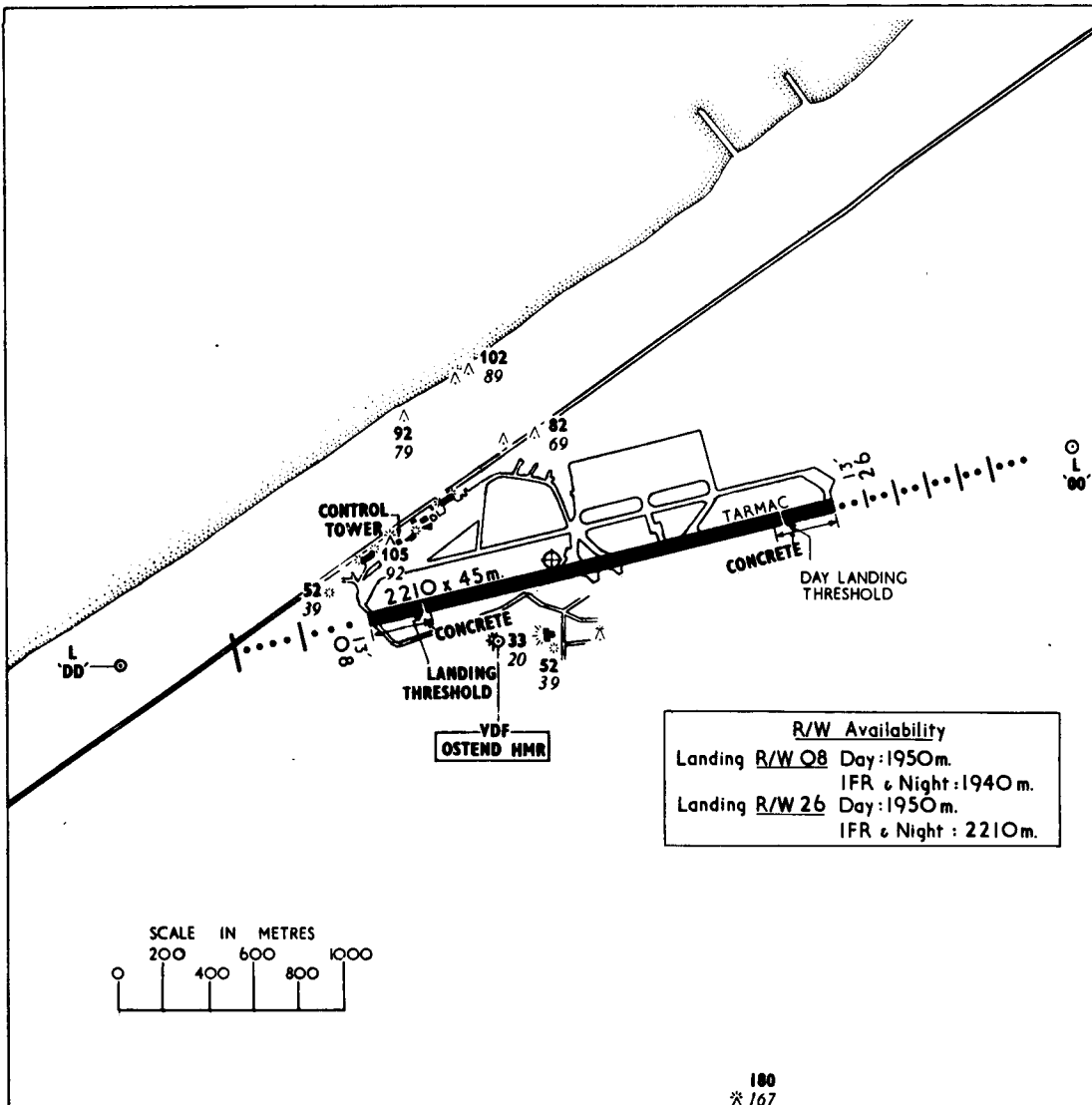
SERIAL No. 40284/1 DATE: 29.12.64

Reason for Re-issue:

TRANS. ALT. REVISED

VDF R/W 29

MANSTON



R/W Availability
 Landing **R/W 08** Day: 1950m.
 IFR & Night: 1940m.
 Landing **R/W 26** Day: 1950m.
 IFR & Night: 2210m.

OSTEND AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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 Dept. as part of the Operations Manual

LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
26	H.I. WHITE CENTRE-LINE 900 M. & 5 BARS.	
08	H.I. WHITE CENTRE-LINE 500 M. & 2 BARS.	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

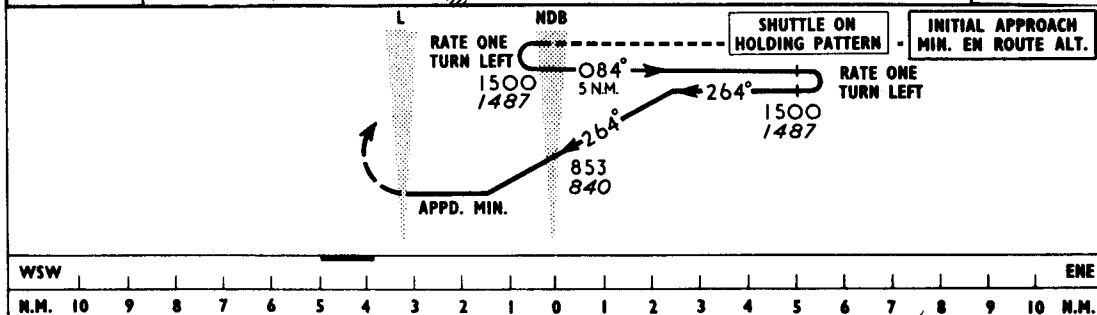
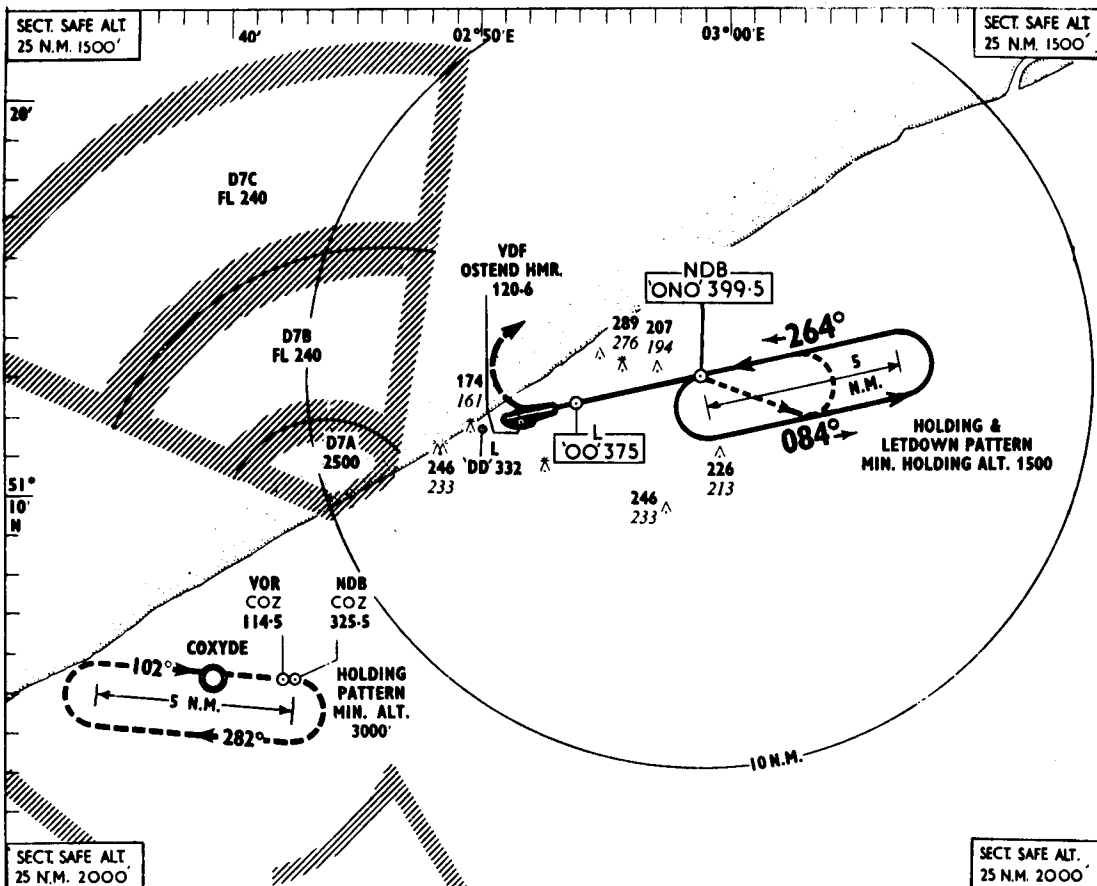
R/W	THRESHOLD	RUNWAY
26	GREEN	WHITE UNI-DIRECTIONAL
08	GREEN WING 270 M. DOWN R/W.	

OBSTRUCTION, TAXIWAY,
 LANDING INDICATOR.

ELEVATIONS (FT) :-

OFFICIAL AERODROME	13
LOWEST THRESHOLD (08 & 26)	13
LAT 51° 12' N, LONG 02° 52' E.	
VARIATION 6° W	Bearings all magnetic
SERIAL No. 40199/1	DATE: 27.1.65
Reason for Re-Issue:	
LIGHTING	

AERODROME CHART OSTEND



OSTEND NDB R/W 26

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APPROACH OSTEND APP.

120.6

TOWER OSTEND TWR.

118.7 119.7 121.5E

TRANS ALT 4500 FT.

G/S (KTS)	70	90	110	130	150
NDB TO TURN	4.17	3.20	2.44	2.19	2.00
NDB TO T/H	3.15	2.32	2.04	1.45	1.31

G/S	170	190			
NDB TO TURN	1.46	1.35			
NDB TO T/H	1.20	1.12			

CRITICAL HEIGHT (FT)

500

OVERSHOOT

TURN RIGHT ON TO 045° CLIMB TO
1000 987 CONTACT ATC.

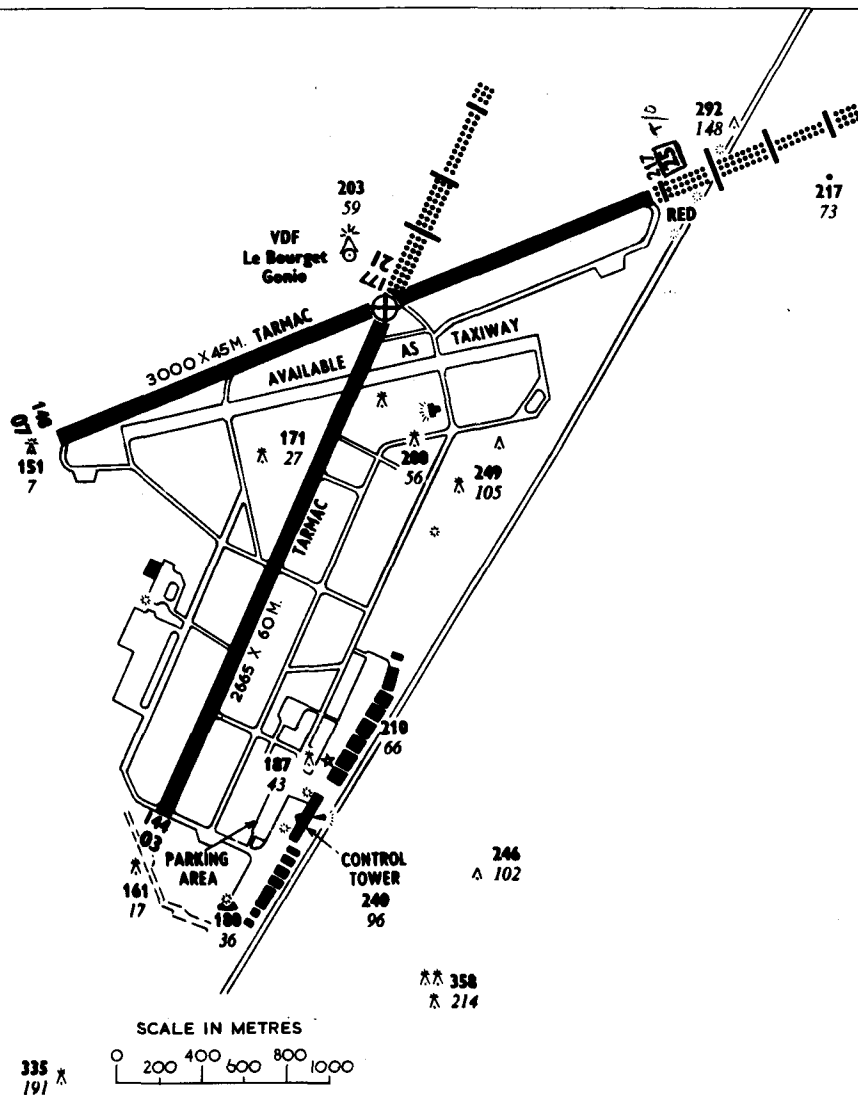
ELEVATIONS (FT) :—

OFFICIAL AERODROME	13
LOWEST THRESHOLD	13
THRESHOLD R/W 26	13
VARIATION 6°W	Bearings all magnetic
SERIAL No. 40244/1	DATE: 27.1.65

Reason for Re-issue:

CRIT. HT.

NDB R/W 26
OSTEND



△
289
145

PARIS (LE BOURGET) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAC
Type
Angle

R/W	APPROACH
03 07	NIL
25	H.I. WHITE CENTRE-LINE & BARS L.I. YELLOW CENTRE-LINE & BAR H.I. WHITE FLASHING CENTRE-LINE ON OUTERMOST 700M.
21	H.I. WHITE CENTRE-LINE & BARS H.I. WHITE FLASHING CENTRE-LINE ON OUTERMOST 730M.

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
03	L.I. Green with uni-dir- ectional wingbars 2 L.I. red bars before threshold	L.I. omni-directional white with wingbar at 305m up R/W
07	L.I. Green with uni-dir- ectional wingbars 2 L.I. red bars before threshold	L.I. omni-directional white
25	H.I. Green L.I. green with uni-directional wing- bars. 2 H.I. & L.I. red bars before threshold	H.I. uni-directional & L.I. omni-directional white H.I. & L.I. white wingbars 305m. up R/W H.I. white touch- down zone 915m.
21	H.I. & L.I. green with L.I. green wingbars 2 H.I. sodium bars with L.I. red bars before threshold	H.I. uni-directional white with wingbars at 305m. up R/W. L.I. omni-directional white with wingbars at 305m up R/W.

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND & LANDING INDICATOR.

ELEVATIONS (FT) :—

OFFICIAL AERODROME 217

LOWEST THRESHOLD (03) 144

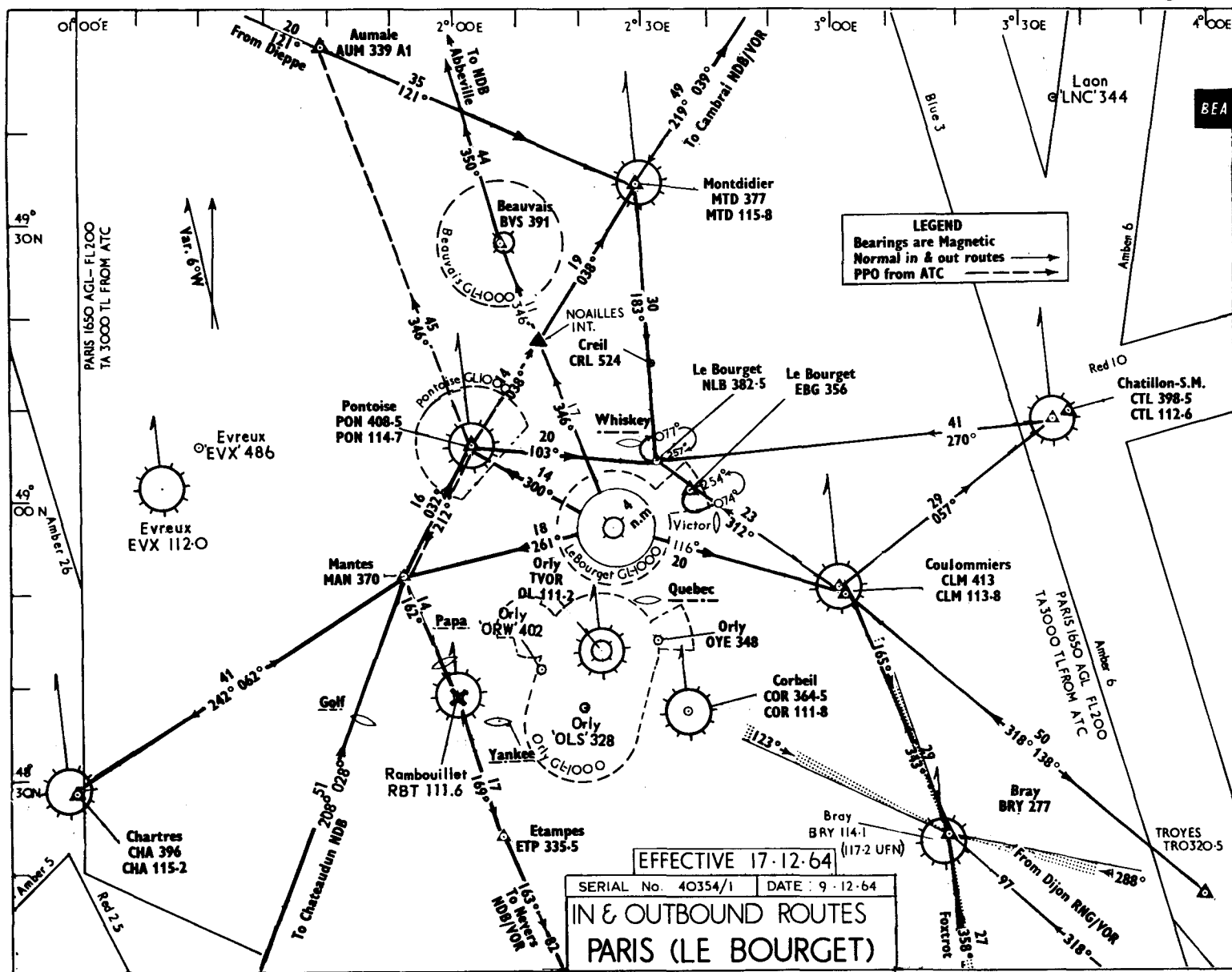
LAT 48° 58' N, LONG 02° 27' E

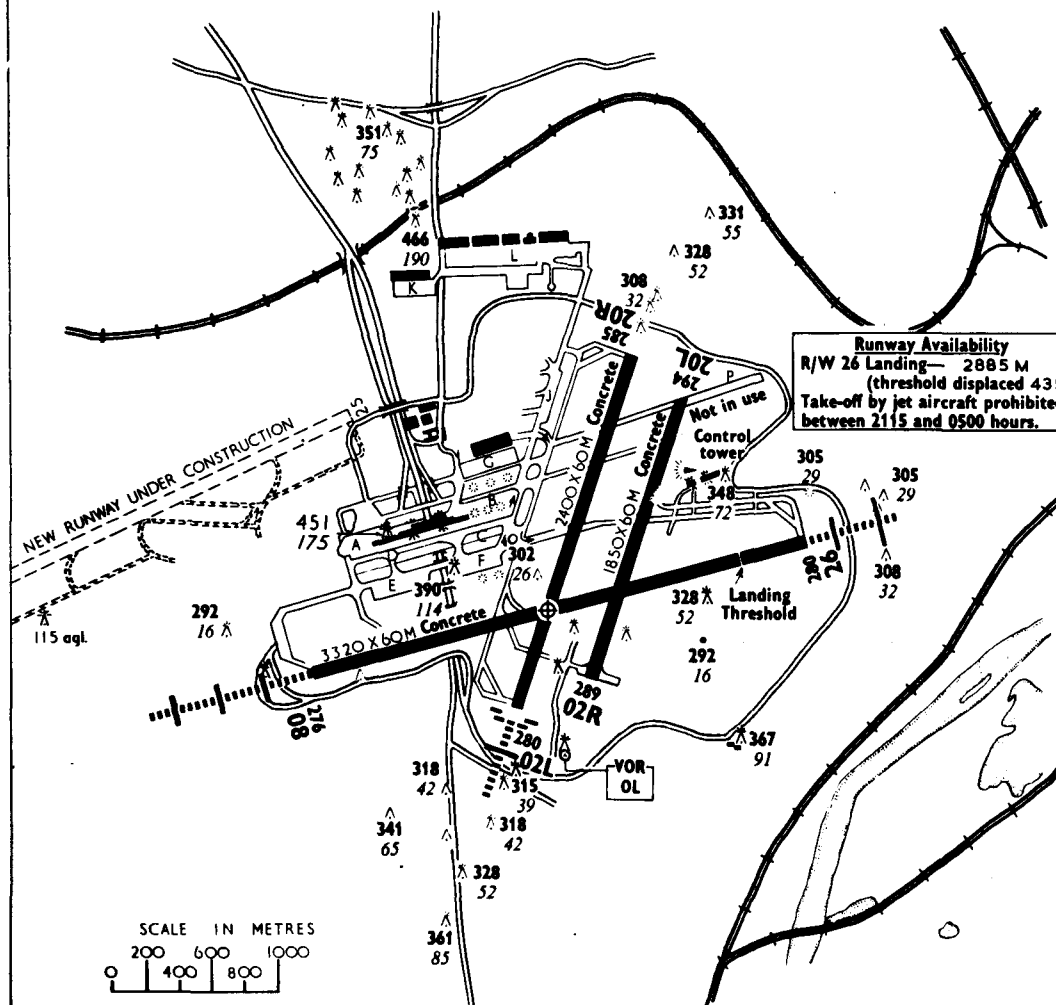
VARIATION 6° W Bearings all magnetic

SERIAL No. 32074 DATE : 5-8-64

Reason for Re-issue:

AERODROME CHART
PARIS (LE BOURGET)





NOTE: GREEN STRIPES ON BRIDGE OVER ROAD CROSSING R/W 08/26 900M. FROM THRESHOLD R/W 08.
DO NOT CONFUSE WITH TAXIWAYS.

* FRENCH "EQUAL BRIGHTNESS" VASI

PARIS (ORLY) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
08	L.I. YELLOW CENTRE-LINE & BARS WITH 2 H.I. & L.I. RED BARS BEFORE THRESHOLD	
26	H.I. WHITE CENTRE-LINE & BARS WITH 2 H.I. & L.I. RED ROWS & BARS EITHER SIDE OF R/W	*
02L	H.I. & L.I. WHITE CENTRE-LINE & BAR WITH 2 H.I. & L.I. RED BARS BEFORE THRESHOLD	
02R	2 L.I. RED BARS BEFORE THRESHOLD	
20R	2 L.I. RED BARS BEFORE THRESHOLD	
20L	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08	H.I. & L.I. green with wingbars	H.I. & L.I. white with wingbars 300m. up R/W. Touchdown zone 900m.
26	H.I. & L.I. green wing bars at displaced threshold	H.I. & L.I. white with wingbars 300m. up R/W from displaced threshold
02L	H.I. & L.I. green with wingbars	H.I. & L.I. white with wingbars 300m. up R/W
02R	L.I. green	L.I. white with wingbars 300m. up R/W
20R	L.I. green with wing- bars	L.I. white with wingbars 300m. up R/W
20L	L.I. green wingbars	L.I. white with wingbars 300m. up R/W

OBSTRUCTION, TAXIWAY, HIGH SPEED TURN OFF
LIGHTS, 6 FLASH ARROWS BEGINNING OF R/W
26 & DISPLACED THRESHOLD. FRENCH
TYPE VASI.

ELEVATIONS (FT) :—

OFFICIAL AERODROME 292

LOWEST THRESHOLD (08) 276

LAT 48° 43' N, LONG 02° 23'

VARIATION 6° W Bearings all magnetic

SERIAL No. 50193/2 DATE: 20.7.65

Reason for Re-issue:

OBSTRUCTION & VASI

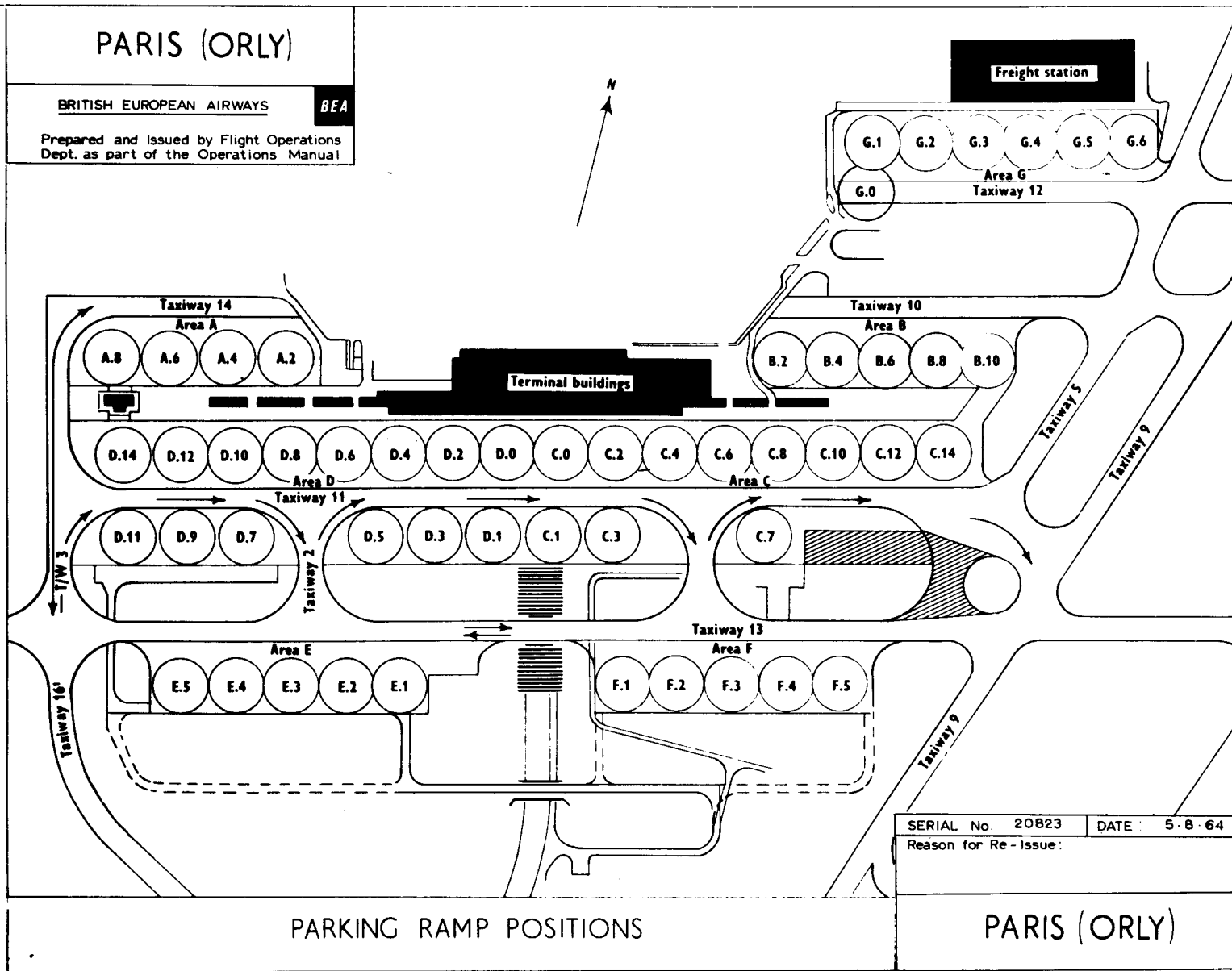
AERODROME CHART PARIS (ORLY)

PARIS (ORLY)

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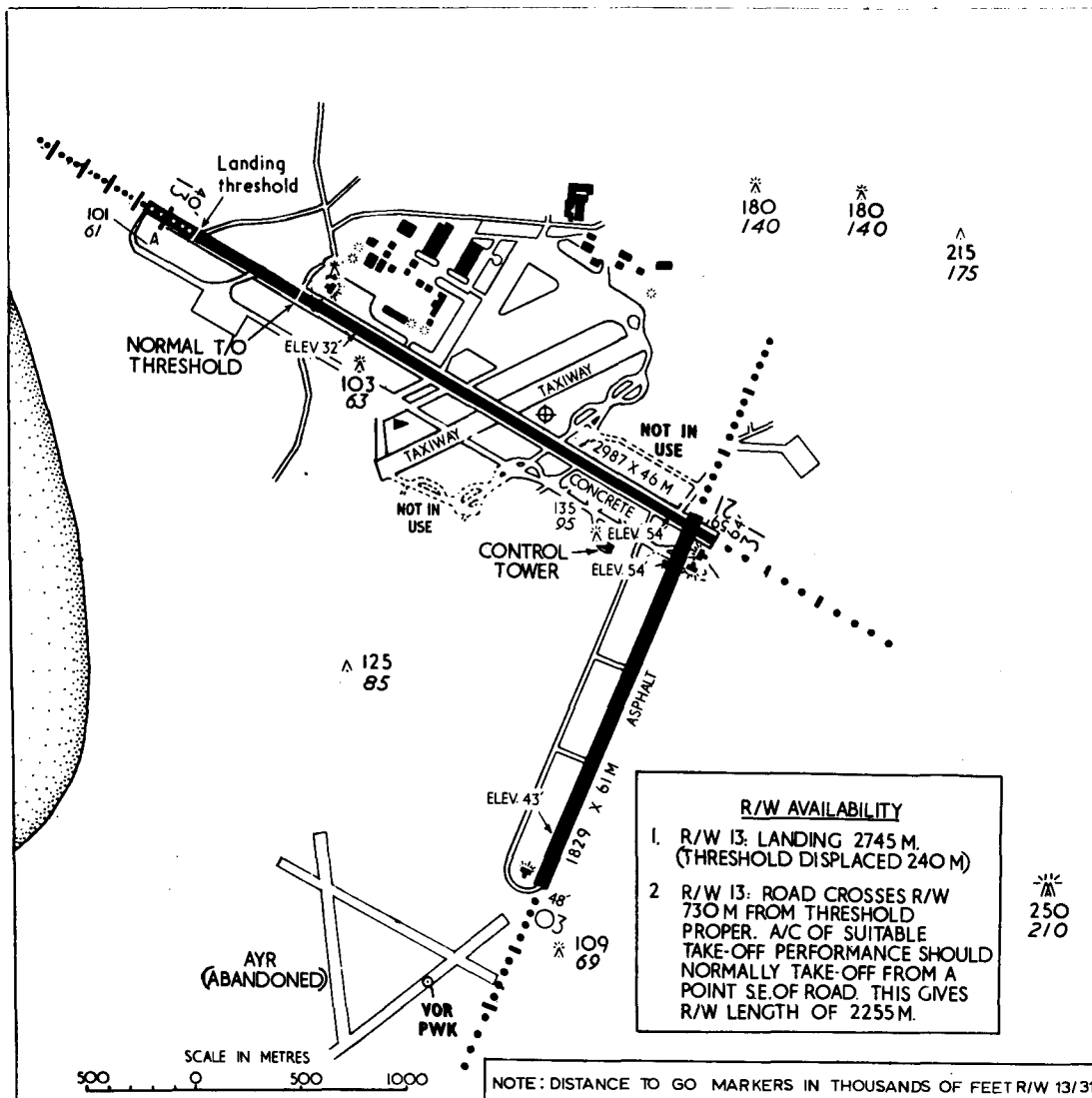
PARKING RAMP POSITIONS

SERIAL No 20823

DATE 5-8-64

Reason for Re-issue:

PARIS (ORLY)



PRESTWICK AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
13	H.I. WHITE C/L & 5 BARS WITH SODIUM ON 4 OUTER BARS	3°
31	H.I. WHITE C/L & 2 BARS L.I. RED C/L & 2 BARS	3-5°
03/21	RED LI CENTRE-LINE & BARS	3-5° (R/W 21)

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
13	GREEN UNI-DIREC- TIONAL WITH H.I. BI-DIRECTIONAL WINGS	WHITE H.I. BI-DIREC- TIONAL ELEVATED L.I. RED ON FIRST 245 M R/W 13 TOUCHDOWN ZONE OF STUB BARS FOR FIRST 920 M AT EACH END
03/21	LI GREEN	LI OMNI-DIRECTIONAL ELEVATED

OBSTRUCTION, TAXIWAY, LANDING INDICATOR.
WHITE BEACON SITED AT OM SITE FOR R/W 21
OPERATES WHEN R/W 21 ILS IN USE. TAXIWAY
GREEN TO R/W 13/31 AMBER/BLUE TO R/W 03/21.
RED LIGHTS ON ALL STOPWAYS.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	64
LOWEST THRESHOLD (13)	40
LAT 55° 30' N, LONG 04° 35' W	
VARIATION 10° W	Bearings all magnetic
SERIAL No. 50340/3	DATE: 15-7-65
Reason for Re-Issue:	

VARIATION & LIGHTING

AERODROME CHART PRESTWICK

APRON HAZARDS. ELEVATED ELECTRIC POWER POINTS ON NORTHERN & SOUTHERN EDGES. (A) APPROX. 2' 8" A.G.L. (B) APPROX. 3' 6" A.G.L.
WARNING R/W 13/31. ELEVATED LIGHTS 10 1/4" HIGH AT EDGES OF CONCRETE, 23M. TARMAC SHOULDERS EITHER SIDE OF LIGHTS NOT
TO BE CONFUSED WITH RUNWAY SURFACE.

STOPWAYS: R/W 03: - 183M. R/W 21: - 290M. (BEARING STRENGTH OF UNPAVED STOPWAYS VARIES SEASONALLY).

R/W AVAILABILITY

- R/W 13: LANDING 2745 M.
(THRESHOLD DISPLACED 240 M)
- R/W 13: ROAD CROSSES R/W
730 M FROM THRESHOLD
PROPER. A/C OF SUITABLE
TAKE-OFF PERFORMANCE SHOULD
NORMALLY TAKE-OFF FROM A
POINT SE. OF ROAD. THIS GIVES
R/W LENGTH OF 2255 M.

NOTE: DISTANCE TO GO MARKERS IN THOUSANDS OF FEET R/W 13/31

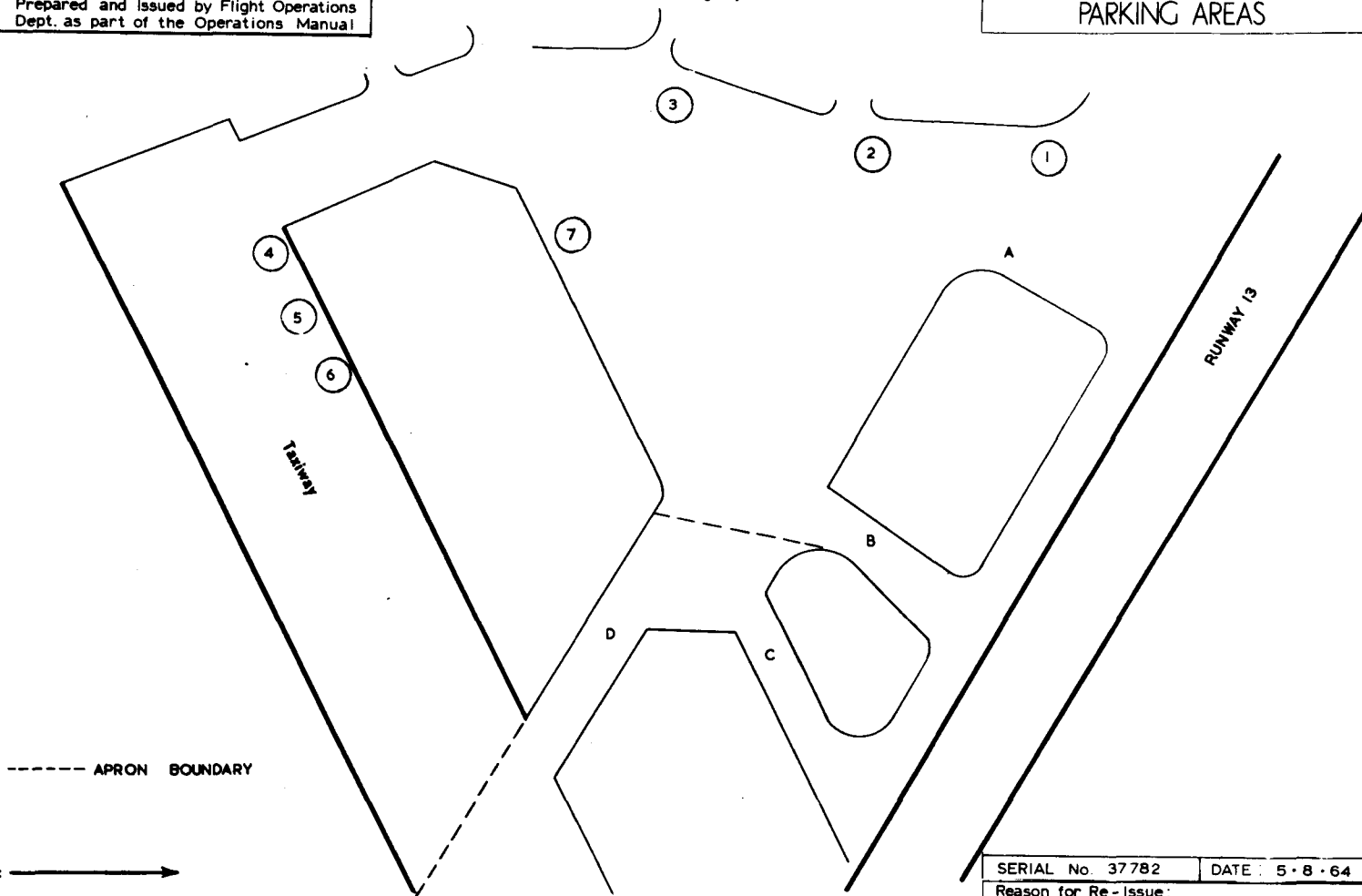
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Aircraft Parking Bays

PRESTWICK
PARKING AREAS

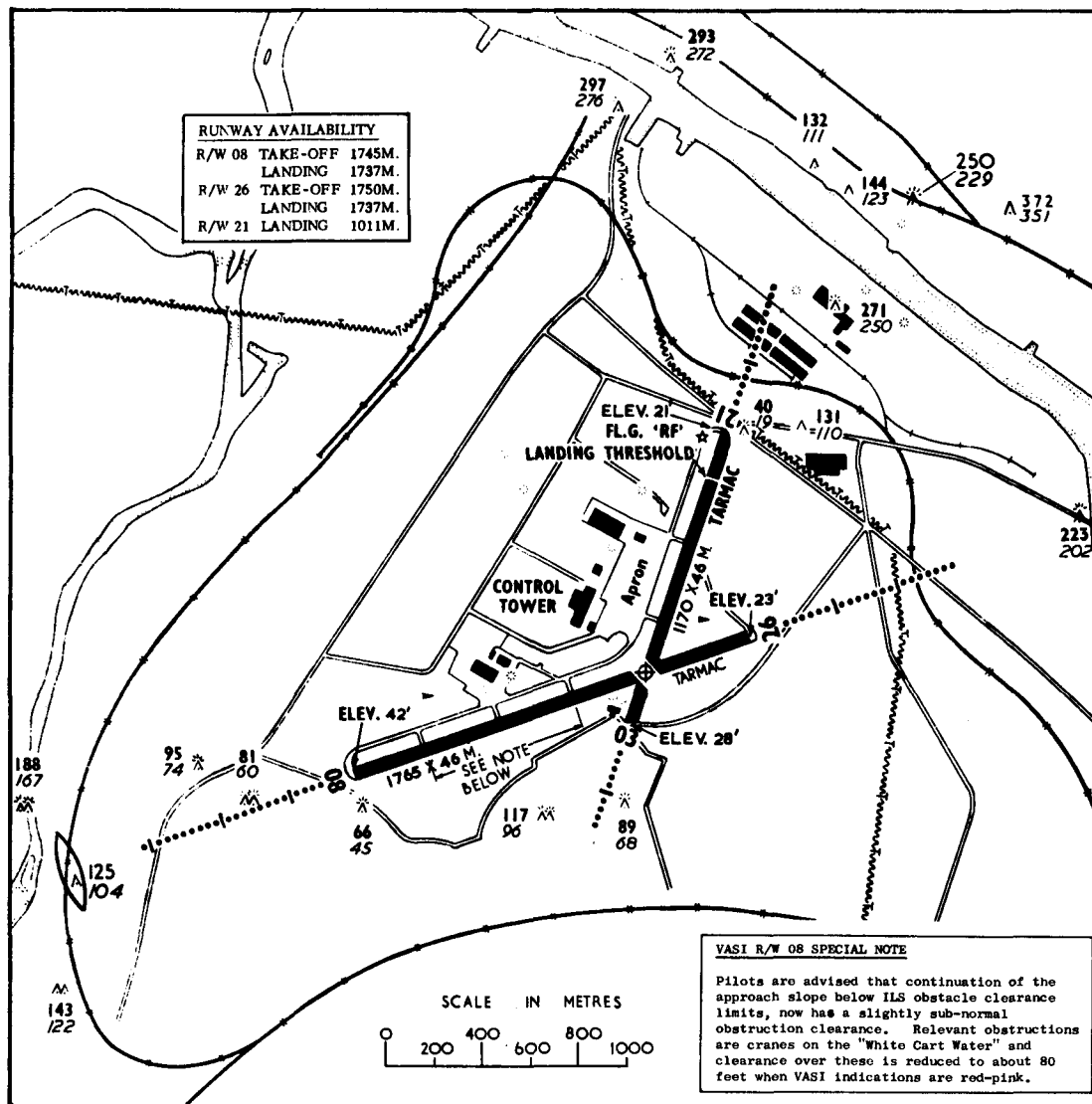


SERIAL No. 37782

DATE: 5.8.64

Reason for Re-issue:

PRESTWICK
PARKING AREAS



BRITISH EUROPEAN AIRWAYS

6EA

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
03	L.I. RED CENTRE-LINE WITH 1 CROSSBAR	
21	L.I. RED CENTRE-LINE WITH 1 CROSSBAR	
08	H.I. CENTRE-LINE WITH 3 CROSS BARS L.I. RED CENTRE-LINE WITH 3 CROSS BARS	3-25
26	SODIUM CENTRE-LINE L.I. RED CENTRE-LINE WITH 2 CROSS BARS	3-0

N.B. NO ENTRY UNDER VASI. ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
03	L.I. GREEN	L.I. OMNI-DIRECTIONAL
21	L.I. GREEN, DISPLACED 150M FROM THRESHOLD GREEN	L.I. OMNI-DIRECTIONAL
08		H.I. & L.I. OMNI-DIRECTIONAL, YELLOW CAUTION ZONE
26	GREEN	H.I. & L.I. OMNI-DIRECTIONAL

OBSTRUCTION, TAXIWAY, IDENT. BEACON 'RF' GREEN, FLOODLIGHTS, ANGLE OF APPROACH INDICATORS R/W 03

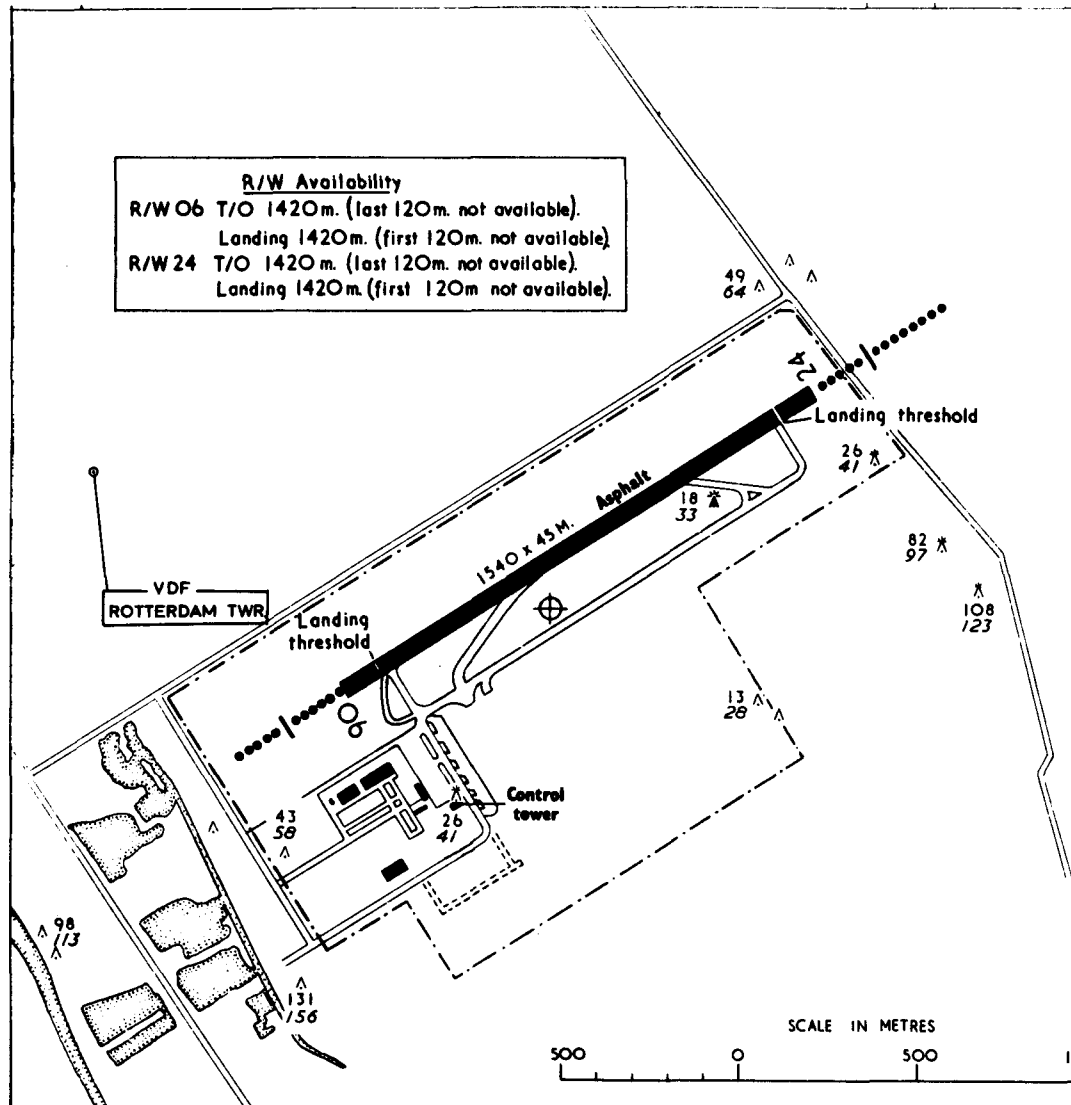
ELEVATIONS (FT) :-

OFFICIAL AERODROME	42
LOWEST THRESHOLD (21)	21
LAT 55° 52' N, LONG 04° 23' W	
VARIATION 11° W	Bearings all magnetic
SERIAL No. 50192 / 2	DATE : 17.3.65

Reason for Re-issue:

OBSTRUCTIONS & NOTES

R/W 08/26 VANGUARD A/C MUST BE KEPT ROLLING AND ARE NOT TO BE TURNED ROUND ON INDICATED SECTION OF RUNWAY. CAUTION WHEN ENTERING OR LEAVING LINK TAXIWAYS DUE TO SOFT NATURE OF ADJACENT GROUND.
"DISTANCE TO GO" MARKERS IN THOUSANDS OF FEET R/W 08/26.



R/W Availability
 R/W 06 T/O 1420m. (last 120m. not available).
 Landing 1420m. (first 120m. not available).
 R/W 24 T/O 1420m. (last 120m. not available).
 Landing 1420m. (first 120m. not available).

ROTTERDAM AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
06	WHITE H.I. (VARIABLE) CENTRE-LINE & BAR WHITE L.I. OMNI-DIRECTIONAL CENTRE-LINE & BAR	2-25°
24	WHITE H.I. (VARIABLE) CENTRE-LINE & BAR YELLOW L.I. OMNI-DIRECTIONAL CENTRE-LINE & BAR	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06	GREEN L.I. OMNI-DIRECTIONAL WING BARS AT DISPLACED THRESHOLD	WHITE H.I. UNI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL (LAST 610m YELLOW)
24		

OBSTRUCTION, TAXIWAY, ADME BEACON *
 LANDING INDICATOR, APRON FLOODS,
 GOOSENECKS IN EMERGENCY.

ELEVATIONS (FT) :—

OFFICIAL AERODROME 15 BELOW SEA LEVEL

LAT 51° 57' N, LONG 04° 26'E

VARIATION 5°W | Bearings all magnetic

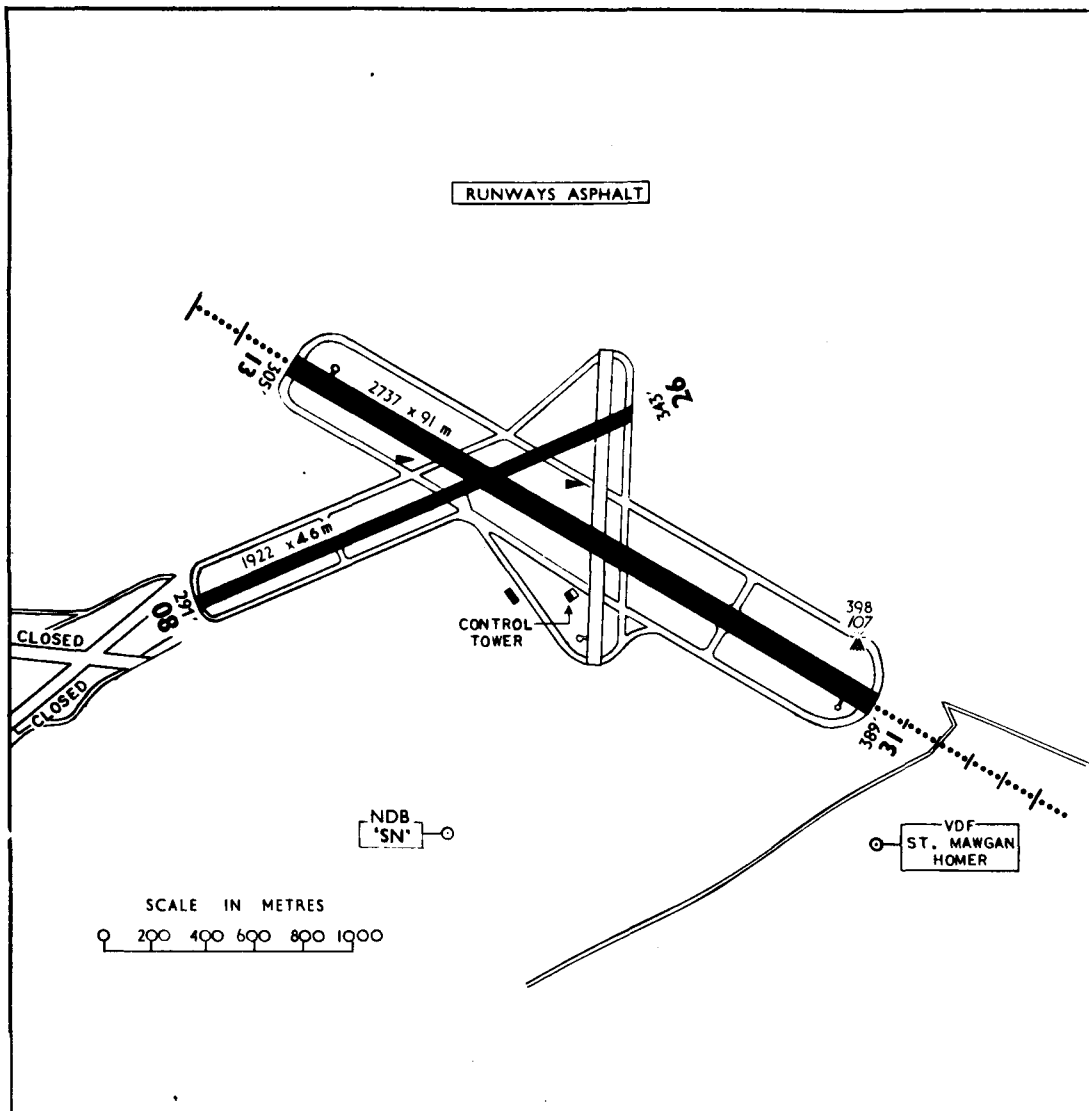
SERIAL No. 40289/1 | DATE : 25-11-64

Reason for Re-Issue:

VASI ANGLE & VARIATION.

AERODROME CHART ROTTERDAM

* AERODROME BEACON 2950M BEFORE R/W 24 ON R/W CENTRELINE FLASHING GREEN/WHITE.



ST. MAWGAN AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI (CAO Type Angle
31	H.I. C/L & 5 BARS	X
13	H.I. C/L & 2 BARS	X
08/26	SODIUM LEAD-IN	

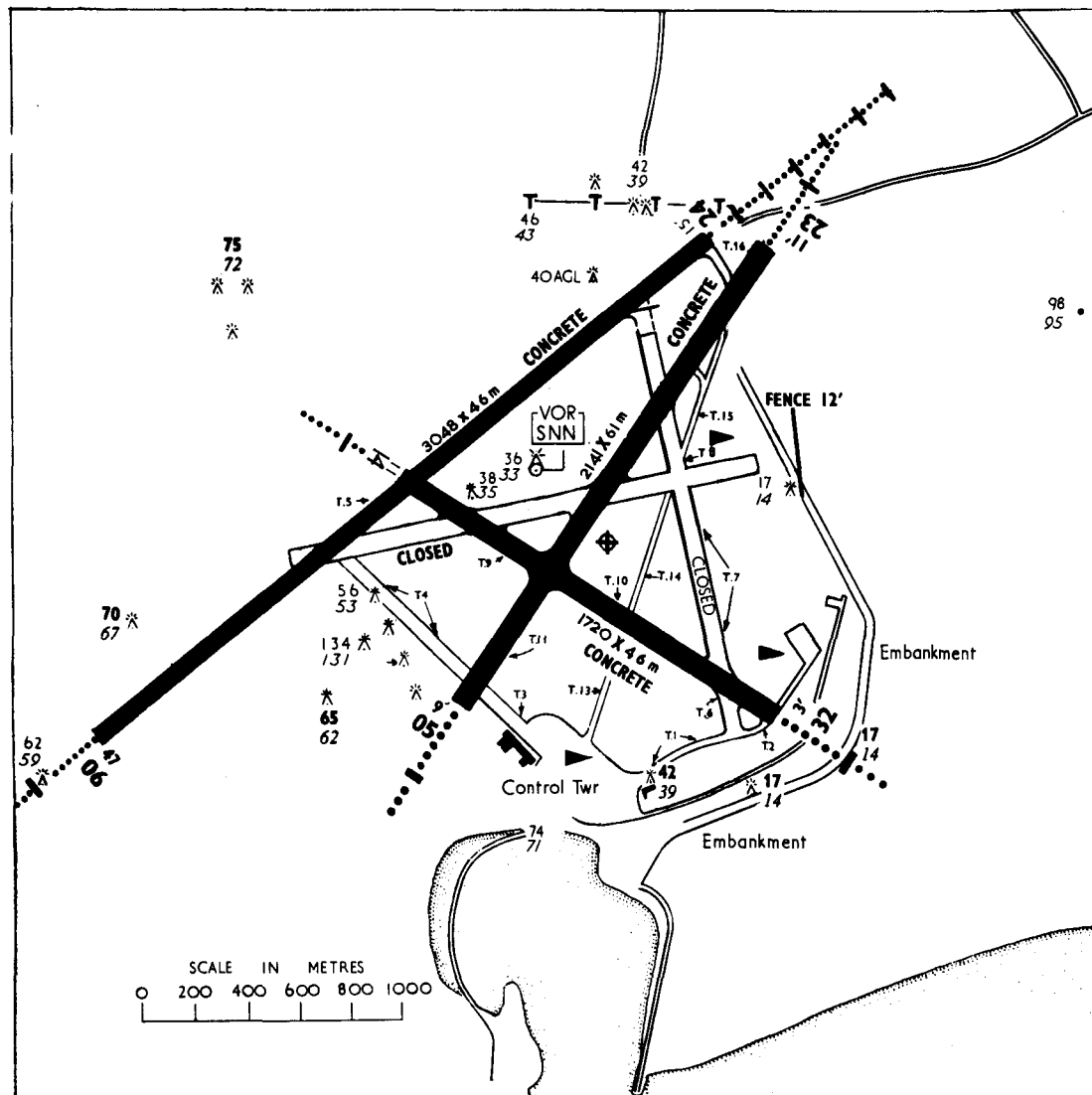
NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
31	GREEN	H.I. UNI-DIRECTIONAL
13	GREEN	H.I. UNI-DIRECTIONAL
08/26	NIL	YES

OBSTRUCTION, TAXIWAY, IDENT BEACON, 'SN' (RED)
WIND INDICATOR.

ELEVATIONS (FT)	
OFFICIAL AERODROME	389
LOWEST THRESHOLD (08)	291
LAT 50° 27' N, LONG 05° 00' W	
VARIATION 10° W	Bearings all magnetic
SERIAL No. 50360/1	DATE 1.7.65
Reason for Re-issue:	
R/W LENGTHS	

**AERODROME CHART
ST. MAWGAN**



SHANNON AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
05 23 14 32	RED L.I. OMNI-DIR. C/L & BAR	3°
06	WHITE L.I. OMNI-DIR C/L & BAR	3°
24	WHITE (VAR) H.I. UNI-DIR. CODED C/L WITH 6 BARS. WHITE L.I. OMNI-DIR C/L WITH 1 BAR.	3°

N.B. NO ENTRY UNDER VASI. ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
05 23 14 32	GREEN (VAR) OMNI-DIR.	WHITE (VAR) OMNI-DIR. (LAST THIRD YELLOW).
06 24	GREEN (VAR) H.I. BI-DIR ELEVATED 19 INS. WITH WINGS (24 ONLY) GREEN L.I. OMNI-DIR.	WHITE (VAR) H.I. BI-DIR ELEVATED 19 INS. (LAST 610 M. YELLOW) WHITE L.I. OMNI-DIR. ELEVATED 19 INS.

OBSTRUCTION, TAXIWAY.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	47
LOWEST THRESHOLD (32)	3
LAT 52° 42' N, LONG 08° 55' W	
VARIATION 12° W	Bearings all magnetic
SERIAL No 50109/2	DATE 12.5.65
Reason for Re-issue: VOR IDENT	

AERODROME CHART SHANNON

ELEVATED TAXIWAY LIGHTS TAXIWAY LIGHTS 2FT. AGL (TAXIWAYS Nos.13 14 & 15 ARE 26 INS AGL)
3 M. OUTSIDE PAVED EDGE (5FT. ON TAXIWAY No.11.)

WARNING 17 FT. EMBANKMENT BEFORE R/W 32 MARKED BY OBSTRUCTION LIGHTS.

SHANNON

JET AIRCRAFT NOISE ABATEMENT PROCEDURE RUNWAY 06

The procedure after take-off from Runway 06 should provide for maximum rate of climb, consistent with safety, up to 1500 ft. Track along the extended runway centre-line should be maintained until 2 minutes after take-off, or until the aircraft is at least 2500 ft. AGL, before commencing turns.

BRITISH EUROPEAN AIRWAYS

BEA

Reason for Re-issue:

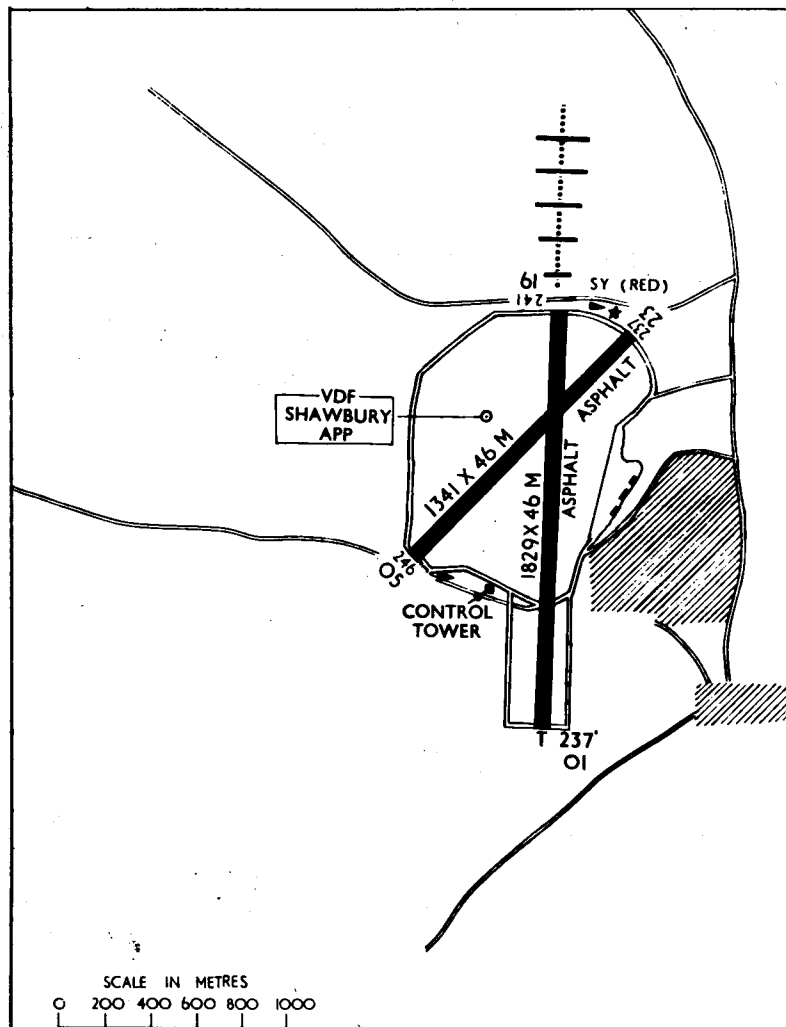
PRESENTATION

SERIAL No.40046/1

DATE : 23.4.65

SHANNON

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CIRCUITS

RIGHT HAND R/Ws 01 & 05

NOTE

TREES ON SW BOUNDARY

SHAWBURY AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
01	L.I. RED TEE	X
05	SODIUM LEAD-IN	
19	H.I. CENTRE-LINE & S BARS L.I. RED TEE	X
23	SODIUM LEAD-IN	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
01	GREEN	L.I. OMNI-DIRECTIONAL
05	GREEN	L.I. OMNI-DIRECTIONAL
19	GREEN	H.I. UNI-DIRECTIONAL L.I. OMNI-DIRECTIONAL
23	GREEN	L.I. OMNI-DIRECTIONAL

OBSTRUCTION, TAXIWAY, IDENT BEACON, SY RED
WIND INDICATOR

ELEVATIONS (FT) :—

OFFICIAL AERODROME 248

LOWEST THRESHOLD 01 & 23 237

LAT 52° 48' N, LONG 02° 40' W

VARIATION 9° W Bearings all magnetic

SERIAL No 50250 DATE 16.6.65

Reason for Re-issue:

REVISED LAYOUT

AERODROME CHART SHAWBURY

SECT. SAFE ALT.
25 N.M. 3200'

50'

02°40'W

30'

SECT. SAFE ALT.
25 N.M. 1800'

SHAWBURY VDF R/W 01

BRITISH EUROPEAN AIRWAYS

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APPROACH SHAWBURY APP.

121°5E 130°7

TOWER SHAWBURY TWR.

121°5E

TRANS ALT 3000 FT.

G/S(KTS)	70	90	110	130	150
VDF TO TURN	6.26	5.0	4.06	3.28	3.0

G/S	170	190	210	230	250
VDF TO TURN	2.39	2.22	2.09	1.57	1.48

OVERSHOOT

JET AC : TURN RIGHT ONTO 030° TO 4500 4263
THEN TURN LEFT ONTO 360° OR AS DIRECTED.
OTHER AC : TURN RIGHT ONTO 150° CLIMB TO
4500 4263 OR AS DIRECTED.

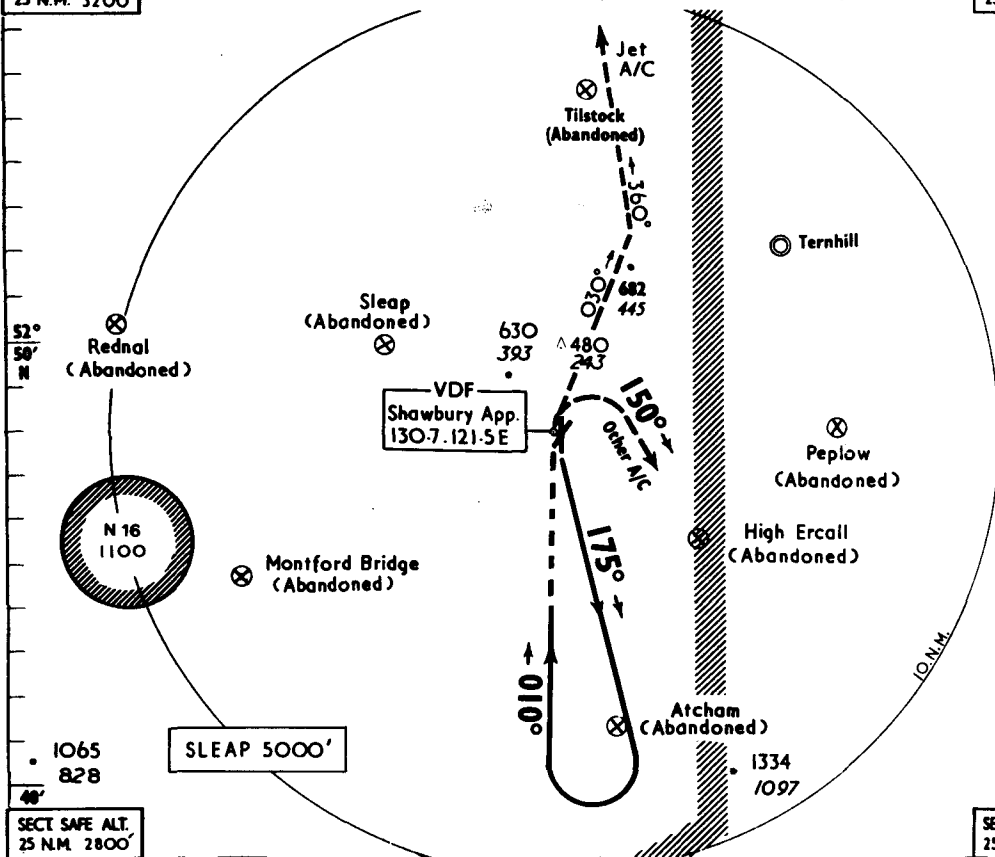
ELEVATIONS (FT) :-

OFFICIAL AERODROME	248
LOWEST THRESHOLD	237
THRESHOLD R/W 01	240

VARIATION 9°W | Bearings all magnetic
SERIAL No. 50250/2 | DATE : 16.6.65

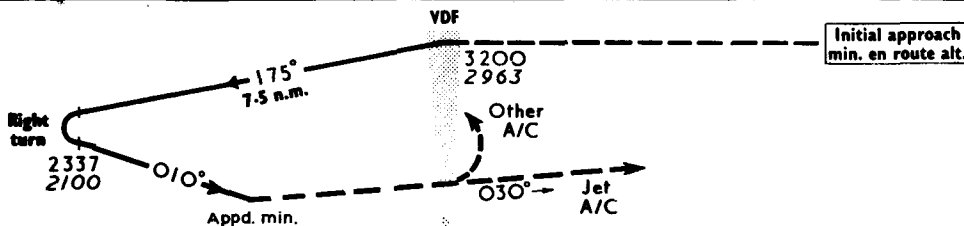
Reason for Re-Issue:
PRESENTATION

VDF R/W 01
SHAWBURY



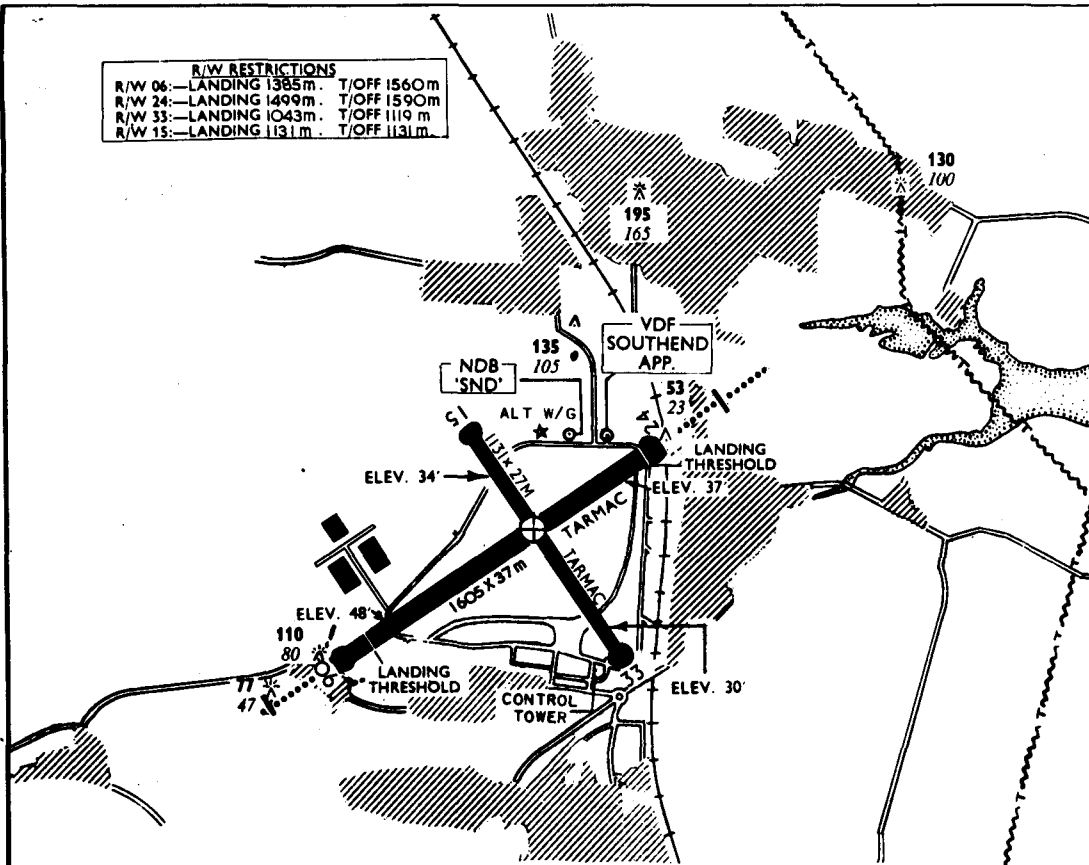
SECT. SAFE ALT.
25 N.M. 2800'

SECT. SAFE ALT.
25 N.M. 2800'



S N.M. 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 N.M.

R/W RESTRICTIONS
 R/W 06:—LANDING 1385m. T/OFF 1560m
 R/W 24:—LANDING 1499m. T/OFF 1590m
 R/W 33:—LANDING 1043m. T/OFF 1119m
 R/W 15:—LANDING 1131m. T/OFF 1131m



SOUTHEND AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06	HIGH INT. & LOW INT. RED CENTRE-LINE & BAR	3 1/4°
24		3 1/4°
15 33	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

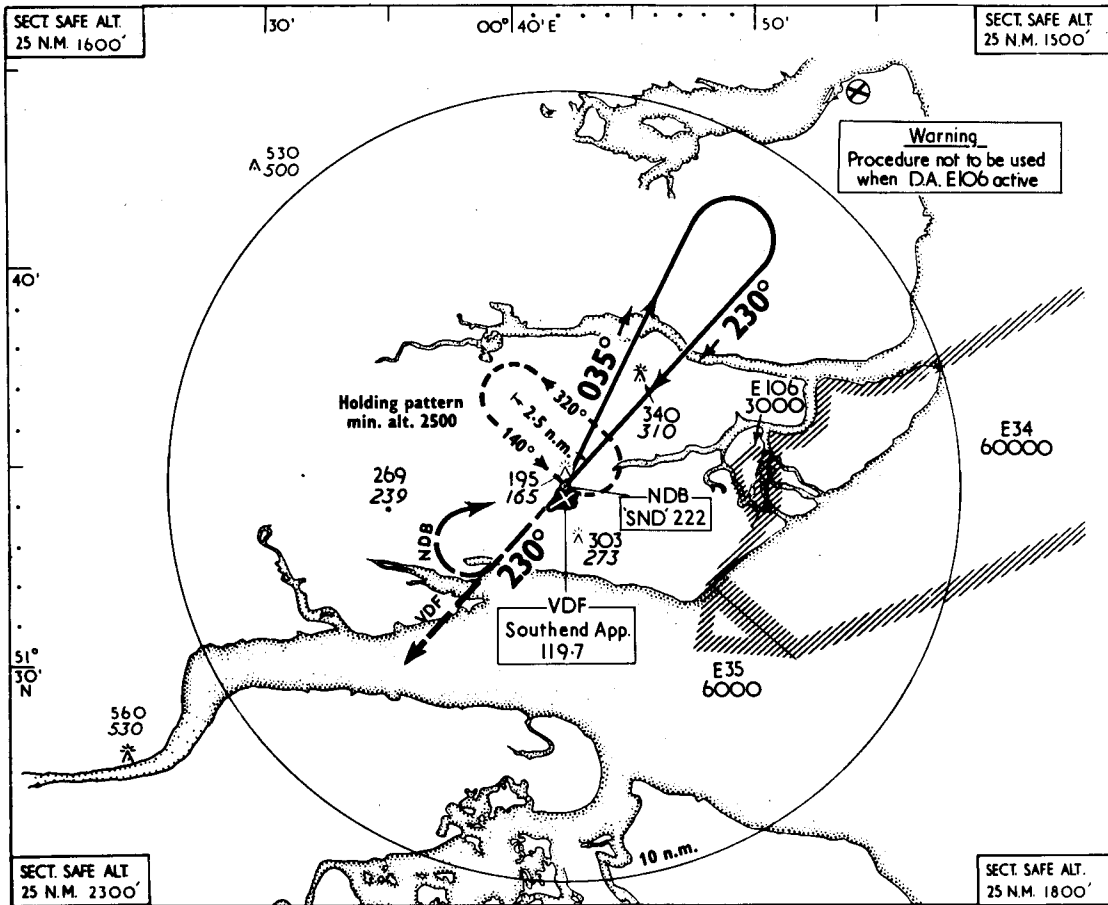
R/W	THRESHOLD	RUNWAY
06	GREEN OMNI-DIR.	ELEVATED H.I. BI-DIR. & L.I. OMNI-DIR. WHITE. LAST 610M. YELLOW.
24		
15	GREEN STUB BAR TWO LIGHTS EACH SIDE	OMNI-DIR. WHITE
33	GREEN OMNI-DIR.	

OBSTRUCTION, TAXIWAY, ADME BEACON
LANDING INDICATOR ANGLE OF APPROACH
LIGHTS R/W 15 & 33

ELEVATIONS (FT)

OFFICIAL AERODROME	48
LOWEST THRESHOLD (33)	30
LAT 51° 34' N, LONG 00° 42' E	
VARIATION 8° W	Bearings all magnetic
SERIAL No. 40246/1	DATE: 22.10.64
Reason for Re-issue:	
REVISED LIGHTING & TAXIWAYS.	

AERODROME CHART SOUTHEND



SOUTHEND NDB OR VDF (B.C.P.)

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APPROACH SOUTHEND APP.

119.7

TOWER SOUTHEND TWR.

119.7 125.05

TRANS ALT 4000 FT.

G/S (KTS)	70	90	110	130	150
FAC TO TURN	6.26	5.0	4.06	3.28	3.0

G/S	170	190	210	230	250
FAC TO TURN	2.39	2.22	2.09	1.57	1.48

OVERSHOOT

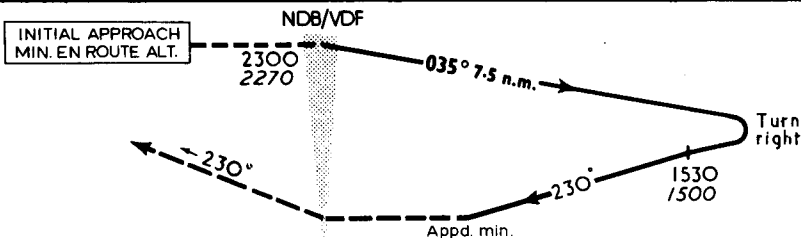
NDB CLIMB AHEAD TO 1030 / 1000 AND
RETURN TO NDB AT 1500 / 1470

VDF CLIMB AHEAD TO 2300 2270

ELEVATIONS (FT)

OFFICIAL AERODROME	48
LOWEST THRESHOLD	30
THRESHOLD R/W	—
VARIATION 8° W	Bearings all magnetic
SERIAL No. 50361/2	DATE: 16.7.65.
Reason for Re-Issue:	
TWR FREQ. & CRITICAL HT. DELETED	

NDB OR VDF (B.C.P.)
SOUTHEND

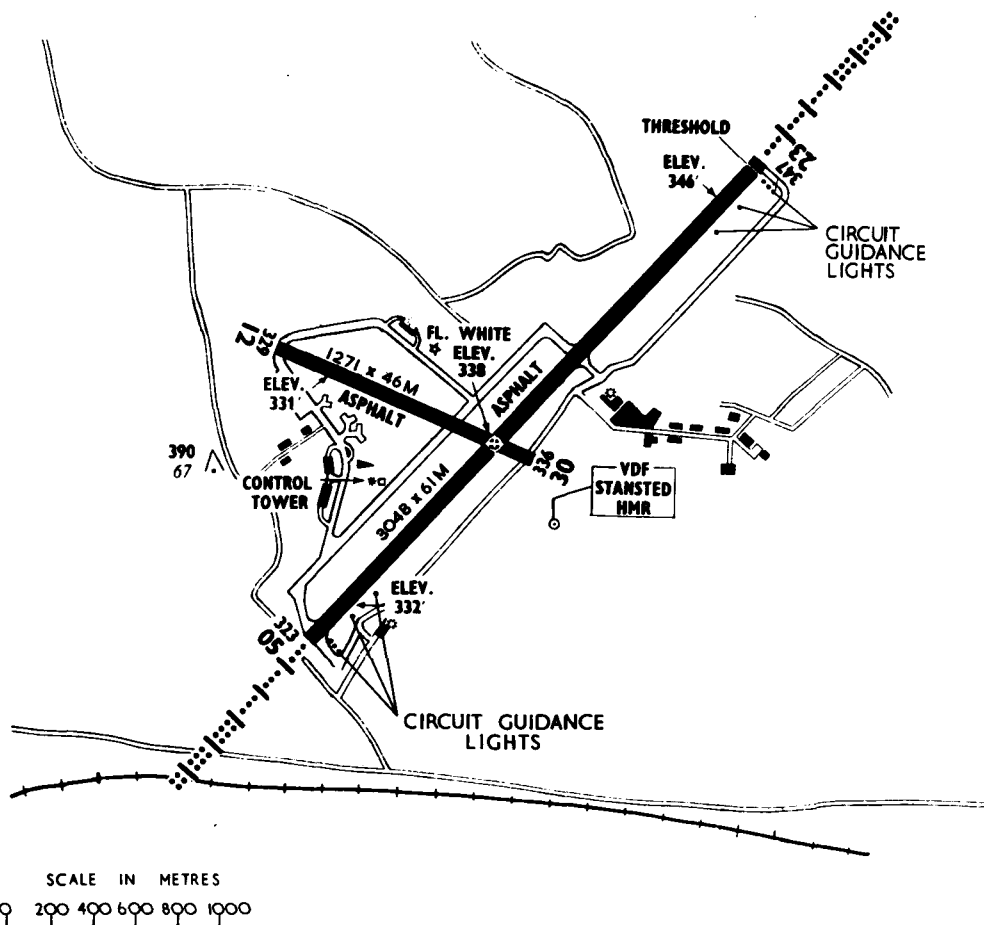


SW N.M. 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 NE N.M.

R/W Availability

R/W 12 Day operations only.

R/W 23 Landing & T/O 3024 m. (first 24 m. sterile).

**NOTE:**

1. A/C IN CIRCUIT TO KEEP N OF AN E-W LINE 2 N.M. S OF A/D.
2. R/W 05 RIGHT HAND CIRCUIT. A/C TAKING OFF R/W 05 TURN RIGHT AS SOON AS POSSIBLE.
3. R/W 12 NOT AVAILABLE AT NIGHT.
4. "DISTANCE TO GO" MARKERS IN THOUSANDS OF FEET R/W 05/23

STANSTED AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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Dept. as part of the Operations Manual**LIGHTING**

R/W	APPROACH	VASI ICAO Type Angle
05 & 23	H.I. CENTRE-LINE WITH CROSSBARS. SUPERIMPOSED RED 'T' 150M. FROM THRESHOLD, EXTENDING 305M.	3° 3°
30	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
05	H.I. GREEN	L.I. OMNI-DIRECTIONAL & H.I. BI-DIRECTIONAL
23	H.I. GREEN WINGBARS	
30	SODIUM WINGBARS	GOOSENECKS

OBSTRUCTION, TAXIWAY, ADME BEACON,
ANGLE OF APPROACH LIGHTS R/W 30, R/W 05/23
SODIUM CIRCUIT GUIDANCE LIGHTS.**ELEVATIONS (FT) : —**

OFFICIAL AERODROME	349
LOWEST THRESHOLD (05)	323
LAT	51° 53' N, LONG 00° 14' E
VARIATION	7° W
Bearings	all magnetic
SERIAL No.	50076/1
DATE	11.2.65

Reason for Re-Issue:

VASI'S & CIRCUIT GUIDANCE LIGHTS.

AERODROME CHART STANSTED

STANSTED

NOISE ABATEMENT

- 1 The following minimum noise routeings and procedures apply to all aircraft taking off from the airport:-

RUNWAY 23

Aircraft should not commence any right turn at less than 3.5 nm from start of roll, in order to ensure that their flight path avoids passing over Bishop's Stortford.

Note:- This procedure may entail flight below Airway Red One.

RUNWAY 05

Aircraft cleared to the West, North West or North via a right turn out should commence their turn over the Middle Marker of the ILS System or as soon as practicable thereafter. A left turn out will be given by ATC where possible, but in all cases aircraft should avoid flying over Bishop's Stortford.

- 2 The above routeings and procedures are compatible with normal ATC requirements; in individual cases they may be varied by ATC whenever necessary. The use of the minimum noise routeings, specified above, is supplementary to the noise abatement take-off techniques as used by piston-engined, turbo-prop and turbo-jet aircraft.

BRITISH EUROPEAN AIRWAYS

BEA

Reason for Re-Issue:

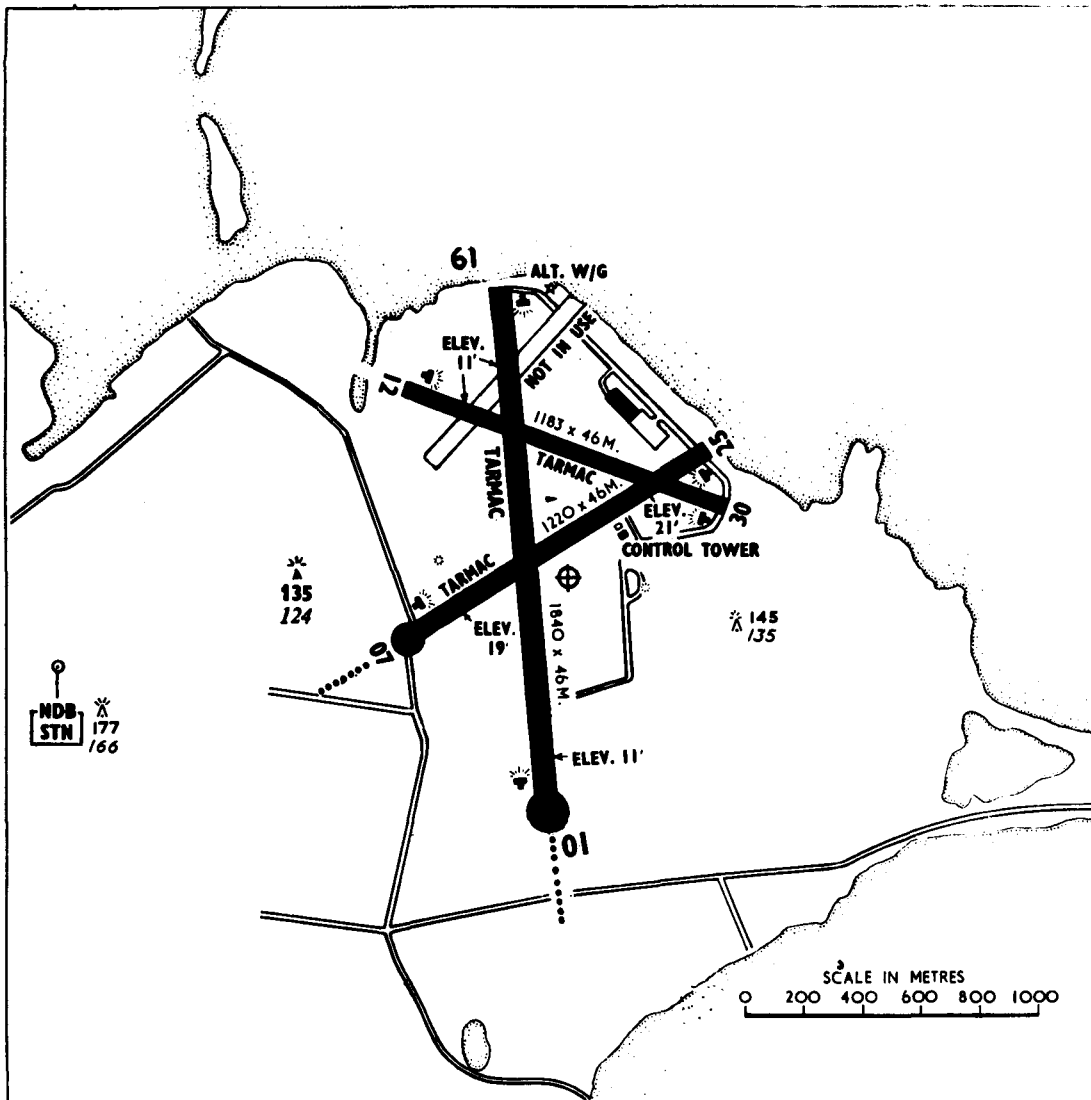
Revised text for
Runway 23

SERIAL No. 50359

DATE : 4.6.65.

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STANSTED



STORNOWAY AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
01 07	SHORT SODIUM CENTRE-LINE	
19	NIL	2.5°
25	SODIUM TEE	3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
01 19	NIL	BI-DIRECTIONAL H.I. WITH OMNI-DIRECTIONAL COMPONENT
07 25	NIL	UNI-DIRECTIONAL

OBSTRUCTION, ADME. BEACON
LANDING INDICATORS

ELEVATIONS (FT) : —

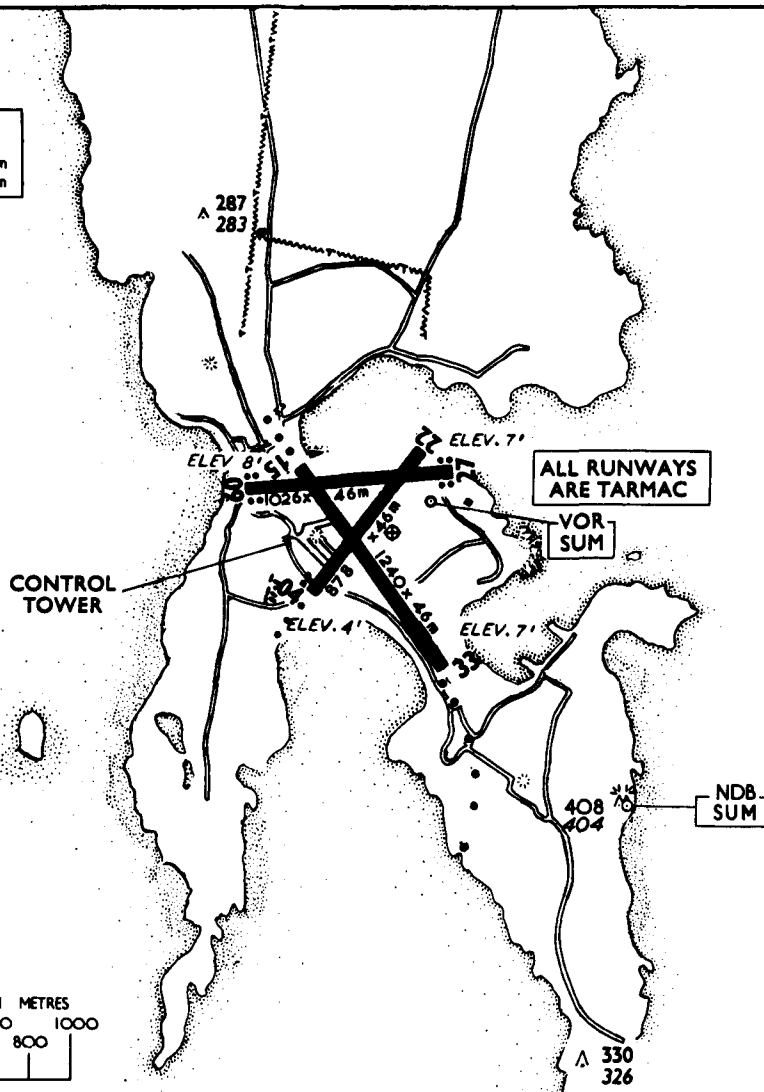
OFFICIAL AERODROME	30	
LOWEST THRESHOLD	01, 12 & 19	11
LAT	58°13'N.	LONG 06° 20W
VARIATION	12°W	Bearings all magnetic
SERIAL No.	50089/1	DATE : 9 . 2 . 65

Reason for Re-issue:

R/W 12/30 LIGHTING WITHDRAWN.

AERODROME CHART STORNOWAY

R/W AVAILABILITY
 R/W 04 LANDING 715m
 R/W 09 LANDING 1000m
 R/W 33 LANDING 1180m



SUMBURGH AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
04 & 15	SHORT SODIUM C/L	3°
09	SODIUM BOXES	
27	SODIUM BOXES	
33	CURVED SODIUM LINE	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
04 & 15	NIL	GOOSENECKS
09 & 27	NIL	GOOSENECKS
33	NIL	GOOSENECKS

OBSTRUCTION, VASI R/W 27 ON RIGHT HAND SIDE
 OF R/W ONLY.

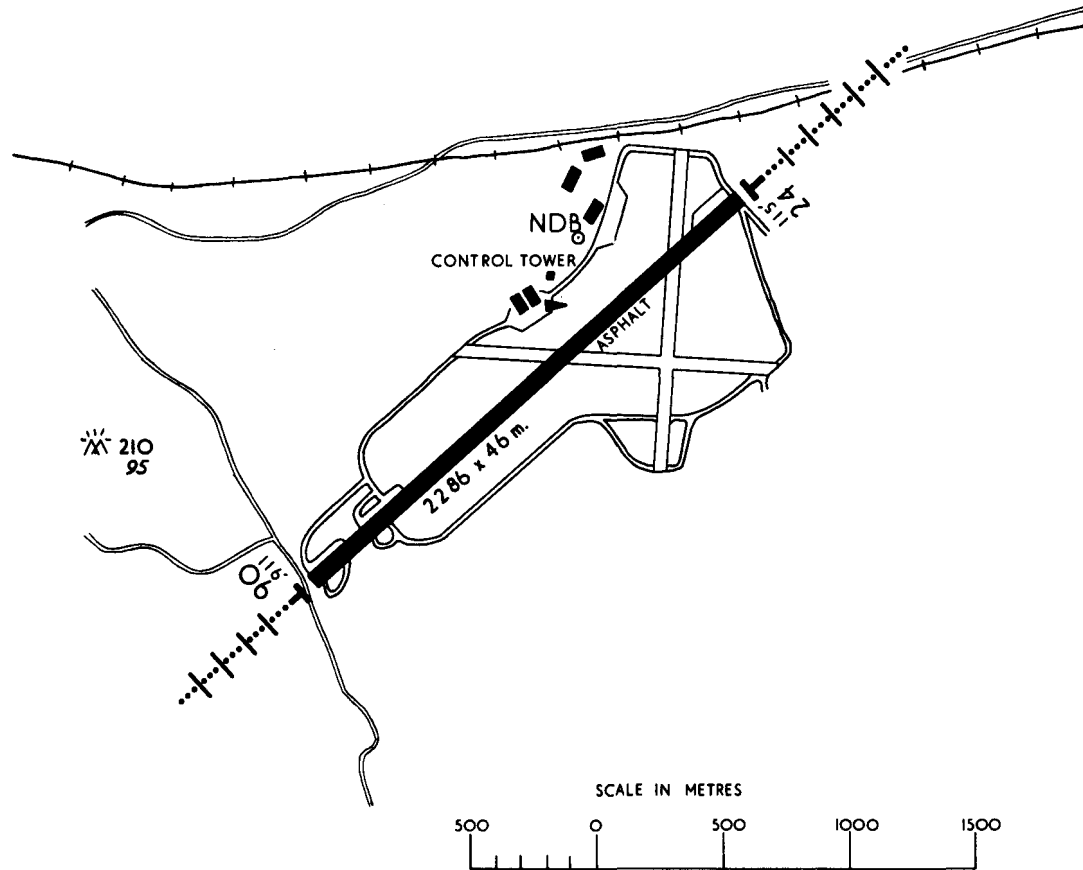
ELEVATIONS (FT) :-

OFFICIAL AERODROME	18
LOWEST THRESHOLD (04)	4
LAT 59° 53' N, LONG 01° 18' W	
VARIATION 10° W	Bearings all magnetic
SERIAL No. 40374/1	DATE: 30.12.64.

Reason for Re-issue:
 NDB RELOCATED, OBSTRUCTION ADDED

EFFECTIVE 14.1.65.

AERODROME CHART
 SUMBURGH



R/W 06 - RH CIRCUITS

TEES SIDE (MIDDLETON) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
06	} HI. CENTRE LINE WITH BARS RED TEE	3°
24		3°

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06	} GREEN	} HI. UNI - DIR. LI. OMNI - DIR. SODIUMS.
24		

OBSTRUCTION, TAXIWAY.

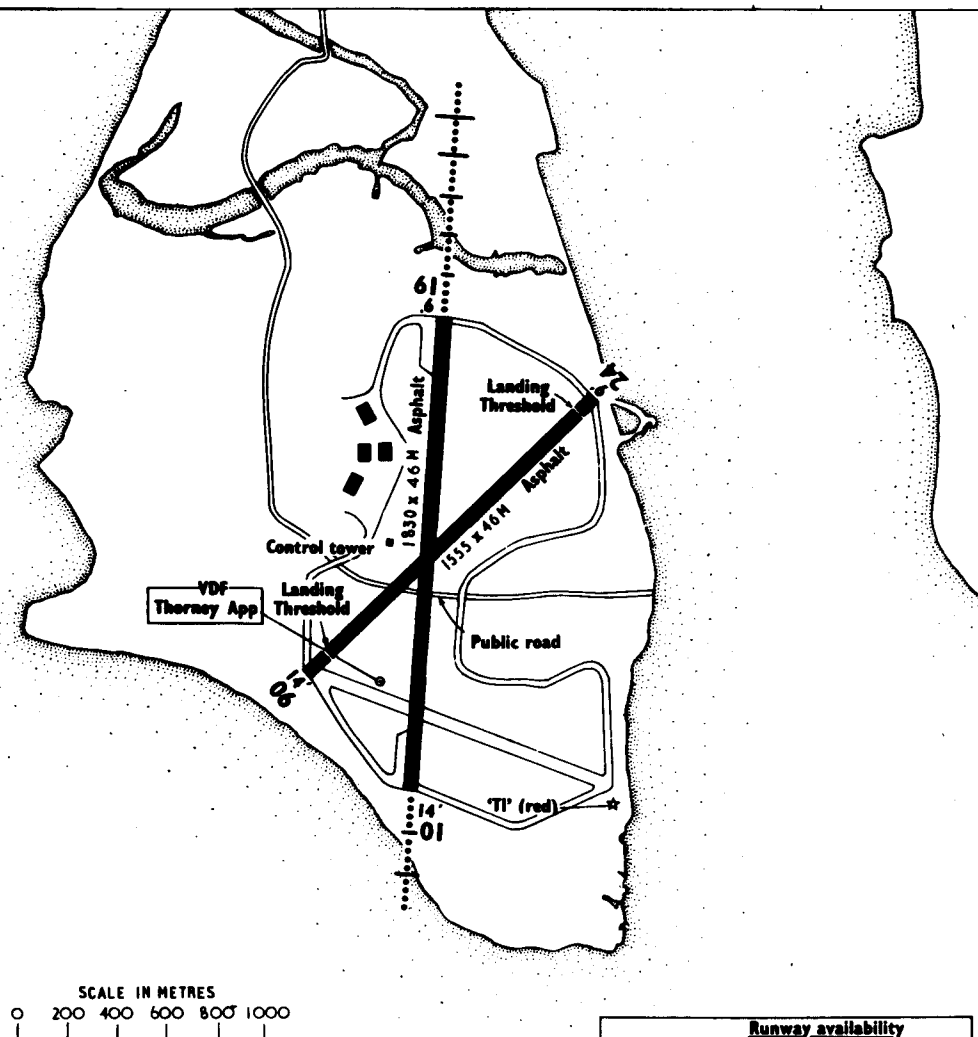
ELEVATIONS (FT) :—

OFFICIAL AERODROME	120
LOWEST THRESHOLD (24)	115
LAT 54° 31' N, LONG 01° 26' W	
VARIATION 9° W	Bearings all magnetic
SERIAL No. 50187	DATE: 12.4.65

Reason for Re-issue:

NEW ISSUE

AERODROME CHART TEES SIDE (MIDDLETON)



NOTE
PUBLIC ROAD CROSSES A/D & TAKE-OFF & LANDING
CLEARANCES MUST BE REQUESTED IN TIME FOR A.T.C.
TO CLOSE & CLEAR THIS ROAD. VARIABLE CIRCUITS ON WEDNESDAYS.

Runway availability	
R/W 06 Landing	1451M (first 104M sterile)
R/W 24 Landing	1482M (first 73M sterile)

THORNEY ISLAND AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06/24	NIL	
01/19	HIGH INT. CENTRE-LINE & BARS	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06/24	GREEN	L.I. OMNI-DIR.
01/19	GREEN	H.I. UNI-DIR.

OBSTRUCTION, TAXIWAY, IDENT BEACON 'TI' (RED)
WIND INDICATOR. ANGLE OF APPROACH R/W 19.

ELEVATIONS (FT)

OFFICIAL AERODROME	18
LOWEST T/H (19, 24)	9
LAT 50° 49' N, LONG 00° 55' W	
VARIATION 8° W Bearings all magnetic	
SERIAL No. 46793	DATE: 5.8.64

Reason for Re-issue:

AERODROME CHART THORNEY ISLAND

SECT. SAFE ALT
25 N.M. 2100'

SECT. SAFE ALT
25 N.M. 2100'

THORNEY ISLAND ILS RW 19

BRITISH EUROPEAN AIRWAYS

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APPROACH THORNEY APP.

130.7 121.5E

TOWER THORNEY TWR.

130.7 121.5E

TRANS ALT 3000 FT.

G/S (KTS)	70	90	110	130	150
OM TO ADME	4.17	3.20	2.44	2.19	2.0
DESCENT ON G/P (FT/MIN)	371	477	583	689	796
G/S	170	190	210	230	250
OM TO ADME	1.46	1.35	1.26	1.18	1.12
DESCENT ON G/P (FT/MIN)	902	1008	1114	1221	1326

SECT. SAFE ALT
25 N.M. 2400'

SECT. SAFE ALT
25 N.M. 1800'

CRITICAL HEIGHTS (FT)

WITH G/P 200 NO G/P 500

OVERSHOOT

CLIMB AHEAD TO 509 500. THEN CLIMBING TURN
RIGHT ONTO 320° TO 2100 2091.

ELEVATIONS (FT) :

OFFICIAL AERODROME 18

LOWEST THRESHOLD 9

THRESHOLD R/W 19 9

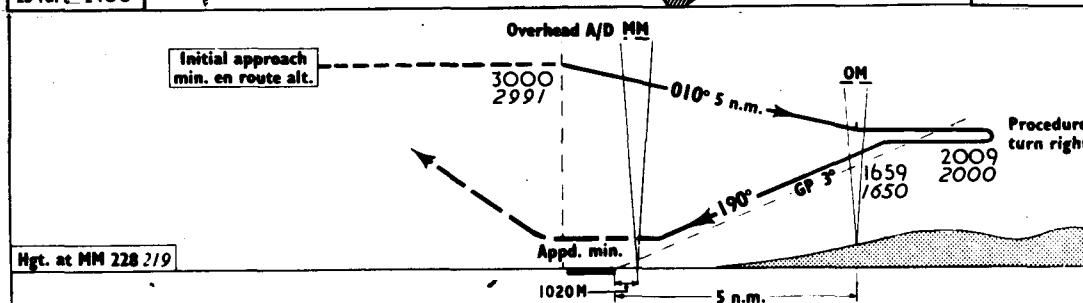
VARIATION 8° W Bearings all magnetic

SERIAL No. 50027/1 DATE 1.2.65

Reason for Re-issue:

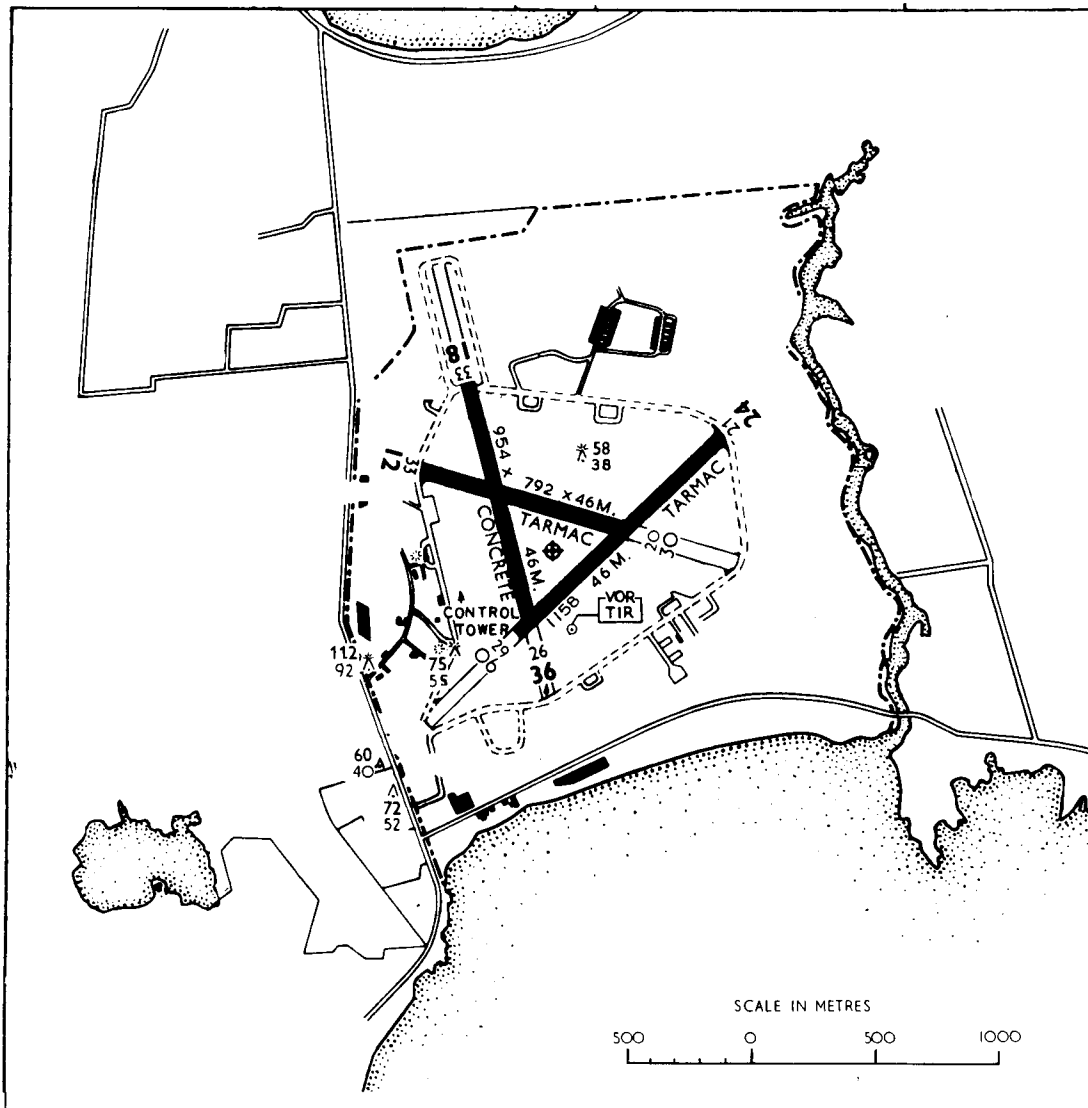
D.A.S

ILS R/W 19
THORNEY ISLAND



NOTE
Localiser Offset 3°
(R/W centre-line 193°)





TIREE AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06 24 12 30 18 36	SHORT SODIUM CENTRE-LINE	3°

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06 24 12 30 18 36	NIL	GOOSENECKS

OBSTRUCTION, TAXIWAY,

ELEVATIONS (FT)

OFFICIAL AERODROME	38
LOWEST THRESHOLD (30)	20
LAT 56° 30' N, LONG 06° 52' W	
VARIATION 12° W	Bearings all magnetic
SERIAL No. 48141/1	DATE 21.10.64

Reason for Re-issue:

TAXIWAY NOTE ADDED

AERODROME CHART TIREE

STOPWAYS R/W 12 - 6CM R/W 18 - 52M R/W 36 - 52M

NOTE ALL TAXIWAYS CLOSED EXCEPT BETWEEN CONTROL TOWER & THRESHOLD R/W 12

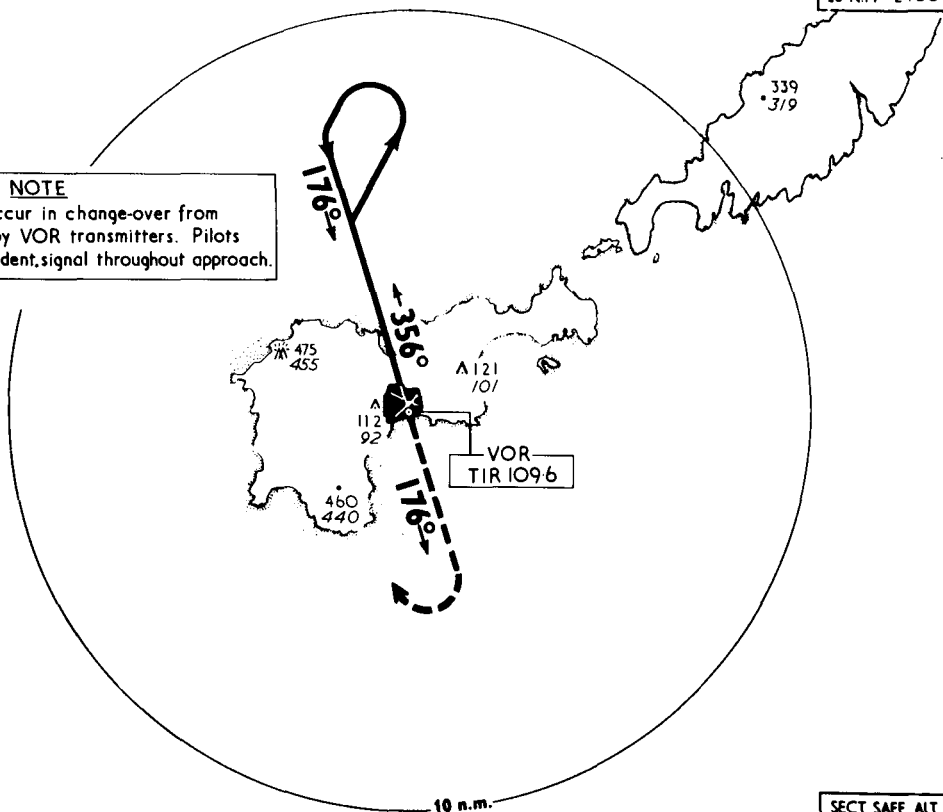
SECT. SAFE ALT.
25 N.M. 1500'

10' 07° 00' W 06° 50' W 40'

SECT. SAFE ALT.
25 N.M. 2400'

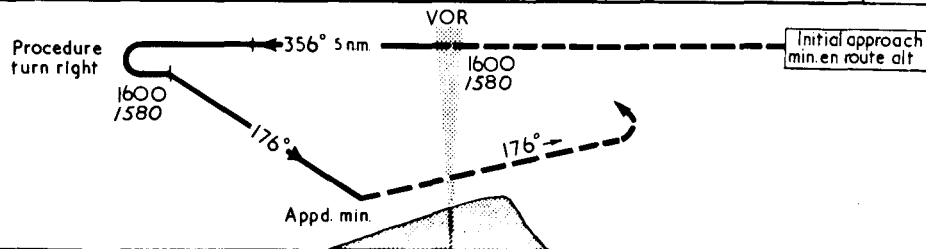
NOTE

Delay may occur in change-over from main to standby VOR transmitters. Pilots must monitor ident, signal throughout approach.



SECT. SAFE ALT.
25 N.M. 1500'

SECT. SAFE ALT.
25 N.M. 3700'



TIREE VOR R/W18

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH TIREE TWR

118.3

TOWER TIREE TWR

118.3

TRANS ALT 3000 FT.

G/S (KTS)	70	90	110	130	150
VOR TO TURN	4.17	3.20	2.44	2.19	2.00

G/S (KTS)	170	190	210	230	250
VOR TO TURN	1.46	1.35			

OVERSHOOT

CLIMB AHEAD TO 1600/1580 THEN TURN
RIGHT & RETURN TO FACILITY

ELEVATIONS (FT)

OFFICIAL AERODROME 38

LOWEST THRESHOLD 20

THRESHOLD R/W 18 33

VARIATION 13° W Bearings all magnetic

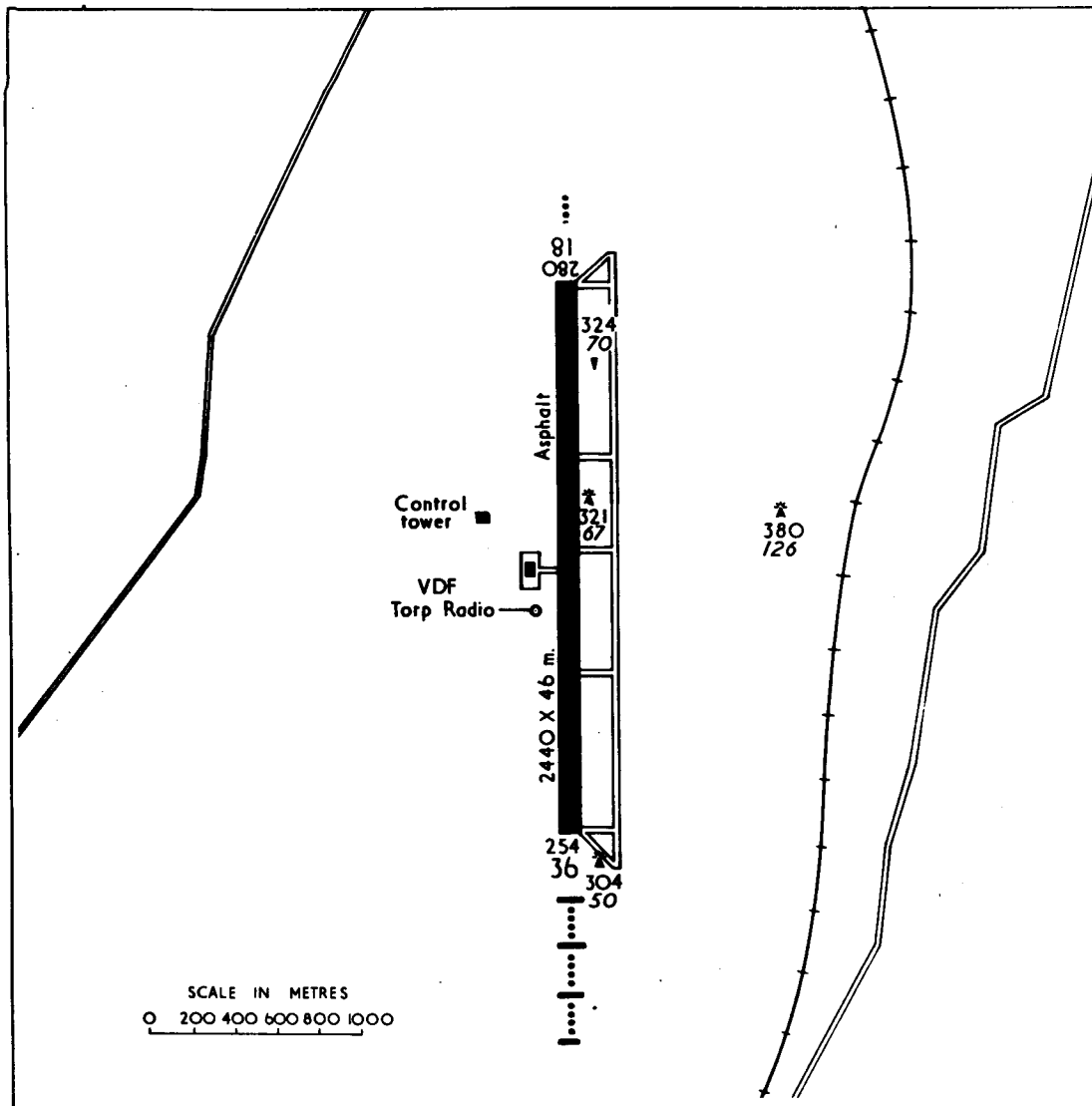
SERIAL No 50306/2 DATE 6.5.65

Reason for Re-issue:

OBSTRUCTIONS

VOR R/W18
TIREE

N.M. 12 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 11 12 N.M.



SCALE IN METRES
0 200 400 600 800 1000

STOPWAY :- R/W 18 & 36 275 M.

TORP AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
18/36	HIGH INTENSITY WHITE	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
18/36	GREEN	HIGH INTENSITY WHITE

OBSTRUCTION, TAXIWAY (GOOSENECKS OR
- WINTER MONTHS), STOPWAYS (RED WITH WHITE
WING BARS).

ELEVATIONS (FT)

OFFICIAL AERODROME	287
LOWEST THRESHOLD (36)	254
LAT 59° 11' N, LONG 10° 16' E	
VARIATION 4° W	Bearings all magnetic
SERIAL No. 50155/1	DATE 15.6.65.

Reason for Re-issue:

PRESENTATION

AERODROME CHART TORP

SECT. SAFE ALT.
25 N.M. 3100

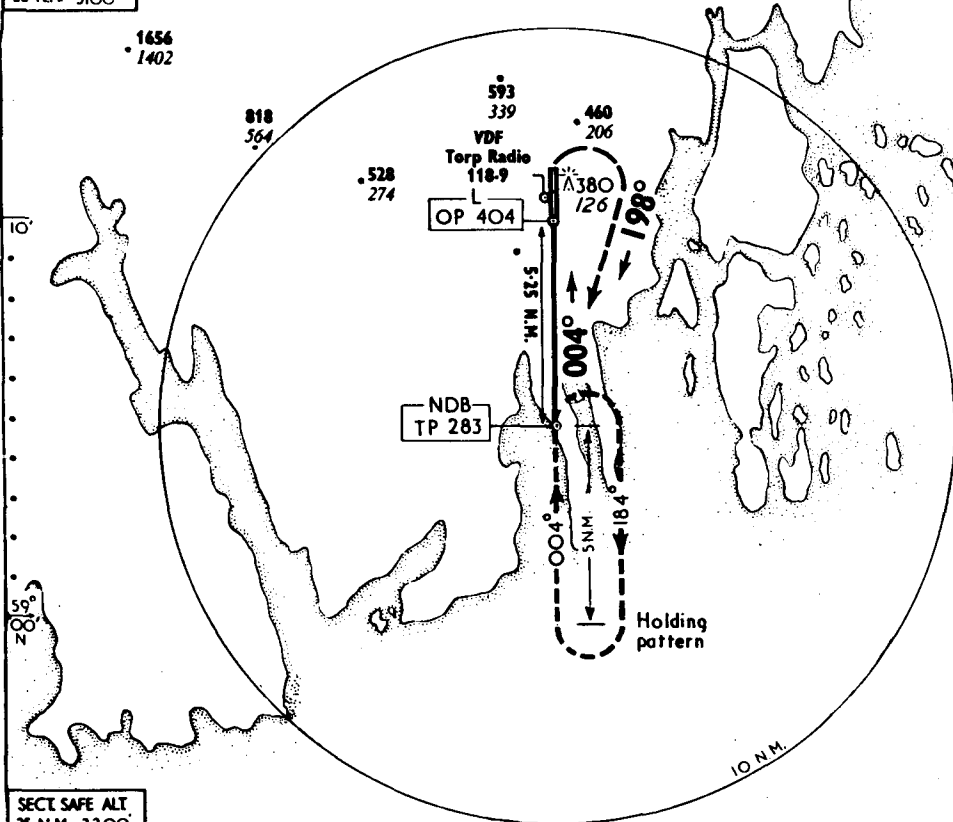
10° 00' E

10°

20°

10° 30' E

SECT. SAFE ALT.
25 N.M. 2000



SECT. SAFE ALT.
25 N.M. 2200

SECT. SAFE ALT.
25 N.M. 1500

Initial approach
min en route alt

Shuttle on
holding pattern

NDB

1500

OP

Appd. min.

S

N.M. 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 N.M.

TORP

NDB R/W 36

BRITISH EUROPEAN AIRWAYS

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APPROACH RYGGE TWR

119.7 119.5

TOWER TORP ADME

(AFIS) 118.3 118.9

TRANS ALT 3500 FT.

G/S (KTS)	70	90	110	130	150
NDB TO ADME	4.30	3.30	2.52	2.25	2.06

G/S	170	190	210	230	250
NDB TO ADME	1.51	1.39			

OVERSHOOT

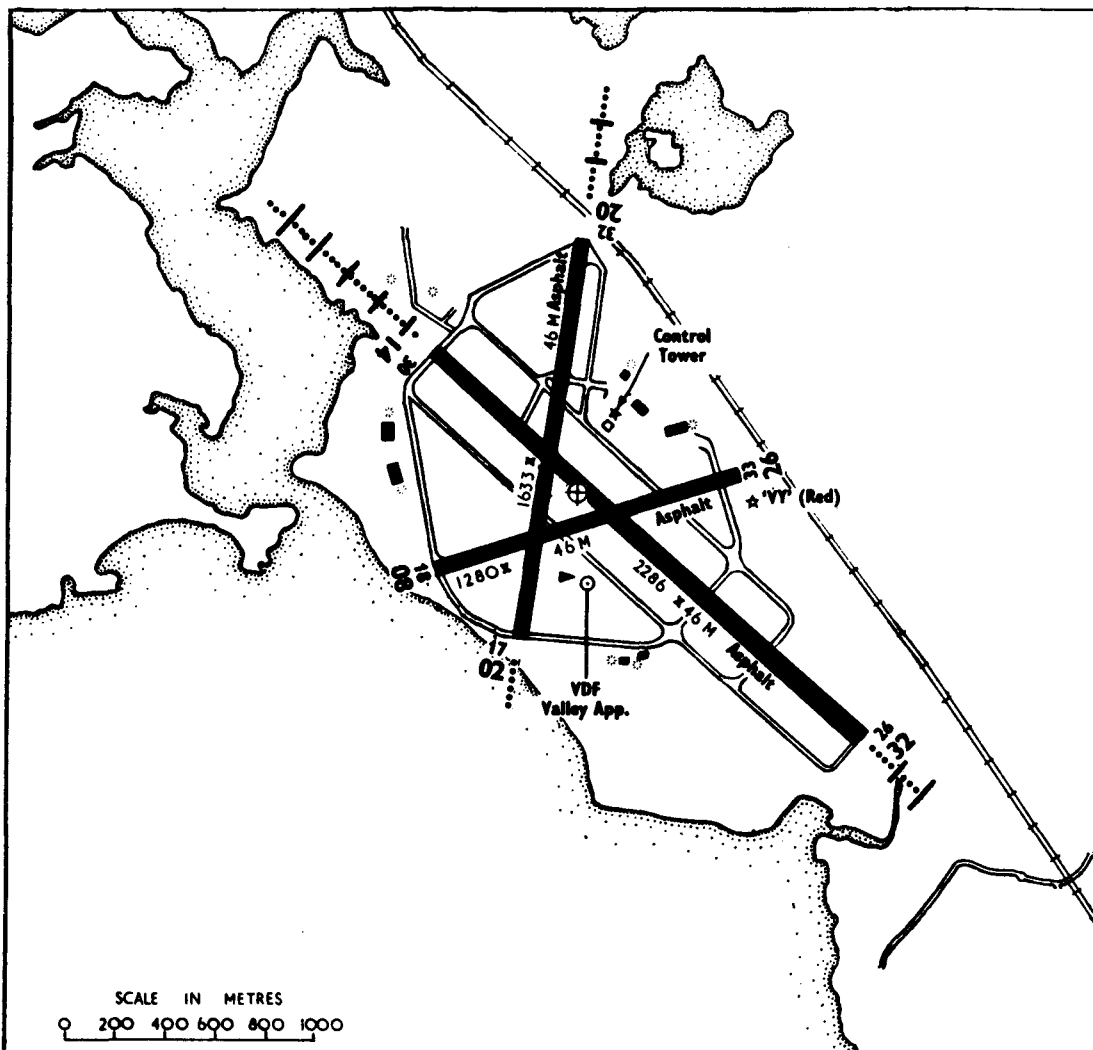
IMMEDIATE TURN RIGHT ONTO 198°
CUMB TO 1500 /246 AT NDB 'TP'

ELEVATIONS (FT)

OFFICIAL AERODROME	287
LOWEST THRESHOLD	254
THRESHOLD R/W 36	254
VARIATION 4°W	Bearings all magnetic
SERIAL No. 50155/1	DATE 15.6.65.

Reason for Re-issue:
COMMS.

NDB R/W 36
TORP



VALLEY AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
02	LOW INT. RED CENTRE-LINE	
20	LOW INT RED CENTRE LINE & BARS	
14	HIGH INT CENTRE-LINE & BARS LOW INT RED TEE	
32	LOW INT CENTRE-LINE & BARS LOW INT RED TEE	
02/20	NIL	

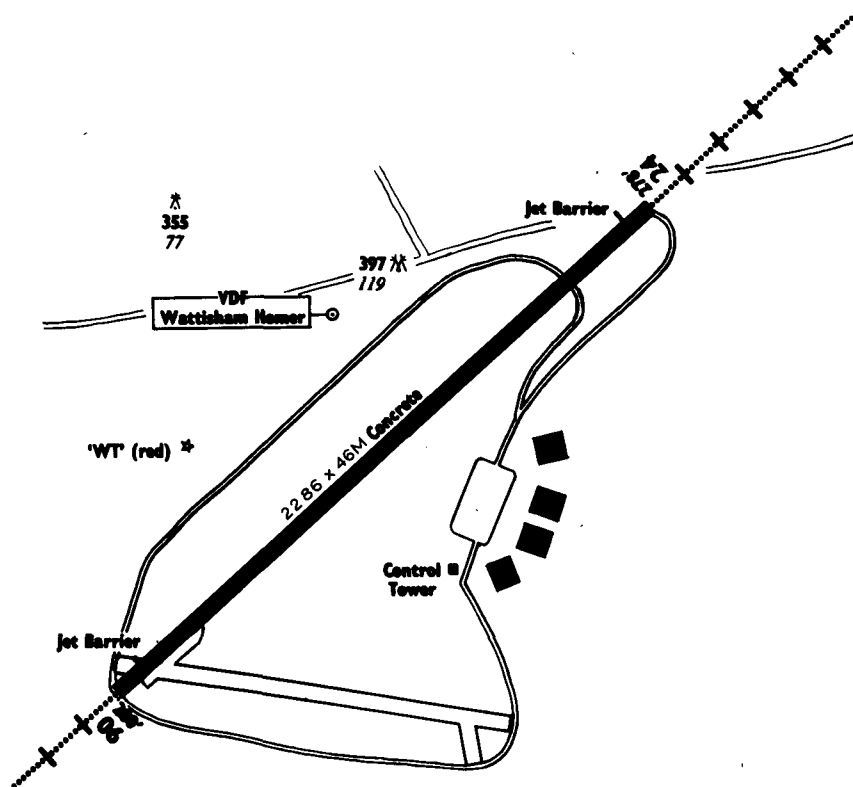
N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
02	NIL	HIGH INT.
20	NIL	HIGH INT.
14	GREEN	HIGH INT.
32	GREEN	HIGH INT.
02/20	NIL	NIL

OBSTRUCTION, TAXIWAY, IDENT BEACON, VY' RED LANDING INDICATOR, FLOODLIGHTS, ANGLE OF APPROACH INDICATORS. R/Ws 14/32, 02/20

ELEVATIONS (FT)	a.m.s.l. above lowest T/H 773
OFFICIAL AERODROME	33
LOWEST THRESHOLD (02)	17
LAT 53° 15' N, LONG 04° 32' W	
VARIATION 10° W	Bearings all magnetic
SERIAL No. 47069	DATE: 28.7.64
Reason for Re-issue:	

AERODROME CHART
VALLEY



SCALE IN METRES
0 200 400 600 800 1000

WATTISHAM AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06/24	HIGH INT. CENTRE-LINE & BARS	X

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06/24	GREEN	HIGH INT. UNI-DIRECTIONAL

OBSTRUCTION, TAXIWAY, IDENT BEACON 'WT' (RED), LANDING INDICATOR.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	284
LOWEST THRESHOLD (24)	278
LAT 52°08' N, LONG 00°57' E	
VARIATION 7°W	Bearings all magnetic
SERIAL No. 46514	DATE: 5.8.64

Reason for Re-issue:

AERODROME CHART
WATTISHAM

SECT. SAFE ALT.
25 NM. 2300'

00°50'E

01°00'E

01°10'E

SECT. SAFE ALT.
25 NM. 2300'

WATTISHAM

ILS R/W 24

BRITISH EUROPEAN AIRWAYS

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APPROACH WATTISHAM APP.

121.5 130.7

TOWER WATTISHAM TWR

121.5

TRANS ALT 3000 FT.

G/S (KTS)	70	90	110	130	150
LLZ TO TURN	6.50	5.20	4.22	3.42	3.12
OM TO T/H	4.17	3.20	2.44	2.18	2.00
DESCENT ON G/P (FT/MIN)	370	475	585	690	795
G/S	170	190	210	230	250
LLZ TO TURN	2.50	2.32	2.17	2.06	1.56
OM TO T/H	1.46	1.35			
DESCENT ON G/P (FT/MIN)	900	1010			

CRITICAL HEIGHTS (FT)

WITH G.P. 200 NO G.P. 300

MISSED APPROACH

Climb ahead to 778 500 then climbing turn left onto 070° to 2900 2622 or as directed by ATC.

ELEVATIONS (FT) :-

OFFICIAL AERODROME	284
LOWEST THRESHOLD	278
THRESHOLD R/W 24	278
VARIATION 7°W	Bearings all magnetic
SERIAL No. 50072	DATE: 3.2.65

Reason for Re-issue:

REDRAWN TO BEA SPECIFICATION

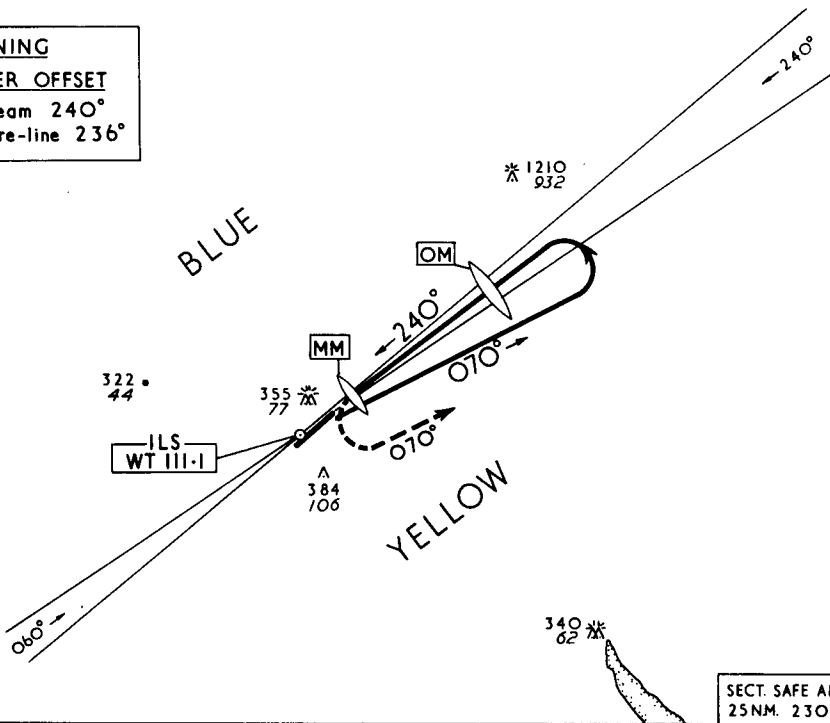
ILS R/W 24

WATTISHAM

WARNING

LOCALIZER OFFSET

Localizer Beam 240°
Runway Centre-line 236°



SECT. SAFE ALT.
25 NM. 2300'

SECT. SAFE ALT.
25 NM. 2300'

LOCALIZER

INITIAL APPROACH
MIN. EN ROUTE ALT.

2900
2622

070°
8 NM.

MM

OM

RATE ONE
TURN LEFT

2228
1950

1928
1650

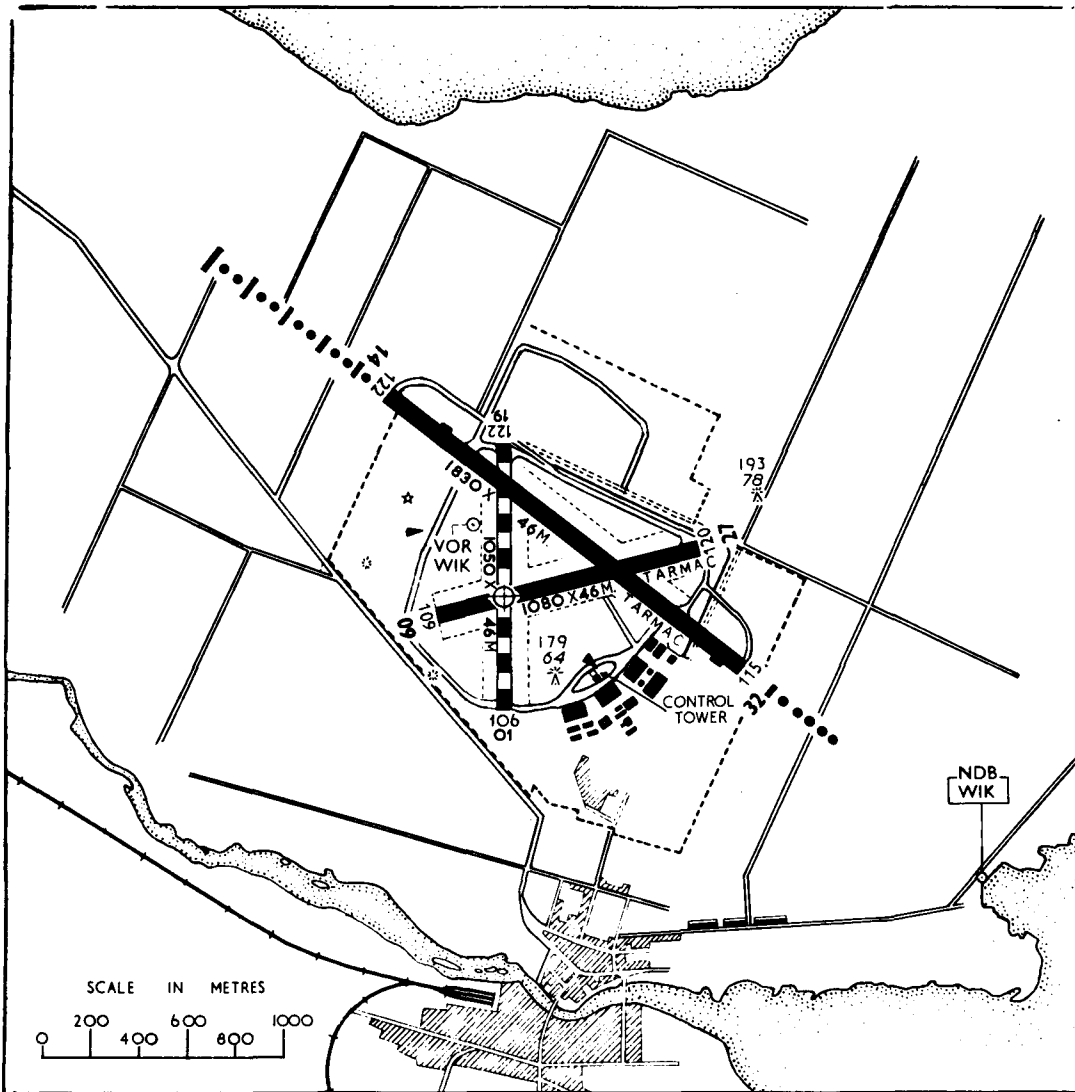
240°

Gr. 3°

0.6 NM.

5 NM.

G.P. HGT. AT MM 488 210



Special Warning R/W. 01/19. This runway is officially closed, but may be used at Captain's discretion and then only by Captains with recent and regular airfield experience. This is because the runway is no longer being maintained, but will be inspected for loose stones, etc., if pilots wish to use it.

Note: R/W 14/32. Marked by red flags during winter months 6 ft. from edge.

R/W Gradients: R/W 09/27; A section 150m. in length, commencing 120m. from SW end rises to NE at 1 in 60. A further section 92m. in length commencing 460m. from SW end rises to NE at 1 in 55.

WICK AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
14	H.I. CENTRE-LINE & BARS. L.I. RED 'T'	3°
32	L.I. RED 'T' & SODIUM BOX	3°

09/27	NIL	
-------	-----	--

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
14	GREEN WITH H.I. WINGS	ELEVATED H.I. UNI-DIRECTIONAL
32	GREEN WITH SODIUM WING BARS	
09/27	NIL	OMNI-DIRECTIONAL

OBSTRUCTION, TAXIWAY, ADME BEACON

ELEVATIONS (FT) : —

OFFICIAL AERODROME	127
LOWEST THRESHOLD (32)	115
LAT 58° 27' N, LONG 03° 06' W	
VARIAION 11° W	Bearings all magnetic
SERIAL No 40101/1	DATE 11.1.65
Reason for Re-issue:	

OBSTRUCTION POSN. CORRECTED.

AERODROME CHART WICK



BRITISH EUROPEAN AIRWAYS

BEA

OPERATIONS MANUAL VOL. IV. A.

AERODROME INFORMATION

AREA 2 NORTHERN EUROPE

The information contained in this part of the Operations Manual is for use in connection with BEA operations in accordance with the standards, limitations and procedures established by BEA. The information should not therefore be used in connection with the operations of any other person or organization.

ALL R/W's ARE CONCRETE

SCALE IN METRES
500 0 500 1000 1500

NOTE
R/W 09R/27L NOT AVAILABLE
TO CIVIL AIRCRAFT

STOPWAYS
R/W 27R 275 M CONCRETE
R/W 09L 65M CONCRETE

ARRESTER NETS
IN OVERRUN AREAS 132 M FROM THRESHOLD
R/W 09L & 125M FROM THRESHOLD R/W 27R

AALBORG AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
09L	WHITE H.I. CENTRE-LINE & 1 BAR	
27R	WHITE H.I. CENTRE-LINE & 5 BARS	
15	WHITE H.I. CENTRE-LINE	
04/22	NIL	
33	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
09L	GREEN	HIGH & LOW INT
27R		
15		LOW INT
04/22		LOW INT ELEVATED
33		LOW INT

OBSTRUCTION, TAXIWAY, ADME BEACON

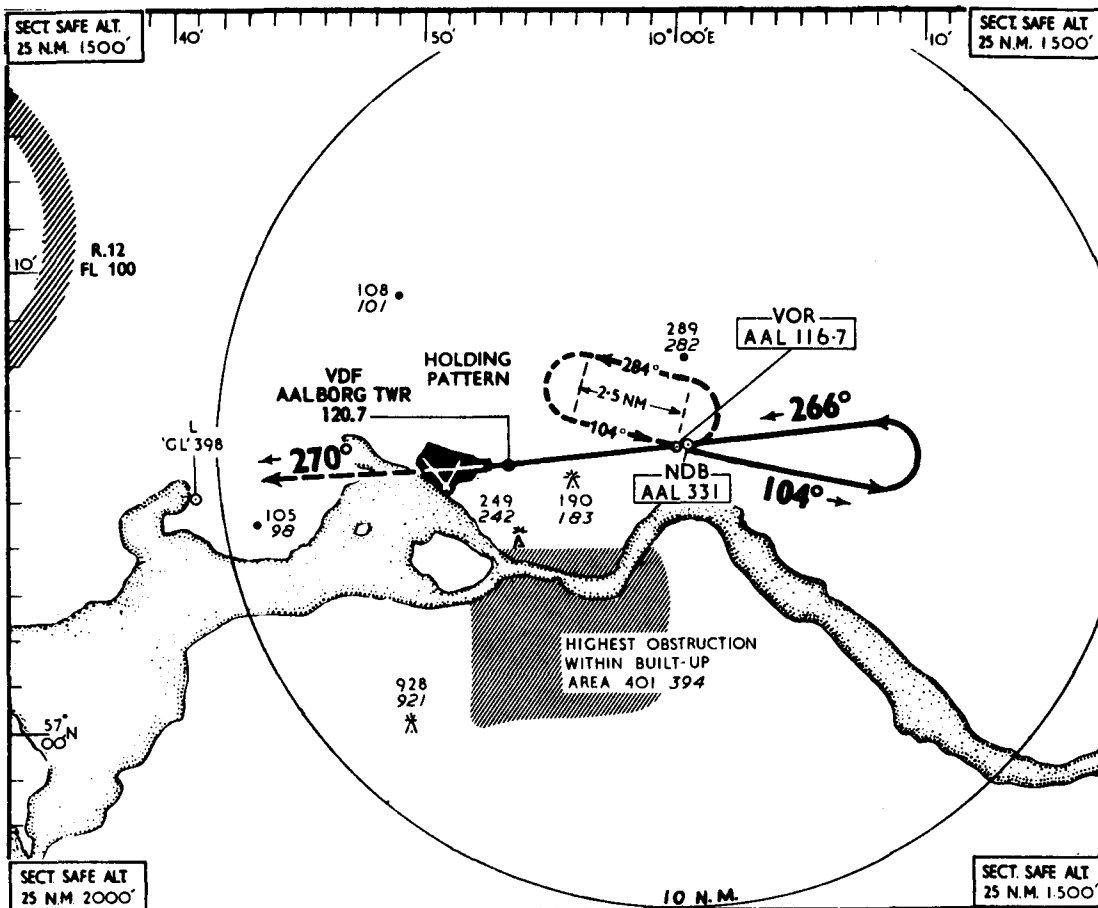
ELEVATIONS (FT) :—

OFFICIAL AERODROME	10
LOWEST T/H 04, 09L, 33	7
LAT 57° 06' N, LONG 09° 51' E	
VARIATION 3° W	Bearings all magnetic
SERIAL No. 40313/1	DATE: 2-12-64

Reason for Re-issue:

PRESENTATION

AERODROME CHART AALBORG



AALBORG

VOR or NDB R/W 27

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APPROACH AALBORG TOWER

120.7 121.5E

TOWER AALBORG TOWER

118.3 121.5E

TRANS ALT 2000 FT.

G/S (KTS)	70	90	110	130	150
NDB TO ADME	3.33	2.46	2.16	1.55	1.40
VOR TO ADME	3.24	2.38	2.09	1.49	1.35
G/S	170	190			
NDB TO ADME	1.28	1.19			
VOR TO ADME	1.23	1.15			

CRITICAL HEIGHTS (FT)

VOR 400 NDB 450

OVERSHOOT

CLIMB TO 1500 /493 ON 270°
OR AS INSTRUCTED.

ELEVATIONS (FT): —

OFFICIAL AERODROME 10

LOWEST THRESHOLD 7

THRESHOLD R/W 27 10

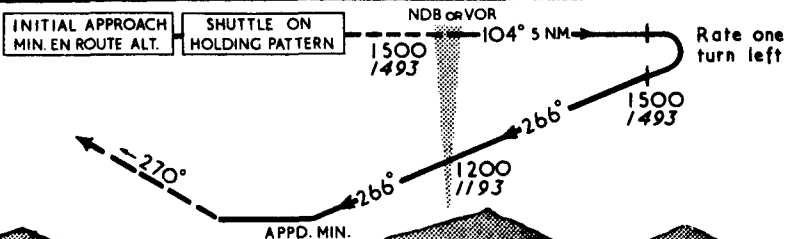
VARIATION 3°W Bearings all magnetic

SERIAL No. 40376/2 DATE: 10.2.65

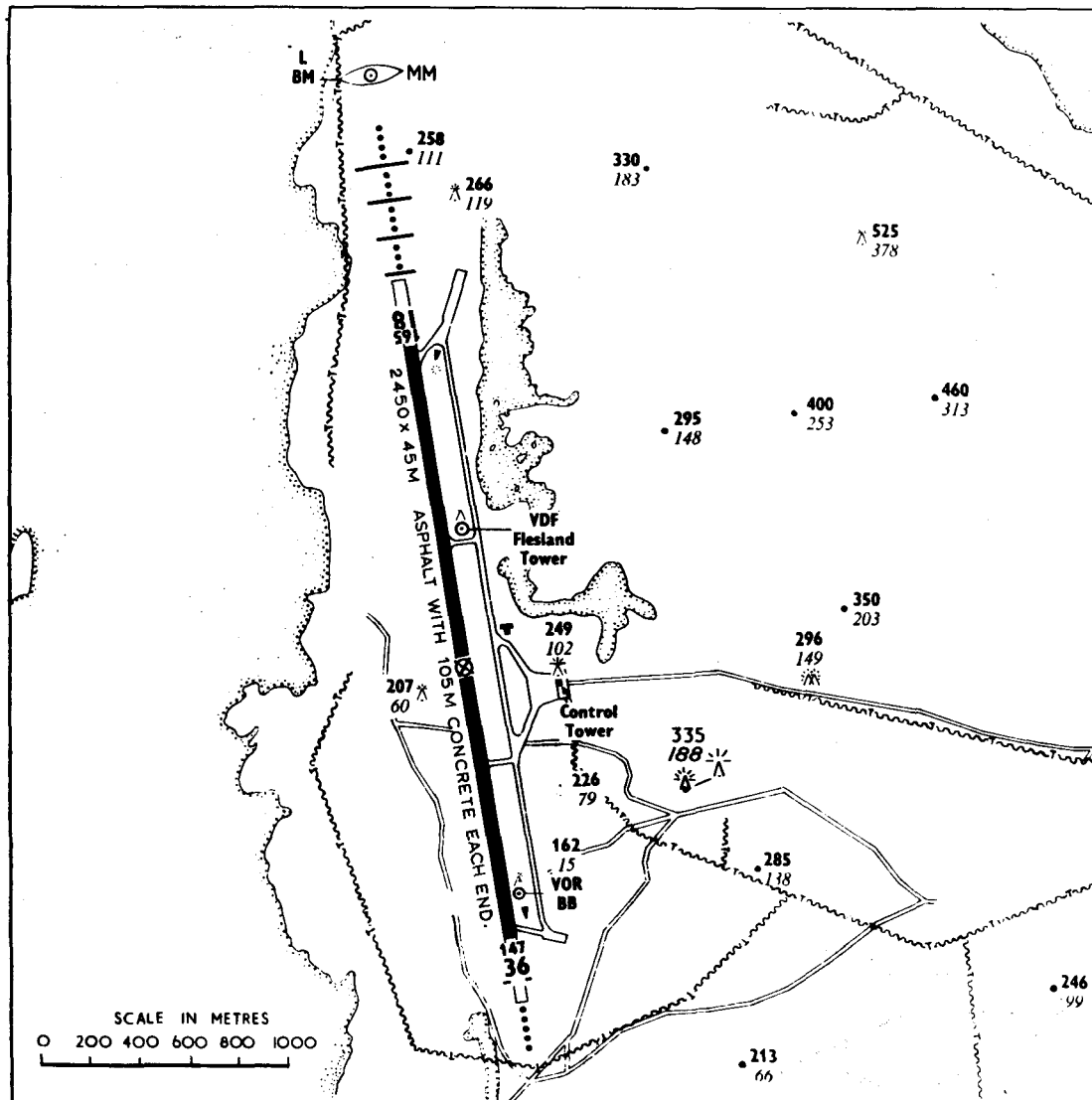
Reason for Re-issue:

L 'GL' ADDED.

VOR or NDB R/W 27
AALBORG



W 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 NM E



SCALE IN METRES
0 200 400 600 800 1000

STOPWAYS: 270M. CONCRETE EACH END.
JET BARRIER: 52M. OUT FROM EACH THRESHOLD.

BERGEN AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
18	VAR. INT. WHITE C/L & 4 BARS WITH WING-BAR TRIPLE RED ROW IN LAST 275M.	
36	VAR. INT. WHITE C/L WITH WINGBAR & DOUBLE RED ROW IN LAST 275M.	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
18	GREEN	VAR. INT. ELEVATED WHITE WITH AMBER EACH END
36		

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, STOPWAY (RED).

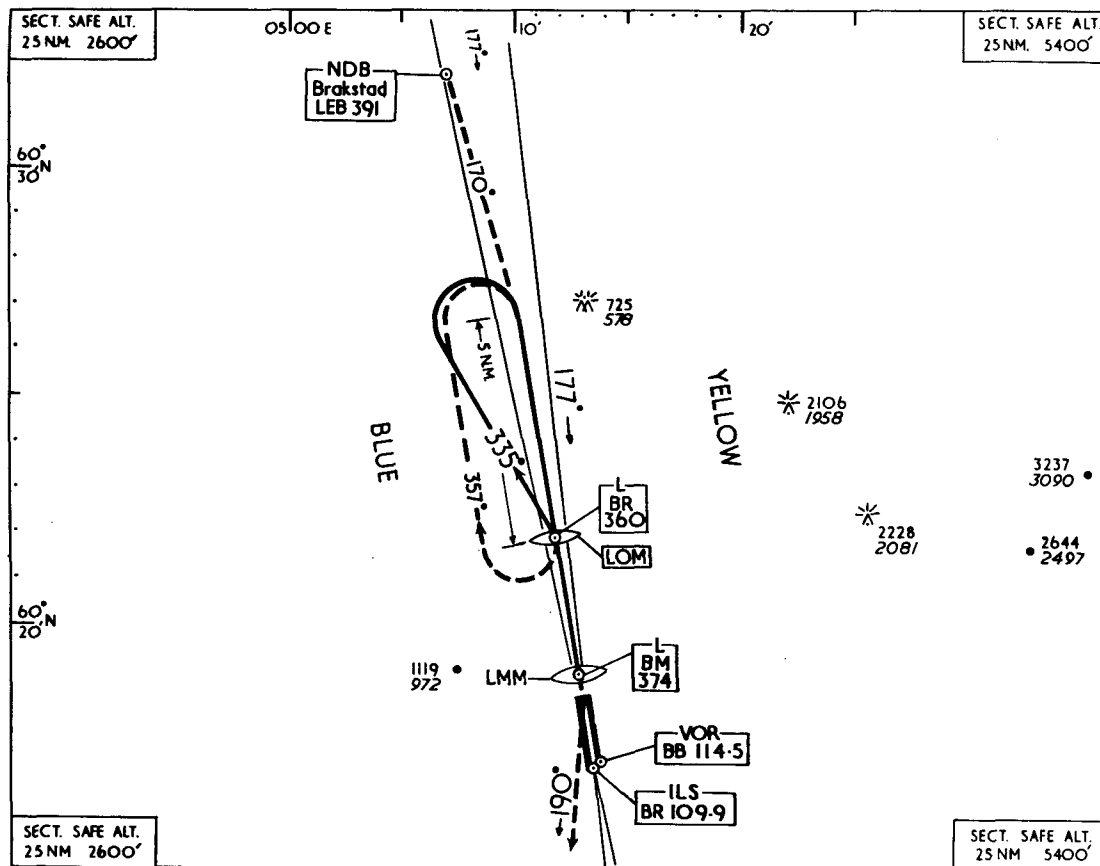
ELEVATIONS (FT)

OFFICIAL AERODROME	165
LOWEST THRESHOLD (36)	147
LAT 60° 18' N. LONG 05° 13' E	
VARIATION 6° W	Bearings all magnetic
SERIAL No. 50375/1	DATE: 11. 6. 65.

Reason for Re-issue:

R/W LENGTH

AERODROME CHART BERGEN



BERGEN ILS R/W 18

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APPROACH

TOWER

FLESLAND TWR.

121.0 119.1

TRANS ALT

3500

FT.

G/S (KTS)	70	90	110	130	150
LOM TO TURN			2.44	2.19	2.0
LOM TO T/H	3.16	2.32	2.04	1.46	1.32
DESCENT ON G/P (FT/MIN)	380	490	600	710	820
G/S	170	190	210	230	250
LOM TO TURN	1.46	1.35	1.26	1.18	1.12
LOM TO T/H	1.21	1.12			
DESCENT ON G/P (FT/MIN)	930	1040			

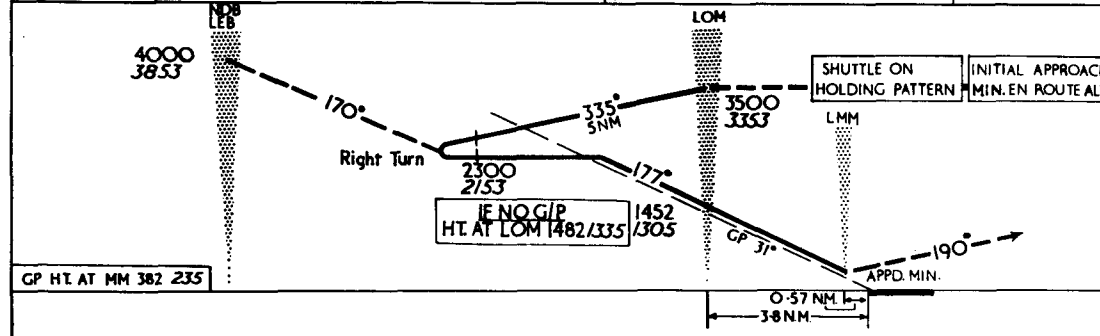
MISSED APPROACH

Climb on 190° to 3000 2853.

ELEVATIONS (FT)

OFFICIAL AERODROME	165
LOWEST THRESHOLD	147
THRESHOLD R/W 18	165
VARIATION b°W	Bearings all magnetic
SERIAL No 50155/1	DATE 13.6.65.
Reason for Re-Issue:	
FEED FROM LEB ADDED.	

ILS R/W 18 BERGEN



BERLIN (TEGEL) AERODROME CHART

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
08R	WHITE STROBE UNI-DIRECTIONAL CENTRE-LINE.	X
26L	HIGH INT WHITE WITH 4 BARS & WING BARS. WHITE STROBE CENTRE-LINE. HIGH INT. RED PRECEDING NIGHT LANDING THRESHOLD (ALL UNI-DIRECTIONAL)	X

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08R	HIGH INT. BI-DIRECTIONAL GREEN	HIGH INT BI-DIRECTIONAL WHITE.
26L	HIGH INT. BI-DIRECTIONAL GREEN UNI-DIRECTIONAL GREEN FLASHING.	HIGH INT BI-DIRECTIONAL WHITE.

TAXIWAY, ADME BEACON

ELEVATIONS (FT) :—

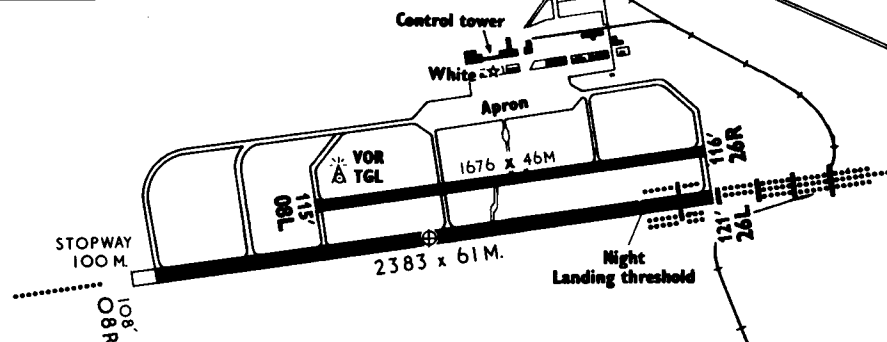
OFFICIAL AERODROME	115
LOWEST T/H [08R]	108
LAT 52° 34' N, LONG 13° 18' E	
VARIATION 2° W Bearings all magnetic	
SERIAL No. 50011/1	DATE : 5.2.65
Reason for Re-issue:	

ADME. REF. POINT

AERODROME CHART BERLIN (TEGEL)

R/W Availability

R/W 26L Landing 2208m.
(first 275m. not available) } including stopway
Take-off 2483m. } 100m.
R/W 08R Take off 2108m.
(last 275m. stopway only).



207
99
A

235
127
A

SCALE IN METRES

0 200 400 600 800 1000

SECT. SAFE ALT.
25 N.M. 2000

SECT. SAFE ALT.
25 N.M. 2000

BERLIN (TEGEL) VOR/ILS R/W 26L

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APPROACH	BERLIN	CONTROL
120.4	120.95	125.5 125.3 125.8 121.5 E
TOWER	TEGEL	AIRPORT
119.7	118.7	125.5 121.5 E
TRANS ALT	2000	FT.

G/S (KTS)	70	90	110	130	150
VOR TO TURN	6.26	5.00	4.06	3.28	3.00
OM TO T/H	2.00	1.33	1.16	1.05	0.56
DESCENT ON G/P (FT/MIN)					
G/S	170	190	210	230	250
VOR TO TURN	2.39	2.22	2.09	1.57	1.48
OM TO T/H	0.49	0.44			
DESCENT ON G/P (FT/MIN)					

CRITICAL HEIGHTS (FT)

WITH G/P 200 NO G/P 300

OVERSHOOT

CLIMBING TURN RIGHT TO NDB 'DLs' (OR AS DIRECTED TO NIEDER INT) AT 2000
1092 & CALL BERLIN CTL.

ELEVATIONS (FT)

OFFICIAL AERODROME	115
LOWEST THRESHOLD	108
THRESHOLD R/W 26L	121

VARIATION 2° W Bearings all magnetic
SERIAL No. 40236/1 DATE: 5.2.65

Reason for Re-issue:
ILS IDENT.

VOR/ILS R/W 26L
BERLIN (TEGEL)

40°

52°
30' N

Note
All holding patterns are
min. alt. 2000, 2.5nm.

NO BACK
BEAM

Warning
ILS Ref. Pt. located
107M. east of Night
Landing Threshold

If VOR u/s commence
procedure at NDB 'GL'

ILS
TL 109.3

NDB
Luebars
DLs 413.5

Schoenwalde

NDB
Tegel West
RW 448

VOR
TGL 111.2

NDB
Tegel East
GL 321.5

BLUE
← 243°
YELLOW
← 263°

Nieder

NDB
Gatow
GW 370.5

VAR
Havel
DIB 110.1

VOR
Havel
HVL 113.3

NDB
Kilo
DIN 484

1030
922

NDB
Gatow
GW 370.5

VAR
Havel
DIB 110.1

VOR
Havel
HVL 113.3

NDB
Kilo
DIN 484

1030
922

NDB
Gatow
GW 370.5

VAR
Havel
DIB 110.1

VOR
Havel
HVL 113.3

NDB
Kilo
DIN 484

1030
922

NDB
Gatow
GW 370.5

VAR
Havel
DIB 110.1

VOR
Havel
HVL 113.3

NDB
Kilo
DIN 484

1030
922

NDB
Gatow
GW 370.5

VAR
Havel
DIB 110.1

VOR
Havel
HVL 113.3

NDB
Kilo
DIN 484

1030
922

VOR
TGL

3000
2892

063° 7.5NM

Rate 1/2 turn
right

2608
2500

263°
G.P. 2.4°

870
762

G.P. height at MM 335 227

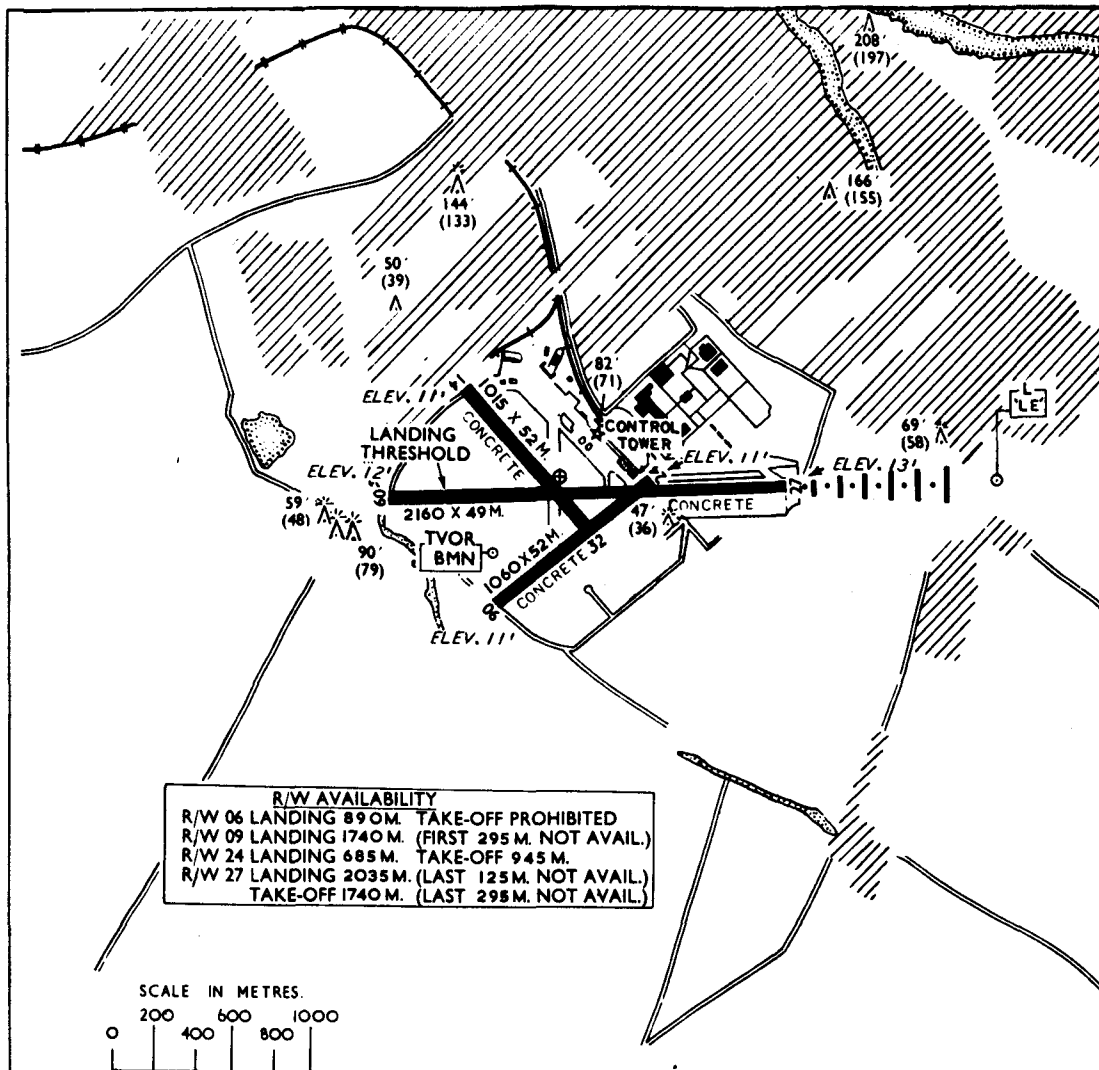
Appd. min.

MM OM

W
N.M. 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 N.M.

SECT. SAFE ALT.
25 N.M. 2100

SECT. SAFE ALT.
25 N.M. 2100



BREMEN **AERODROME CHART**

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
 ICAO
 Type
 Angle

R/W	APPROACH	
27	H.I. WHITE UNI-DIRECTIONAL AND OMNI-DIRECTIONAL CENTRE-LINE & BARS	
09	NIL	X

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
27	H.I. GREEN	WHITE H.I. UNI-DIR. & OMNI-DIR. LAST THIRD YELLOW.
09	H.I. GREEN WINGBARS	

OBSTRUCTION, TAXIWAY, ADME BEACON
 WIND INDICATOR.

ELEVATIONS (FT) : —

OFFICIAL AERODROME	11
LOWEST THRESHOLD (08 24)	11
LAT 53° 03' N, LONG 08° 47' E	
VARIATION 4° W Bearings all magnetic	
SERIAL No. 31502/1	DATE: 24.2.65

Reason for Re-Issue:

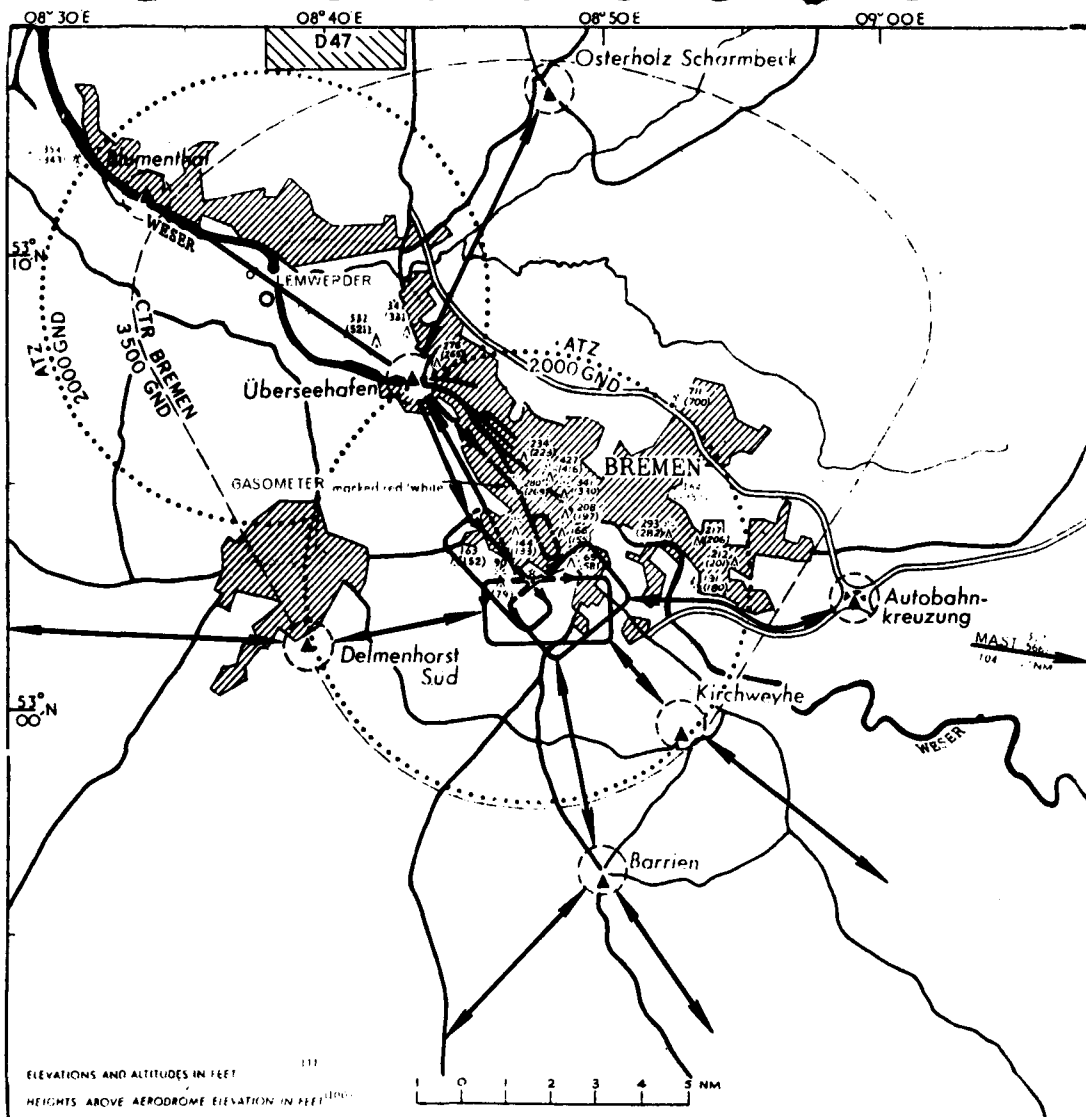
MINOR

AERODROME CHART **BREMEN**

NOTES: 1. CIRCLING APPROACHES TO BE MADE S. OF A/D.

2. A/C TAKING OFF FROM R/W 32 WILL TURN ONTO 345 (M) AS SOON AS POSSIBLE AND WILL MAINTAIN THIS COURSE UNTIL CROSSING THE RAILWAY TRACKS 1° 08' N.W. OF A/D. THIS WILL AVOID OVERFLYING BUILT-UP AREA.

STOPWAYS R/W 06 60M. R/W 24 115M. R/W 27 125M.



BREMEN

VISUAL ROUTES CHART

BRITISH EUROPEAN AIRWAYS

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PROCEDURES

- Approach is permitted only via the compulsory reporting points shown on the chart. Radio contact with Aerodrome Control Service shall be established at a distance of 15 NM from the aerodrome.
- Aircraft without functioning two-way radio must obtain
 - a permission for landing from "Senator für Hafen, Schifffahrt und Verkehr", Bremen and
 - a permission for approach from Bremen Aerodrome Control Service prior to commencing flight.
 Approach shall be executed via position "Überseehafen" avoiding the overflying of the city (shown thus ———) Clearance to leave the holding pattern and to enter the traffic circuit will be given to aircraft without radio equipment by light signals. The routing for aircraft without radio is also mandatory for helicopters.
- When entering the aerodrome traffic zone and with the holding pattern for aircraft without radio equipment a height of 1000 feet AGL shall be maintained.
- Strict adherence to the approach and departure routes and to the traffic circuits is mandatory for reasons of noise abatement. In particular, the departure route for RWY 32, shown on the chart, must be observed.

ELEVATIONS (FT) : —

OFFICIAL AERODROME

II

VARIATION 4°W | Bearings all magnetic

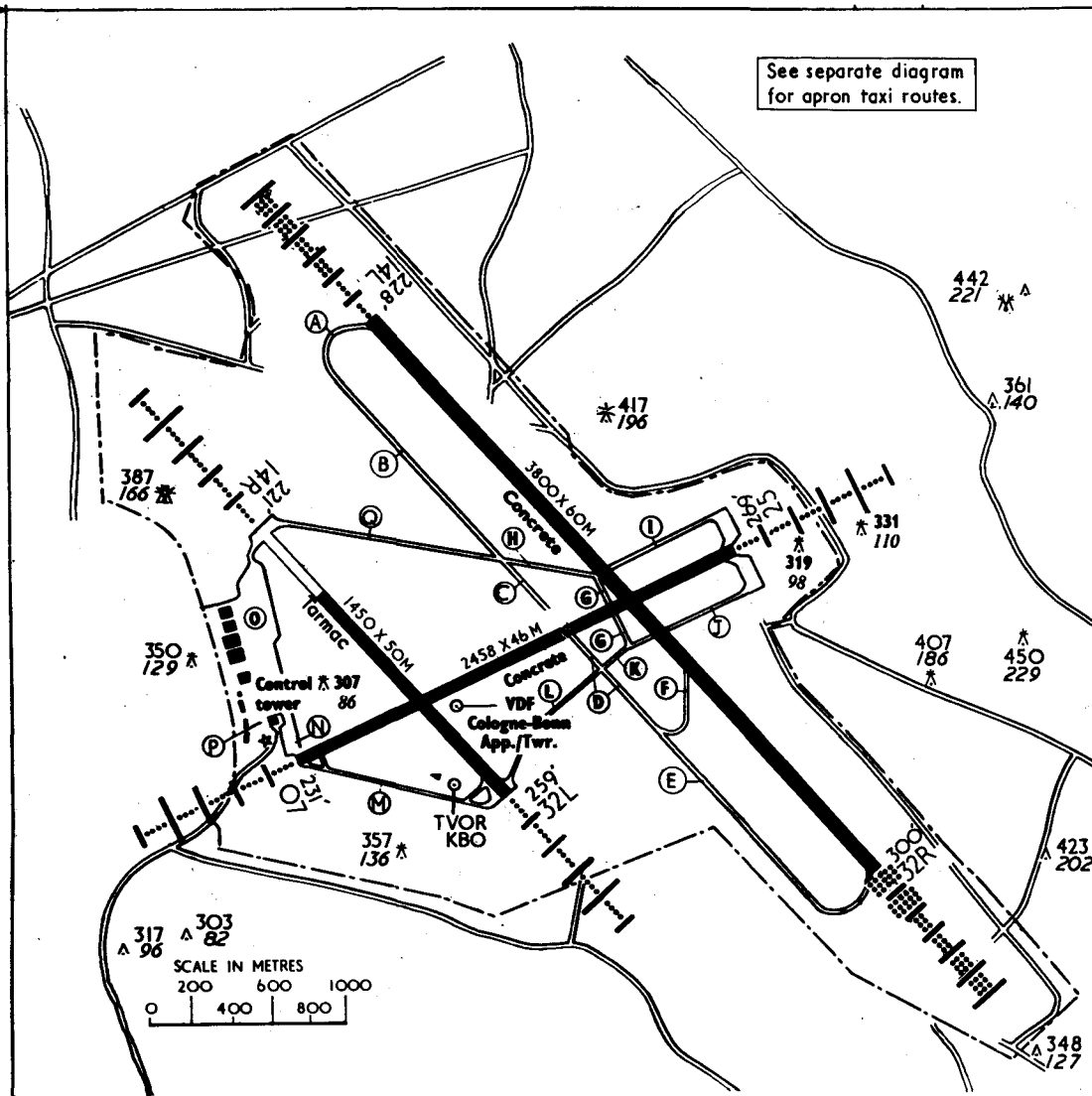
SERIAL No. 40038/1 | DATE: 24.2.65

Reason for Re-Issue:

MINOR

VISUAL ROUTES CHART

BREMEN



- NOTE**
1. LIGHTS ON EDGES OF R/W'S & TAXIWAYS ARE ELEVATED.
 2. TAXIING OF JET A/C WITH MORE THAN 2 ENGINES IS PROHIBITED BETWEEN THRESHOLD R/W 07 VIA THRESHOLD R/W 32L TO THRESHOLD R/W 25.
 3. R/W 07/25 SLIPPERY WHEN WET.

STOPWAYS: R/W'S 14L & 32R, 60M. BITUMEN.
TAXIWAYS: 1. T/WAY G. J. K. L. M. PPO.
 2. ACCESS TO R/W 14L/32R VIA T/WAY O ONLY.

COLOGNE-BONN AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
14L 32R 14R 32L 07 25	H.I. (VAR.) WHITE UNI- & L.I. RED OMNI-DIRECTIONAL CENTRE-LINE & BARS	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
14L 32R	H.I. & L.I. GREEN GREEN WINGS	H.I. (VAR) UNI. & L.I. OMNI-DIR WHITE WITH WHITE WING BARS 305M. UP R/W, ALSO WHITE C/L R/W 32R HAS WHITE FLUSH BARS ALONG FIRST 1280M.
14R 32L	L.I. GREEN	H.I. (VAR) UNI. & L.I. OMNI-DIR.
07 25	L.I. GREEN	FIRST 650M. R/W 07 H.I. (VAR) UNI. & L.I. OMNI-DIR WHITE. REMAINING 1800M OF R/W L.I. WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON

ELEVATIONS (FT): -

OFFICIAL AERODROME	300
LOWEST THRESHOLD (14R)	221
LAT 50° 52' N. LONG 07° 09' E	
VARIATION 4° W	Bearings all magnetic
SERIAL No. 50369/2	DATE 15. 6. 65.

Reason for Re-issue:

R/W LENGTHS & OBSTRUCTIONS.

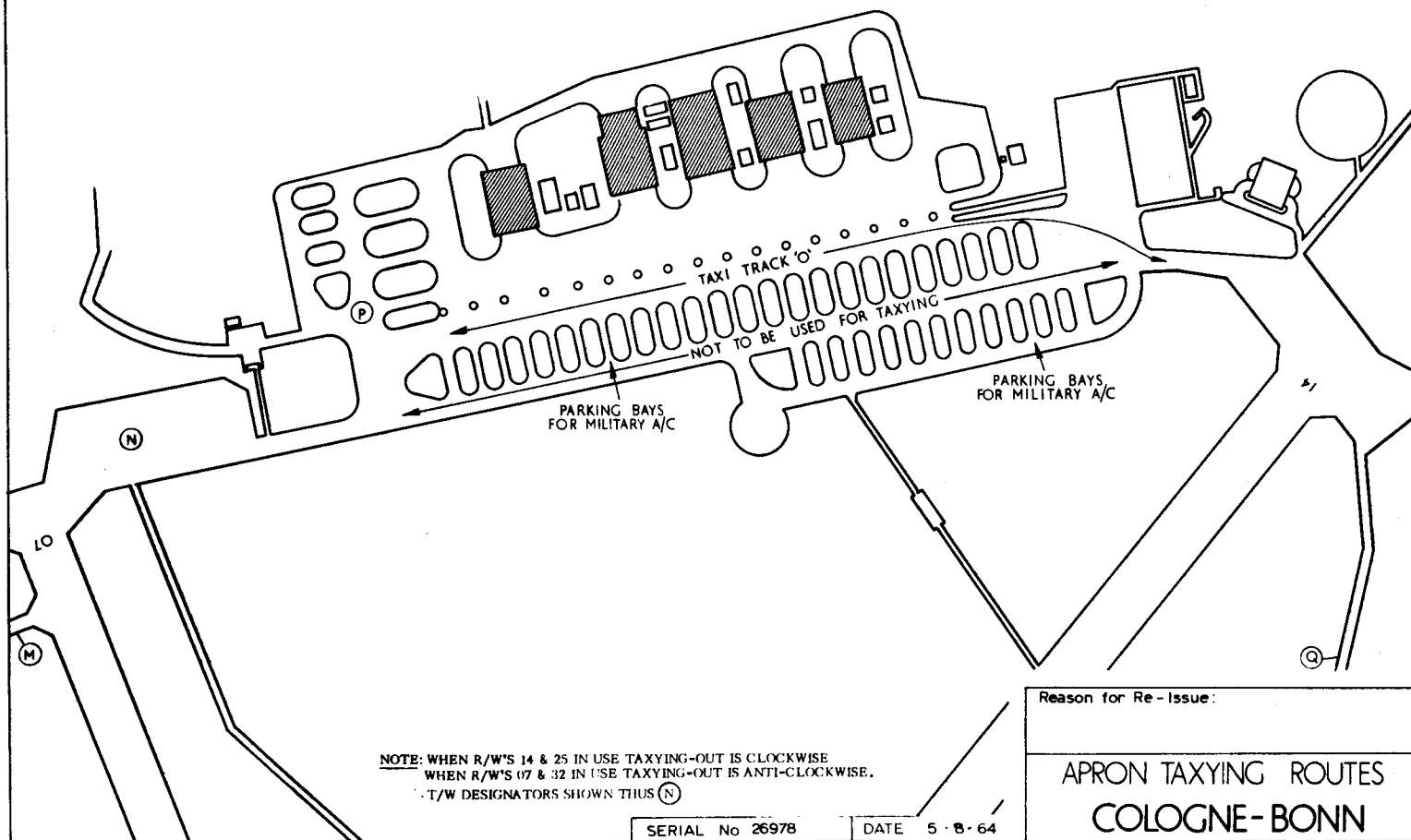
AERODROME CHART COLOGNE-BONN

COLOGNE-BONN APRON TAXYING ROUTES

BRITISH EUROPEAN AIRWAYS

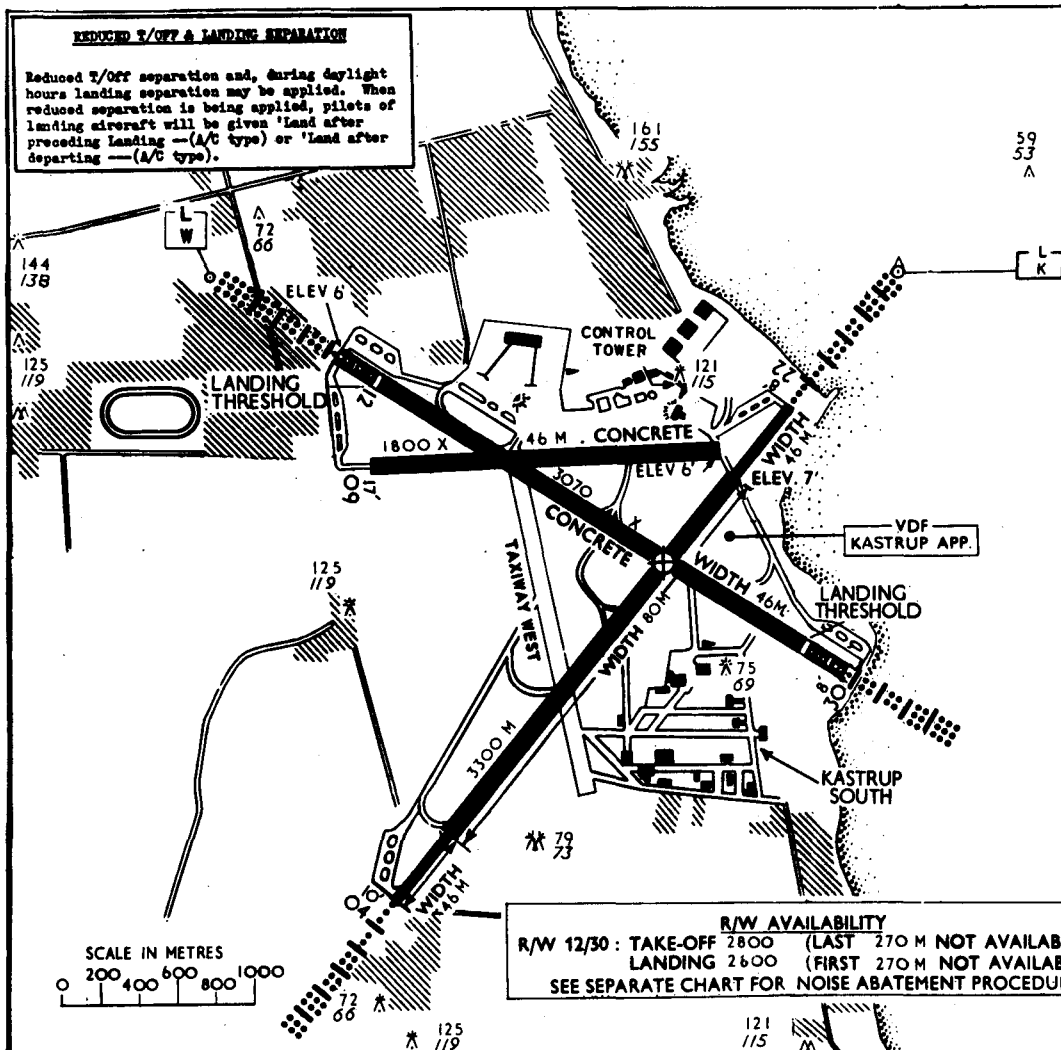
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REDUCED T/OFF & LANDING SEPARATION

Reduced T/OFF separation and, during daylight hours landing separation may be applied. When reduced separation is being applied, pilots of landing aircraft will be given 'Land after preceding landing' — (A/C type) or 'Land after departing' — (A/C type).



R/W AVAILABILITY

R/W 12/30 : TAKE-OFF 2800 (LAST 270M NOT AVAILABLE)
LANDING 2800 (FIRST 270M NOT AVAILABLE)
SEE SEPARATE CHART FOR NOISE ABATEMENT PROCEDURES

COPENHAGEN (KASTRUP) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI Type Angle
12/30	WHITE H.I. & L.I. CENTRE-LINE & BARS	R/W 30 2-75°
09/27	NIL	
04/22	H.I. & L.I. CENTRE-LINE & BARS	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
12/30	H.I. & L.I. GREEN & WINGBARS 270M UP R/W.	WHITE H.I. & L.I. OMNI-DIRECTIONAL FIRST & LAST 270M. RED.
09/27	NIL	WHITE L.I. OMNI-DIRECTIONAL.
04/22	H.I. & L.I. GREEN & WHITE WING-BARS	WHITE H.I. UNI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL, WIDTH 46M.

OBSTRUCTION, TAXIWAY, ADME BEACON

ELEVATIONS (FT): —

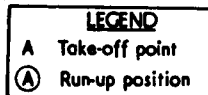
OFFICIAL AERODROME	17
LOWEST THRESHOLD (27)	6
LAT 55° 37' N, LONG 12° 39' E	
VARIATION 2° W Bearings all magnetic	
SERIAL No. 50112/2	DATE: 7.7.65
Reason for Re-issue:	
R/W WIDTHS & OBSTRUCTIONS	

AERODROME CHART COPENHAGEN (KASTRUP)

NOTE: SEE SEPARATE CHARTS FOR DETAILED LIGHTING PLANS INCLUDING TOUCHDOWN ZONES.

WARNING: AFTER HEAVY RAIN SHOWERS OR SUDDEN THAW, WATER COLLECTS IN POOLS ON R/W 09/27 & R/W 04/22

THE TAXIWAYS ON SOUTHERN PART OF THE AERODROME (KASTRUP SOUTH) ARE ONLY 15M. WIDE AND THE SAFETY ZONE OF THESE TAXIWAYS IS 23M EITHER SIDE OF CENTRE-LINE.



TAKE OFF		DISTANCES AVAILABLE	
R/W 12 from A	3070M.	R/W 30	3070M.
R/W 12 from B	2400M.		
R/W 12 from C	2000M.		
R/WO4 from A	3300M.	R/W 22	3300M.
R/WO4 from B	2500M.		
R/WO4 from C	1900M.		

NOISE ABATEMENT PROCEDURES COPENHAGEN

A. TAKE-OFF

1. R/W 09/27. Prohibited to jet aircraft
2. R/W 12. Heavy jet and long range piston aircraft commence take-off run at position B. Other aircraft at position C.
NOTE: Piston aircraft run up area is at position D on R/W 09/27.
3. R/W's 22, 30. Heavy jet aircraft commence take-off at end of R/W.
4. R/W 04. Heavy jet and piston aircraft take-off at position A. Other aircraft at position B or C.
5. Preferential Runways.

(b) Other aircraft up to a cross-wind component of 15 knots. R/W's 09/27 are not normally used for take-off by piston engine a/c unless the cross-wind component on other runway exceeds 15 knots.

NOTE: Heavy jet aircraft may request permission to take-off from position A on R/W 12 if conditions do not allow for take-off on R/W's 04 & 22 or position B on R/W 12.

B. LANDING

1. R/W 09. Prohibited to jet aircraft.
2. R/W's 04 & 22. To used by aircraft of DC3 type and larger when cross-wind component does not exceed 15 knots.
3. R/W 12. May be used if landing minima for R/W 04 is below specified figures.

SERIAL No. 50112/2	DATE : 7.7.65
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Reason for Re-Issue:

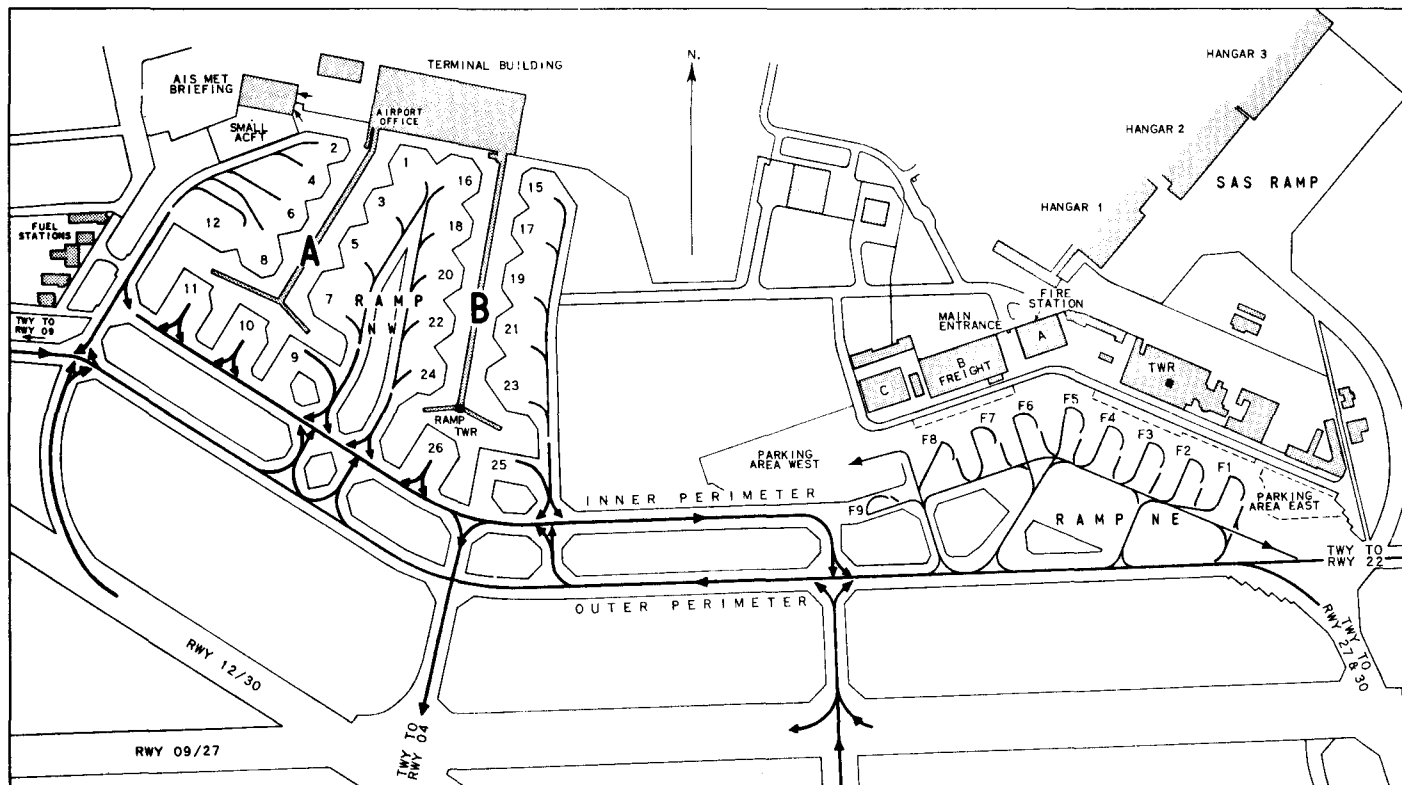
PRESENTATION

NOISE ABATEMENT PROCEDURES COPENHAGEN

BRITISH EUROPEAN AIRWAYS

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TAXIING TO AND FROM RAMPS

- 1 Inbound aircraft will be advised of number of parking point by KASTRUP TAXI (KASTRUP TWR) on RTP.
Aircraft will be guided to the parking point by marshallsers.
- 2 Departing aircraft shall state their number of parking point when requesting taxi clearance from KASTRUP TAXI (KASTRUP TWR).
Aircraft departing from RAMP NE will normally contact KASTRUP TWR before departure.
Departing aircraft will normally leave the parking points without the guidance by marshallsers.

Note: Pilots are requested to follow the routes marked by day and night markings as close as possible.

TAXIING GUIDANCE SYSTEM

Inbound traffic to RAMP: Yellow signboards with black lettering
Outbound traffic to runways: White signboards with blue lettering
Warnings at intersections: White signboards with red lettering

LIGHTING OF VEHICLES

Official vehicles on the movement area are marked with lights as follows:

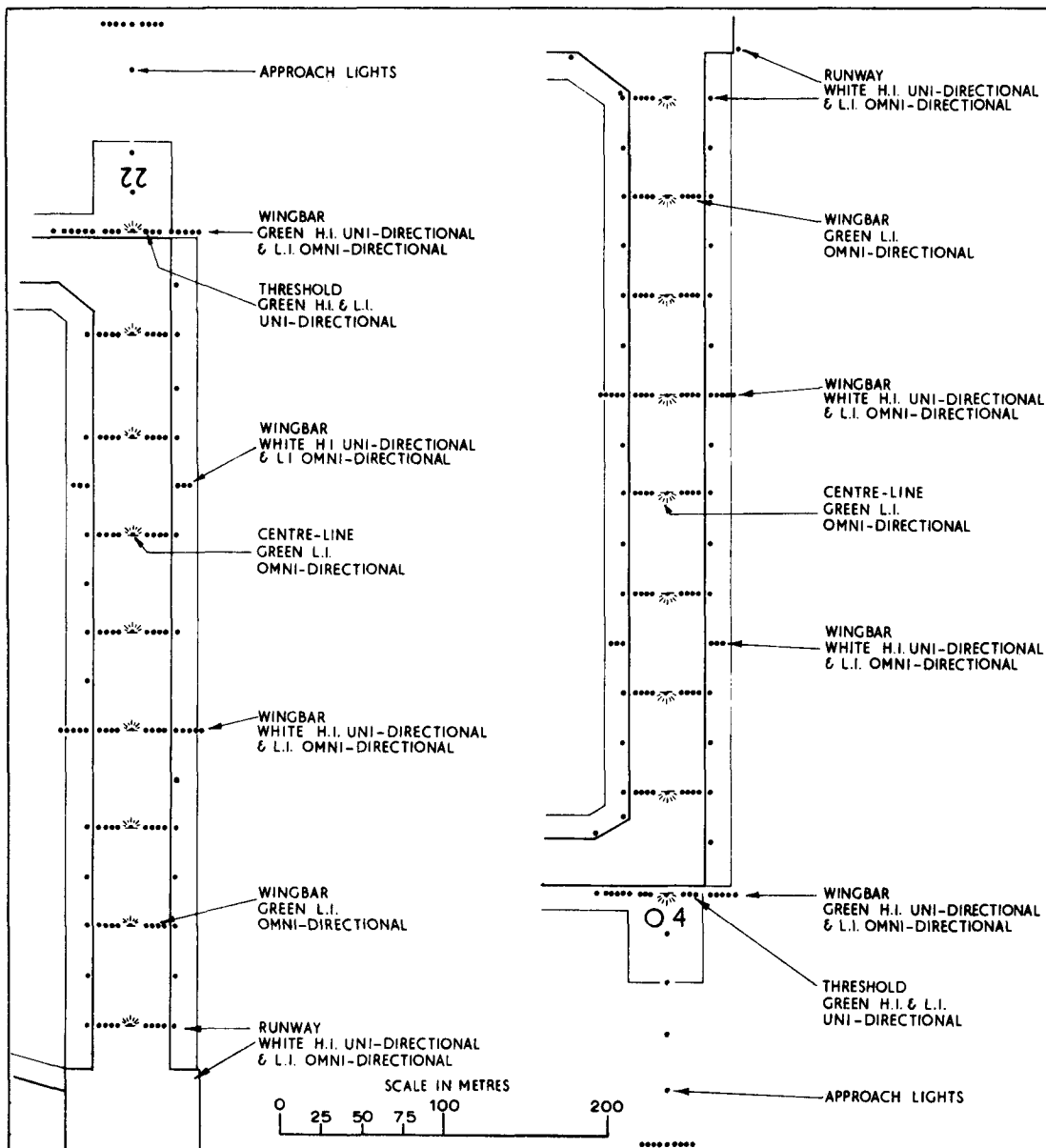
- 1 Rescue and fire fighting vehicles: Blue flashing light
- 2 "FOLLOW-ME" vehicles: White flashing light
- 3 Other vehicles: Yellow flashing light

SERIAL No. 50112/2 DATE: 19-2-65

Reason for Re-Issue:

COMPLETE REVISION

**APRON LAYOUT
COPENHAGEN (KASTRUP)**



COPENHAGEN (KASTRUP)

R/W 04 & 22 LIGHTING

BRITISH EUROPEAN AIRWAYS

BEA

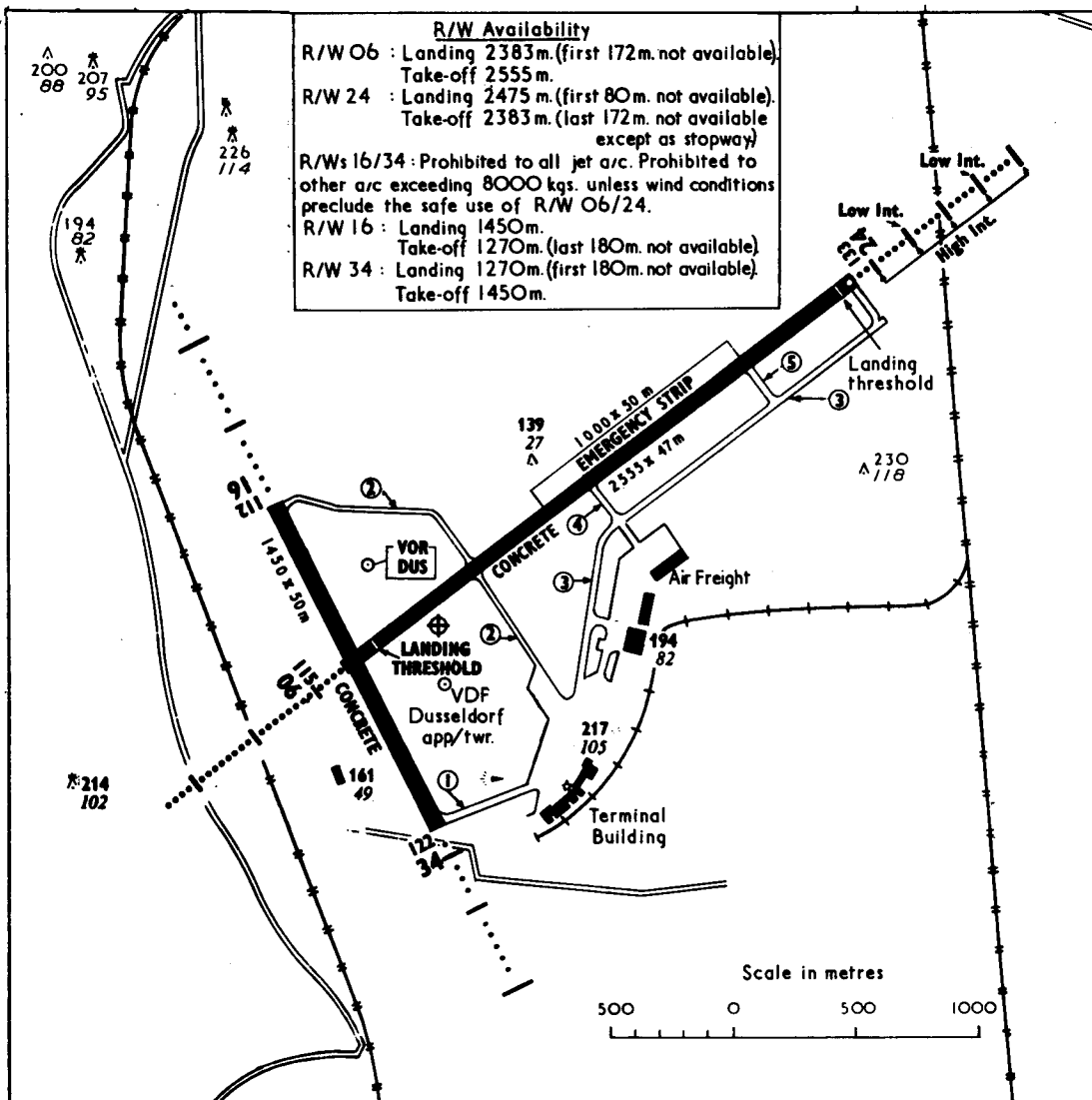
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SERIAL No. 40059

DATE: 25.8.64

Reason for Re-issue:

R/W 04 & 22 LIGHTING
COPENHAGEN (KASTRUP)



DUSSELDORF AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI (CAO Type Angle
06, 24	H.I. WHITE C/L & SODIUM BARS L.I. RED C/L & BARS	3° R/W 06
16	L.I. RED C/L & BARS	
34	L.I. RED C/L & BARS	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06, 24	H.I. & L.I. GREEN	H.I. WHITE WITH WING-BARS AT 300M. FROM DISPLACED THRESHOLD
16	L.I. GREEN	H.I. WHITE (EXCEPT LAST 180M) WITH WING BARS AT 300M. ALONG R/W.
34	L.I. GREEN WINGS 180M. UP R/W	H.I. WHITE (EXCEPT FIRST 180M) WITH WING BARS AT 300M. FROM DISPLACED THRESHOLD. FIRST 180M. AMBER.

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR.

ELEVATIONS (FT) :—

OFFICIAL AERODROME 133

LOWEST THRESHOLD (16) 112

LAT 51° 17' N, LONG 06° 45' E

VARIATION 5° W Bearings all magnetic

SERIAL No. 50299/3 DATE 17.5.65

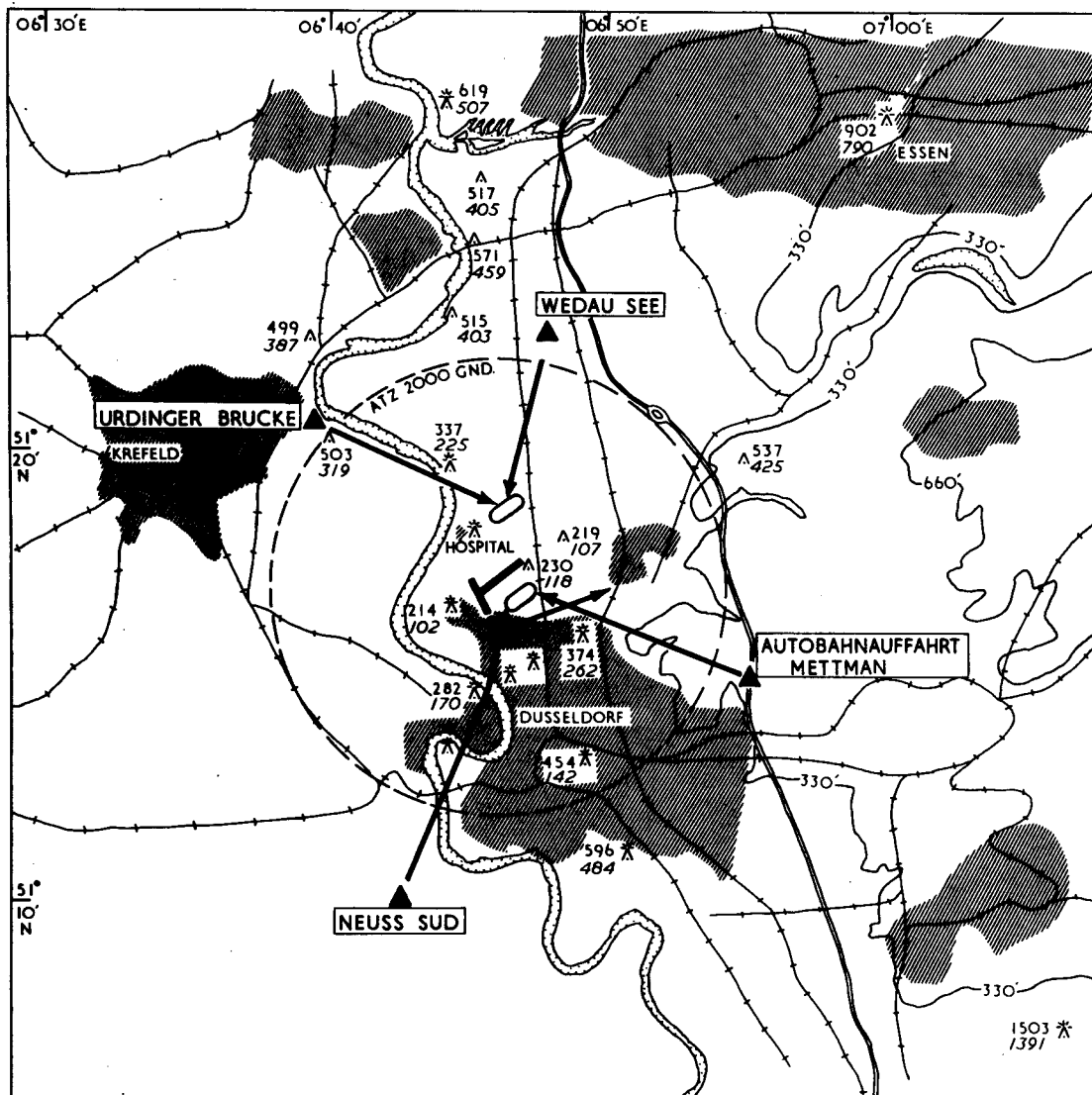
Reason for Re-issue:

VASI ANGLE & APRON

AERODROME CHART DUSSELDORF

WARNING: SHEEP GRAZING UNDER CONTROL ON GRASS AREAS ADJACENT TO ALL RUNWAYS. MAINTAIN SHARP LOOK-OUT.

- NOTES:**
1. R/W 06/24 H.I. APPROACH, THRESHOLD & RUNWAY LIGHTS ARE BRILLIANCY CONTROLLED.
 2. R/W LIGHTS ARE 22 INS. A.G.L. POSITIONED APPROX. 0.6 m IN FROM RUNWAY EDGES.
 3. TAXIWAYS ARE APPROX. 25 m WIDE WITH LIGHTS 13 1/2 INS. A.G.L.



DUSSELDORF VISUAL APPROACH CHART

BRITISH EUROPEAN AIRWAYS

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PROCEDURES

1. Approach is permitted only via the compulsory reporting points shown on the chart. Radio contact with Aerodrome Control Service shall be established at a distance of at least 15 n.m. from the aerodrome.
2. Aircraft without functioning two-way radio must obtain permission for landing from the Aerodrome Control Service before the flight is commenced; such aircraft may approach only via position "Mettman".
3. When entering aerodrome traffic zone, a height of 1500 ft. GND, shall not be exceeded. When entering the holding patterns, a height of 1000 ft. GND, shall be maintained.
4. Aircraft which due to their performance criteria are unable to follow the procedures shown on the chart, shall use the routes shown on Dusseldorf Routes Chart and establish radio contact with the Aerodrome Control Service at a distance of 25 n.m. from aerodrome.
5. When departing from R/W's 34 and 16, turn to the right or left as soon as possible in order to avoid overflying of the hospitals, Kaiserswerth and of the Dusseldorf City.
6. For reasons of noise abatement strict adherence to the prescribed approach routes is mandatory.

VARIATION 5° W Bearings all magnetic

SERIAL No. 4022Q/1 DATE: 29 10 64

Reason for Re-issue:

OBSTRUCTION 503' ADDED

VISUAL APPROACH CHART
DUSSELDORF

FRANKFURT AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
07L	WHITE H.I. UNI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL C/L & BARS	X
25R	WITH SEQ. FLASHERS ON C/L (915M)	X
25L	WHITE H.I. UNI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL C/L & BARS.	
07R	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
ALL	GREEN HIGH INT. UNI-DIRECTIONAL & L.I. OMNI-DIR. WING BARS (ON R/W 25L LIGHTS 260M. UP R/W.). STROBE BEACONS ON R/W 07L / 25R	WHITE HIGH INT. UNI-DIR. & L.I. OMNI-DIR. (R/W 07R & 25L TOUCH - DOWN LIGHTS 300M FROM THRESHOLD)

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR.

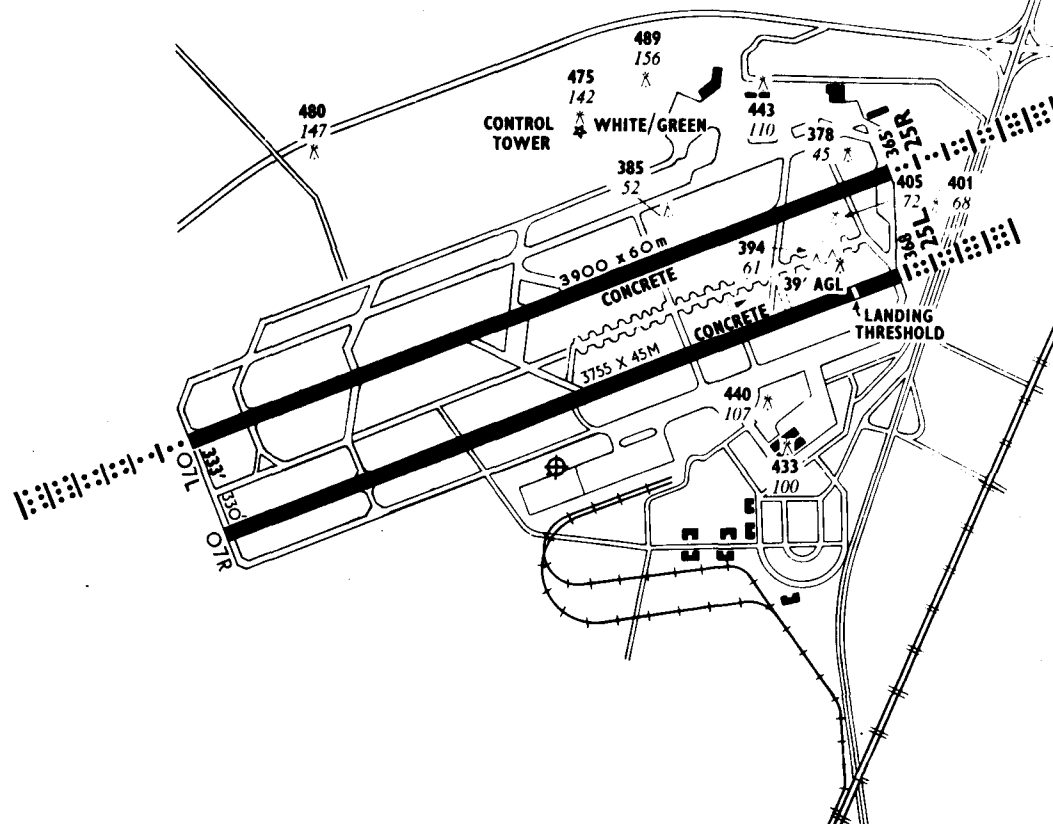
ELEVATIONS (FT) : —

OFFICIAL AERODROME	368
LOWEST THRESHOLD (07L)	333
LAT 50° 02' N, LONG 08° 34' E	
VARIATION 4° W	Bearings all magnetic
SERIAL No. 50241/2	DATE 25.5.65

Reason for Re-issue:

R/W 07R / 25L LENGTH

AERODROME CHART FRANKFURT



SCALE IN METRES
0 200 400 600 800 1000

R/W Availability
R/W 25L Landing 3495 (First 260m not available)

WARNINGS: 1. R/W LIGHTS 26' AGL 1.2m OFF EDGES.
T/W LIGHTS 14' AGL 1m OFF EDGES.
2. APPROACH & R/W LIGHTS FOR BOTH R/W'S MAY BE SWITCHED
ON SIMULTANEOUSLY. EXERCISE CARE IN IDENTIFYING R/W IN
USE. APP LIGHTS TO R/W 25L HIGHER INTENSITY THAN THOSE TO
R/W 25R & MORE EASILY DISCERNIBLE.

STOPWAYS: R/W 25R 100m ASPHALT

R/W 25L 300m.

CIRCUITS: VFR CIRCUITS TO BE CARRIED OUT S. OF A/D
AND NOT BELOW 1400FT. (AMSL).

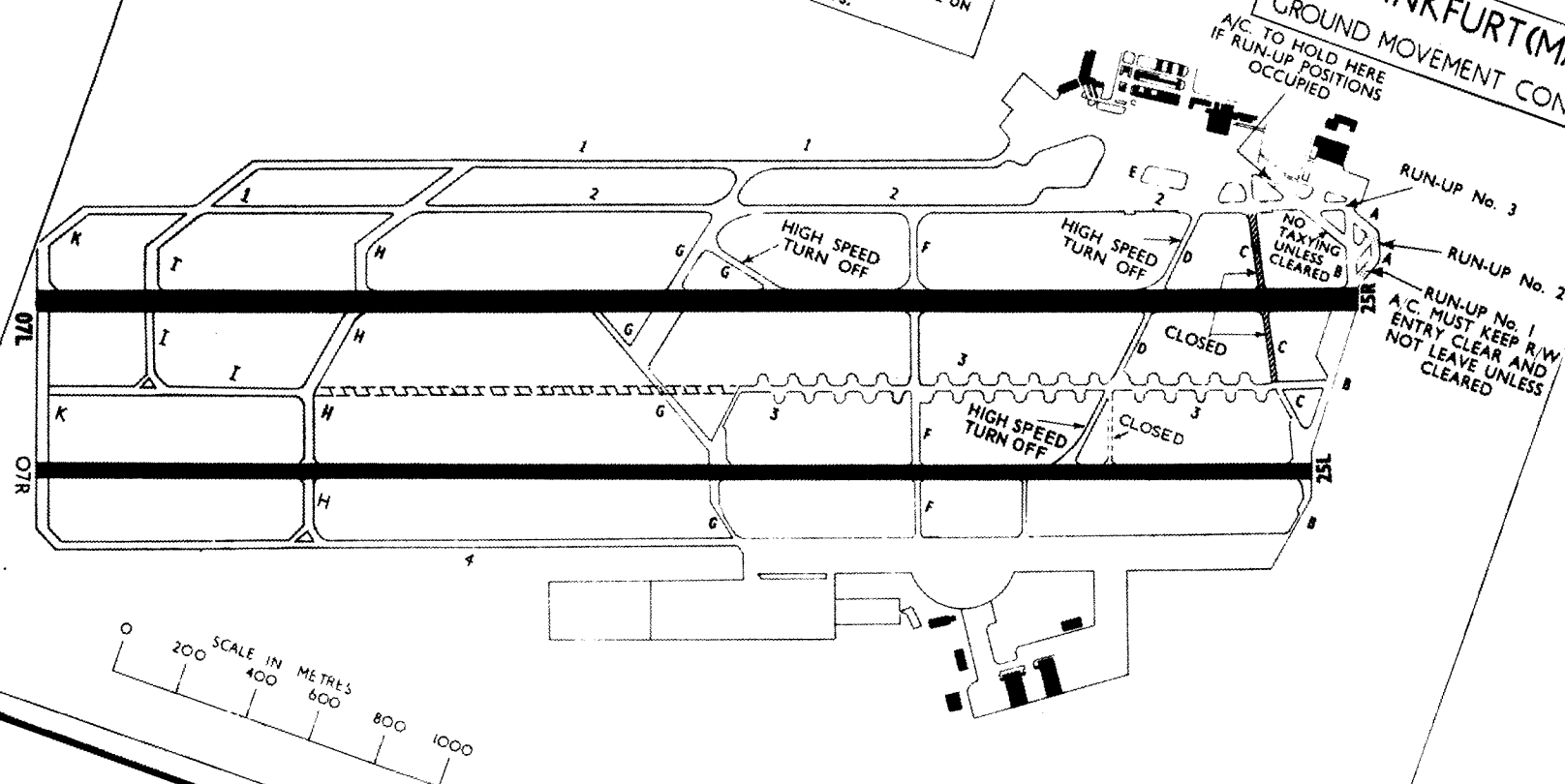
BEA

DESIGNATION OF TAXIWAYS AND RUN-UP AREAS

- NOTES**
1. A/C TAXIING FOR RUN-UP TO R/W 25R WILL NORMALLY PROCEED ON TAXIWAY 'A' UNLESS OTHERWISE INSTRUCTED. RUN-UP WILL BE ACCOMPLISHED AT DESIGNATED RUN-UP POSITIONS 1, 2 & 3.
 2. A/C WHICH HAVE COMPLETED RUN-UP ARE EXPECTED TO MOVE ON IN SEQUENCE WITHOUT SPECIFIC INSTRUCTIONS.
 3. UNCONTROLLED VEHICULAR TRAFFIC ON TAXIWAYS.

FRANKFURT(MAIN)

GROUND MOVEMENT CONTROL



SERIAL No 50241/1
Reason for Re-Issue:

DATE 25.5.65

R/W 07R / 25L LENGTH

GROUND MOVEMENT CONTROL
FRANKFURT(MAIN)

GOTHENBURG AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
22	H.I. WHITE & L.I. WHITE/GREEN CENTRE-LINE & BARS	
04	L.I. WHITE/GREEN CENTRE-LINE WITH H.I. & L.I. BAR	
09 27	L.I. WHITE/GREEN CENTRE-LINE	
14 32	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
22 04	H.I. & L.I. GREEN	H.I. & L.I. WHITE WITH LAST PART YELLOW
09/27 14/32	L.I. GREEN	L.I. WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR.

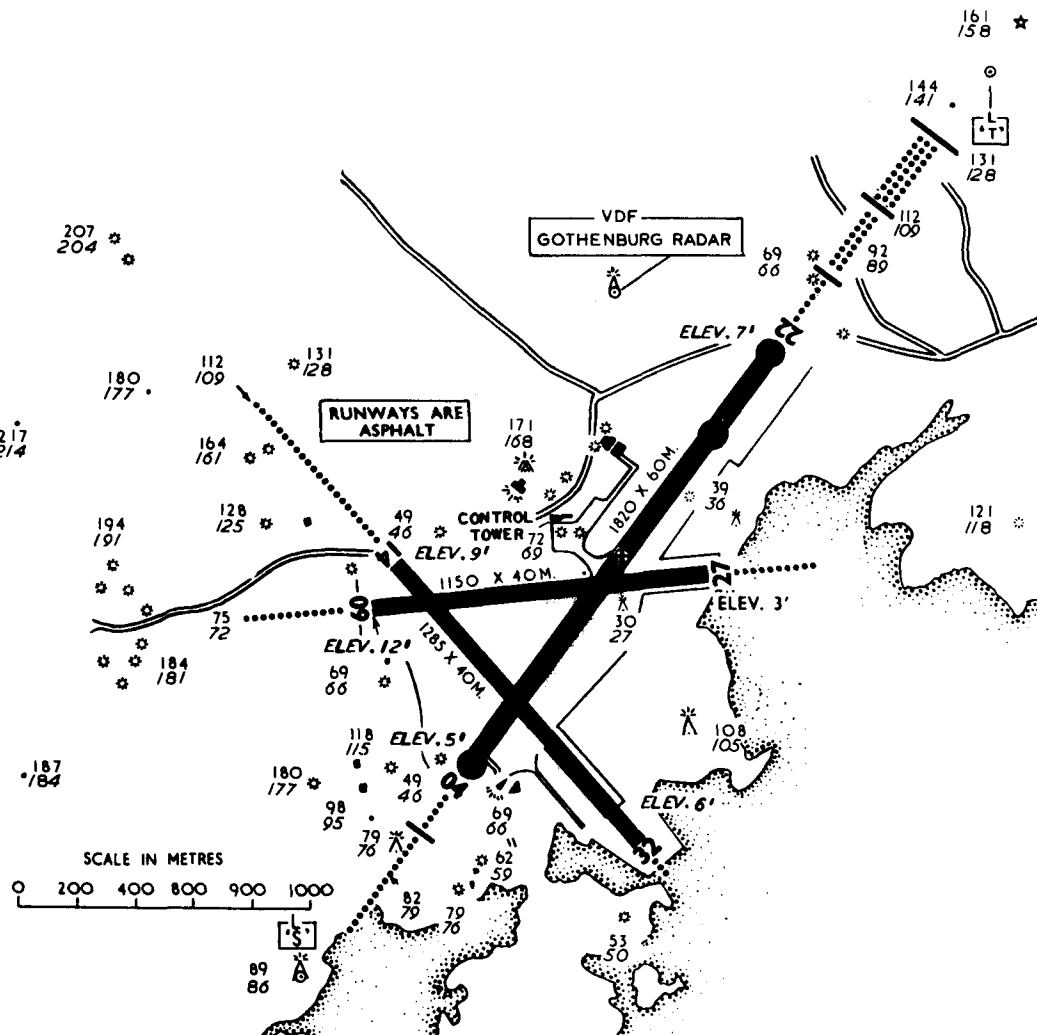
FLOODLIGHTS

ELEVATIONS (FT) : -

OFFICIAL AERODROME	13
LOWEST THRESHOLD (27)	3
LAT 57° 43' N, LONG 11° 47' E	
VARIATION 2° W	Bearings all magnetic
SERIAL No. 48015/1	DATE : 21.1.65
Reason for Re-issue:	

PRESENTATION

AERODROME CHART GOTHENBURG



STOPWAY: R/W 22 :- 395 M.

- NOTES:
1. CLEARANCE REQUIRED FOR FLIGHT THROUGH SAVE CTL. ZONE (SEE APPROACH CHARTS).
 2. AIRCRAFT RUNNING-UP AT THE END OF R/W 22 SHOULD AVOID HEADINGS WHICH WOULD CAUSE DISTURBANCE IN SECTOR 256° THROUGH N. TO 126°

SECT. SAFE ALT.
25 N.M. 2000'

30° 40° 50° 120°E

SECT. SAFE ALT.
25 N.M. 2500'

GOTHENBURG

ILS R/W 22

BRITISH EUROPEAN AIRWAYS

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APPROACH

TOWER

TORSLANDA TWR

118.7 119.7 122.6 121.5

TRANS ALT

2500 FT.

G/S (KTS)	70	90	110	130	150
LOM TO T/H	2.54	2.16	1.51	1.34	1.21

DESCENT ON G/P (FT/MIN)	405	515	630	745	860
-------------------------	-----	-----	-----	-----	-----

G/S (KTS)	170	190			
LOM TO T/H	1.12	1.05			

DESCENT ON G/P (FT/MIN)	975	1090			
-------------------------	-----	------	--	--	--

CRITICAL HEIGHTS (FT)

WITH G/P 250

NO G/P 350

OVERSHOOT

TURN RIGHT TO NDB 'VA'
& CLIMB TO 1500 / 497

ELEVATIONS (FT) :-

OFFICIAL AERODROME	13
LOWEST THRESHOLD	3
THRESHOLD R/W 22	7
VARIATION 2°W Bearings all magnetic	
SERIAL No. 40278/1	DATE: 21.1.65

Reason for Re-issue:

PRESENTATION & CRIT. HT.

ILS R/W 22
GOTHENBURG

BLUE

YELLOW

NDB
VINGA
'VA' 351

L
'OG' 342

L
'T' 324

OM

MM

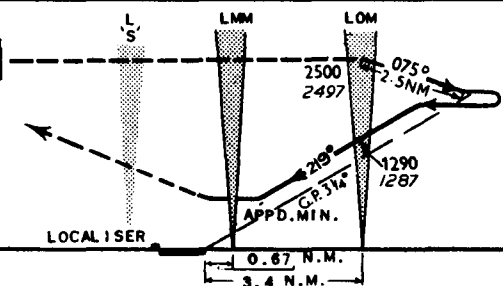
ILS
GB 110.3

L
'S' 369

SECT. SAFE ALT.
25 N.M. 1500'

SECT. SAFE ALT.
25 N.M. 2500'

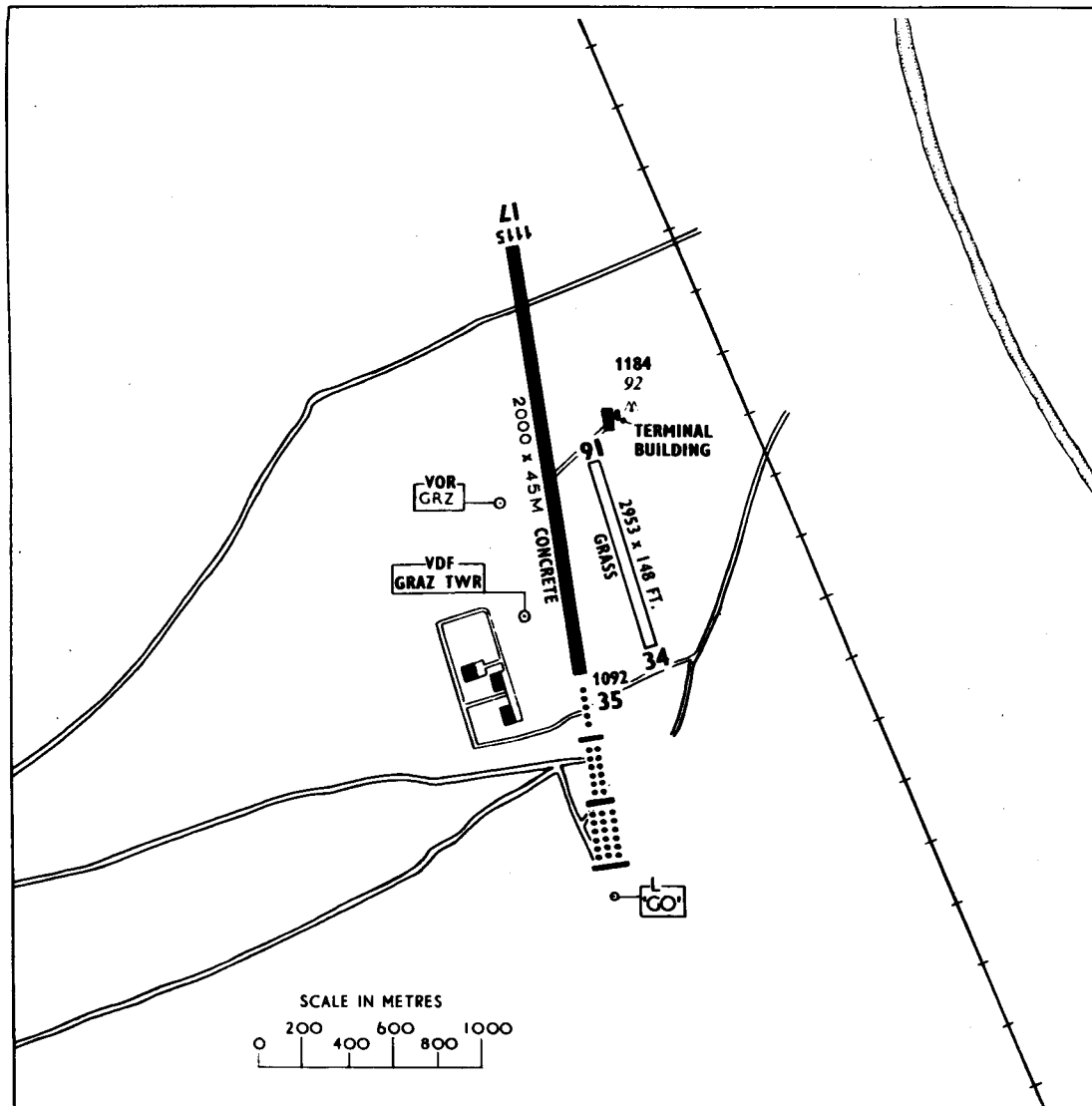
INITIAL APPROACH
25 N.M. 2500



LEVEL RATE
ONE TURN LEFT
AT 2000 (1987)

LOCAL ISER

0.67 N.M.
3.4 N.M.



GRAZ AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI (CAO Type Angle
35	WHITE H.I. & L.I. CENTRE-LINE & BAR	
17	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
35	H.I. & L.I. GREEN	H.I. & L.I. WHITE
17	H.I. & L.I. GREEN	H.I. & L.I. WHITE

OBSTRUCTION, TAXIWAY,

ELEVATIONS (FT) : —

OFFICIAL AERODROME	1115
LOWEST THRESHOLD (35)	1092
LAT 47° 00' N, LONG 15° 27' E.	
VARIATION 1° W Bearings all magnetic	
SERIAL No. 50153/2	DATE : 16.6.65
Reason for Re-Issue:	
L/BCN IDENT.	

AERODROME CHART
GRAZ

SECT. SAFE ALT.
25 N.M. 7600'

15°20' E

2507
1415

30°

40°

SECT. SAFE ALT.
25 N.M. 6300'

GRAZ

NDB R/W 35

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APPROACH GRAZ APP

119.3

TOWER GRAZ TWR

119.2

TRANS ALT 4000 FT.

G/S (KTS)	70	90	110	130	150
ABM NDB TO TURN	4 - 17	3 - 20	2 - 44	2 - 19	2 - 0
NDB TO ADME	3 - 26	2 - 40	2 - 11	1 - 51	1 - 36
G/S	170	190	210	230	250
ABM NDB TO TURN	1 - 46	1 - 35			
NDB TO ADME	1 - 25	1 - 16			

OVERSHOOT

CLIMB AHEAD TO 2000 908 TURN RIGHT
TO NDB 'GRZ' CLIMBING TO 3500 2408

ELEVATIONS (FT) :—

OFFICIAL AERODROME	1115
LOWEST THRESHOLD	1092
THRESHOLD R/W 35	1092
VARIATION 1°W	Bearings all magnetic
SERIAL No. 50348/2	DATE: 16.6.65
Reason for Re-Issue:	
L/BCN IDENT. & SPOT HT.	

NDB R/W 35
GRAZ

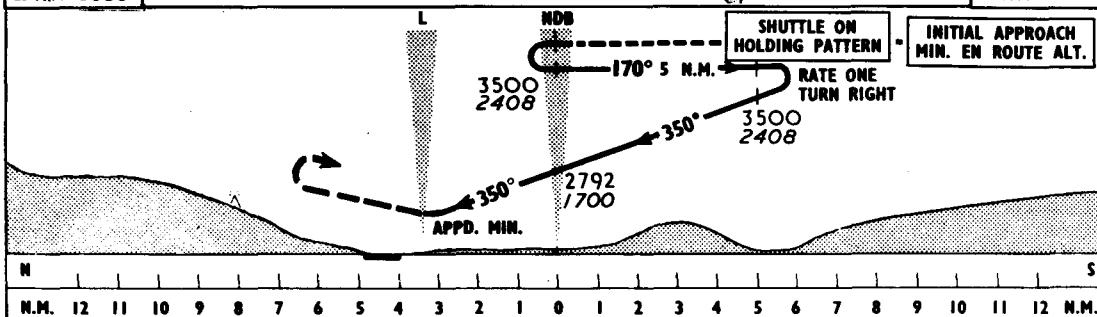
47° 08' N

4469
3377

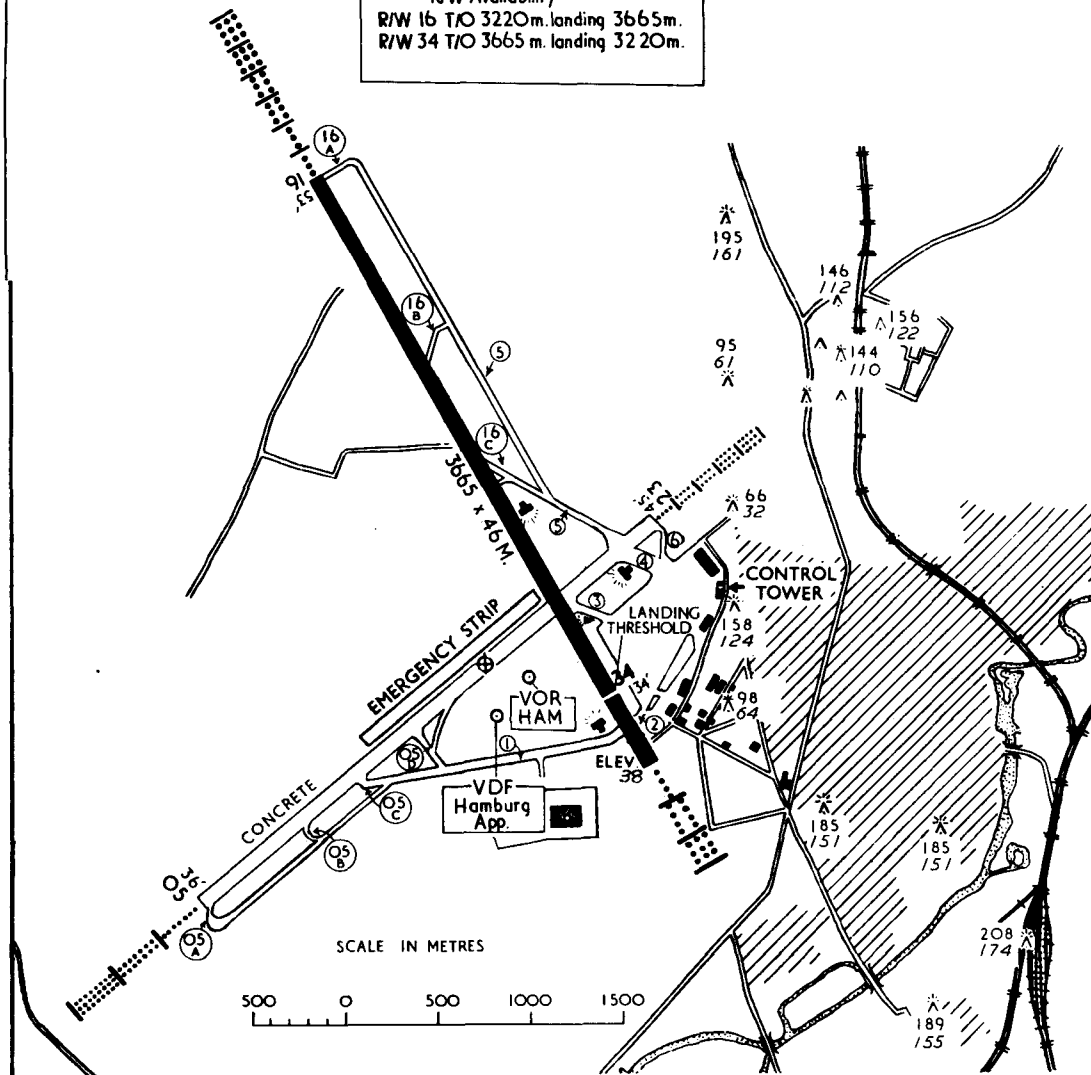
50'

SECT. SAFE ALT.
25 N.M. 8000'

SECT. SAFE ALT.
25 N.M. 5300'



R/W Availability
R/W 16 T/O 3220m. landing 3665m.
R/W 34 T/O 3665 m. landing 3220m.



HAMBURG AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
05 23	WHITE (VAR) H.I. UNI-DIR. & RED L.I. OMNI-DIR. C/L & BARS. PLUS WHITE STROBE UNI-DIR. C/L	X X
16	H.I. & L.I. WHITE CALVERT (914 M.) WITH 6 BARS	X
34	MODIFIED CALVERT VARIABLE L.I. & FIXED H.I. WHITE	X

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
05 23	L.I. GREEN & H.I. WING-BARS. (ELEVATED R/WOS)	H.I. (VAR) WHITE UNI-DIR. WITH LAST 600M. AMBER. EDGE LIGHTS GREEN & AMBER/GREEN L.I. FLUSH C/L.
16	L.I. GREEN & VAR. H.I.	WHITE (VAR) H.I. UNI-DIR. & L.I. OMNI-DIR. EDGE LIGHTS. WHITE (VAR) H.I. UNI-DIR. FLUSH C/L.
34	GREEN (VAR) L.I. UNI-DIR. + RED R/W EXTREMITY. BAR AT S. END.	AS FOR 16. THEN H.I. & L.I. WING-BARS AT 305M.

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND AND LANDING INDICATOR
RED UNI-DIRECTIONAL ELEVATED -
200M. BEFORE LDG. T/H R/W 23

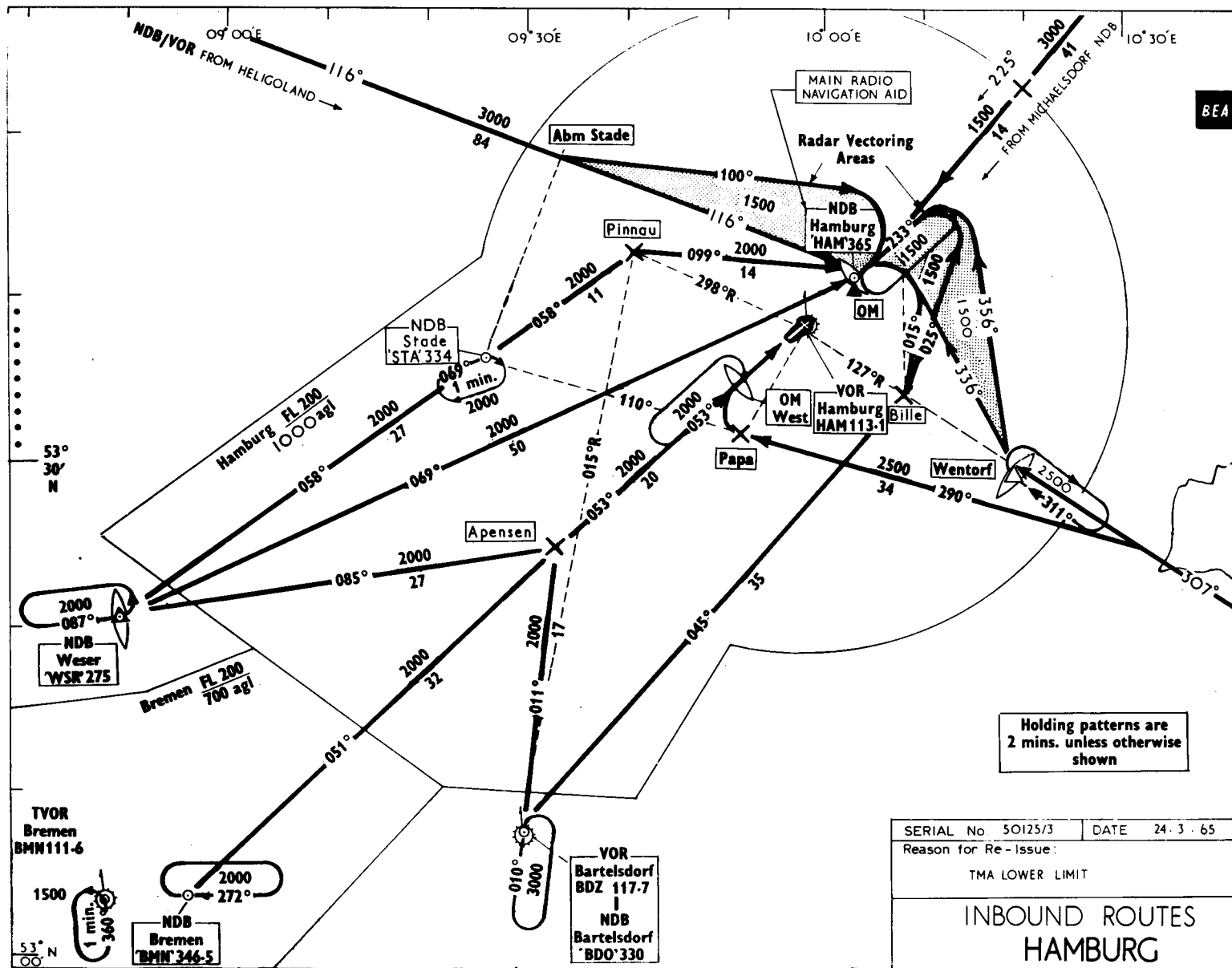
ELEVATIONS (FT)

OFFICIAL AERODROME	53
LOWEST THRESHOLD (34)	34
LAT	53° 38' N. LONG 09° 59' E
VARIATION	3° W Bearings all magnetic
SERIAL No.	50413/4
DATE	24. 6. 65
Reason for Re-issue: 05/23 CLOSED PLUS TAXIWAYS & LIGHTING	

AERODROME CHART HAMBURG

ELEVATED LIGHTING: R/W LIGHTS ELEVATED 22 INCHES,
T/W LIGHTS ELEVATED 14 INCHES.

NOISE ABATEMENT DO NOT FLY BELOW VASI'S
ON FINAL APPROACH.



Holding patterns are 2 mins. unless otherwise shown

SERIAL No 50125/3	DATE 24.3.65
Reason for Re-Issue:	
TMA LOWER LIMIT	
INBOUND ROUTES HAMBURG	

SECT. SAFE ALT.
25 N.M. 2000'

30'

69° 40' E

50'

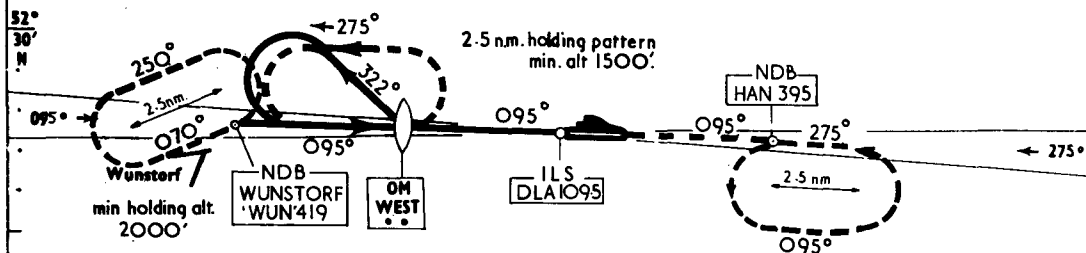
SECT. SAFE ALT.
25 N.M. 1700'

NO GLIDE PATH

NOTE

Caution when using OM. West.
Due Wunstorf ILS OM. on test.
Approx. 9 n.m. W. of Hannover.

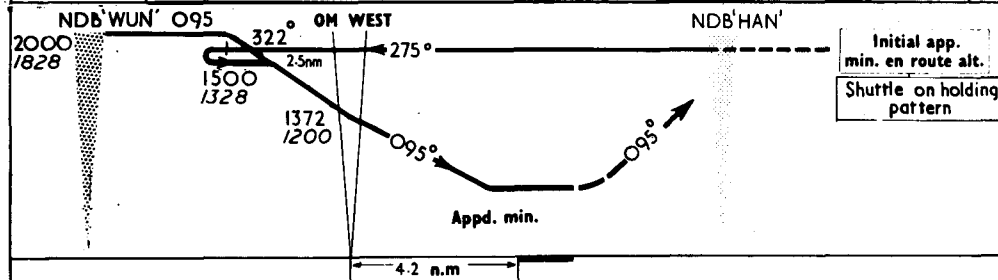
BLUE



YELLOW

SECT. SAFE ALT.
25 N.M. 2500'

SECT. SAFE ALT.
25 N.M. 2500'



HANNOVER

ILS R/W 10 (BACK BEAM)

BRITISH EUROPEAN AIRWAYS

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APPROACH HANNOVER APP

118.05 121.35 121.5 E

TOWER HANNOVER TWR.

118.7 121.5 E

TRANS ALT 4000 FT.

G/S (KTS)	70	90	110	130	150
OM W TO T/H	3.36	2.48	2.17	1.56	1.41

G/S	170	190			
OM W TO T/H	1.29	1.20			

OVERSHOOT

CLIMB AHEAD TO NDB 'HAN' AT
1500 1328

ELEVATIONS (FT) :-

OFFICIAL AERODROME 179

LOWEST THRESHOLD 172

THRESHOLD R/W 10 172

VARIATION 3 °W Bearings all magnetic

SERIAL No. 50271/2 DATE 18.5.65

Reason for Re-issue:

NDB IDENT CORRECTED

ILS R/W 10 (BACK BEAM)
HANNOVER

HANNOVER AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
10	WHITE HI. (VARIABLE) UNI-DIRECTIONAL CENTRE LINE & BARS RED LI OMNI-DIRECTIONAL CENTRE LINE. 28 ELECTRONIC, FLASHING LIGHTS ON CENTRE LINE.	
28	WHITE HI. (VARIABLE) UNI-DIRECTIONAL CENTRE LINE & BARS WHITE LI OMNI-DIRECTIONAL CENTRE LINE. 28 ELECTRONIC FLASHING LIGHTS ON CENTRE LINE.	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
10/ 28	GREEN HI. (VARIABLE) UNI DIRECTIONAL. LI OMNI DIRECT- IONAL	WHITE HI (VARIABLE) UNI-DIREC- TIONAL. STUB BARS 250M ALONG R/W. GREEN & AMBER/GREEN CENTER-LINE LIGHTS

OBSTRUCTION, TAXIWAY, WIND INDICATOR, A/D
BEACON ROT. WHITE.

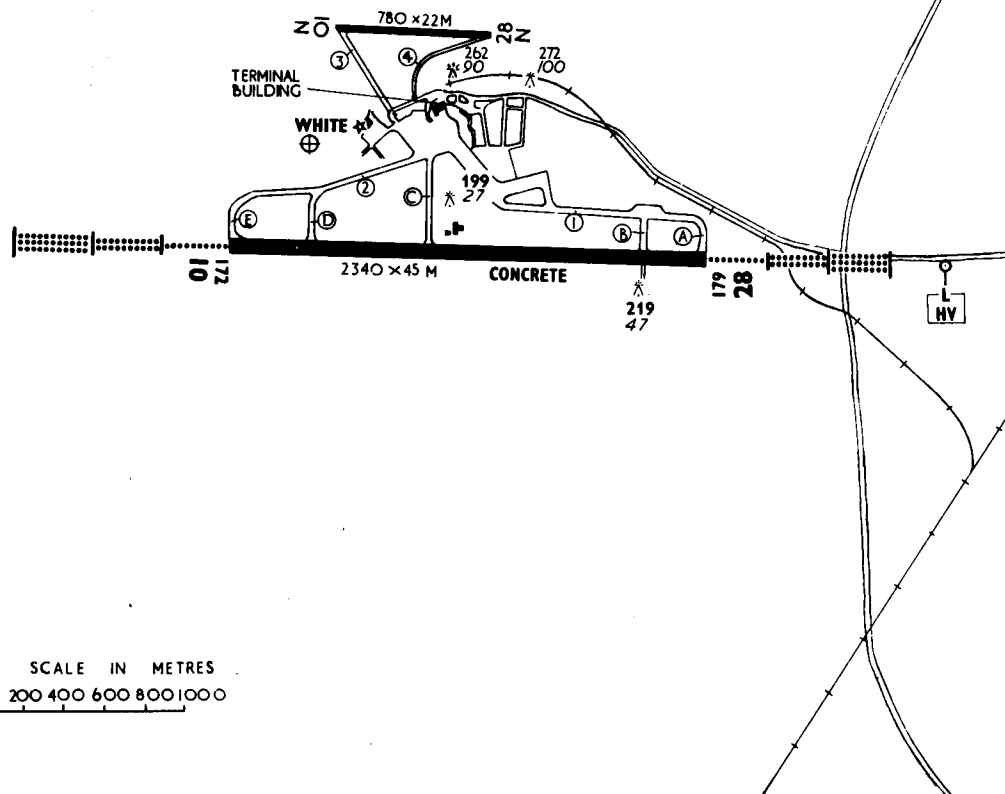
ELEVATIONS (FT)

OFFICIAL AERODROME	179
LOWEST THRESHOLD (10)	172
LAT 52° 28' N, LONG 09° 41' E	
VARIATION 3° W	Bearings all magnetic
SERIAL No. 40273/1	DATE 24.3.65

Reason for Re-Issue:

LIGHTING

AERODROME CHART HANNOVER



SCALE IN METRES
0 200 400 600 800 1000

WARNING

LIGHTS ON RUNWAYS AND TAXIWAYS ARE ELEVATED

CAUTION WHEN TAXYING ON EXTENDED EASTERN PORTION OF APRON, WHICH IS NOT VISIBLE
FROM TOWER

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
04		3°
15	H.I. WHITE UNI DIRECTIONAL & L.I. RED	2.7°
22	OMNI DIRECTIONAL CENTRE LINE & BARS	2.7°
33		3°

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
04	H.I. UNI-DIRECTIONAL	H.I. BI-DIRECTIONAL &
15	& L.I. OMNI DIRECT-	L.I. OMNI DIRECTIONAL
22	IONAL GREEN	WHITE WITH LAST 600M
33		YELLOW
		ALSO WHITE C/L LIGHTS
		VARIABLE INTENSITY
		SPACED 15M INTERVALS.

OTHER LIGHTING: OBSTRUCTION, TAXIWAY, APRON

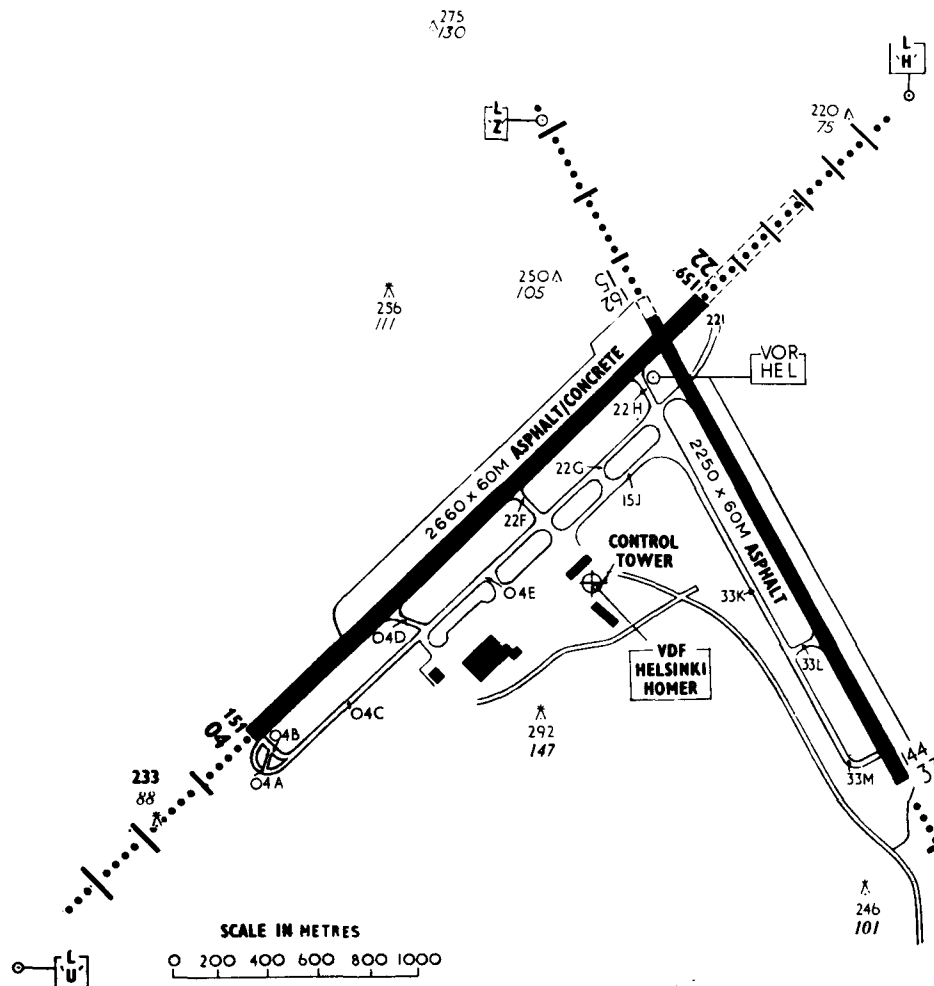
ELEVATIONS (FT) :-

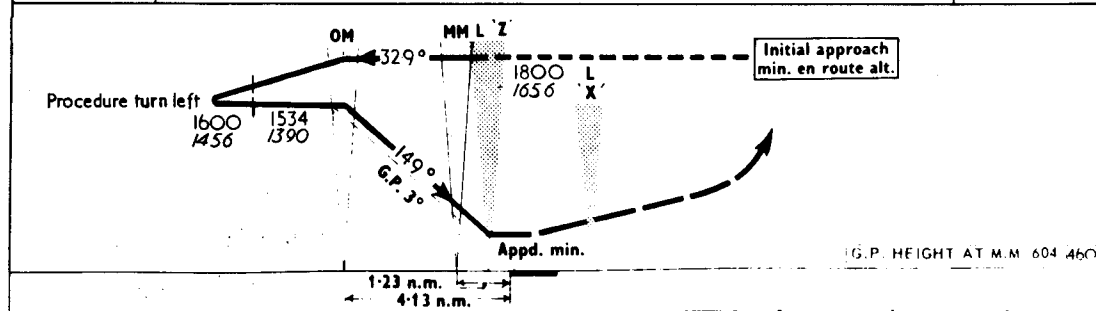
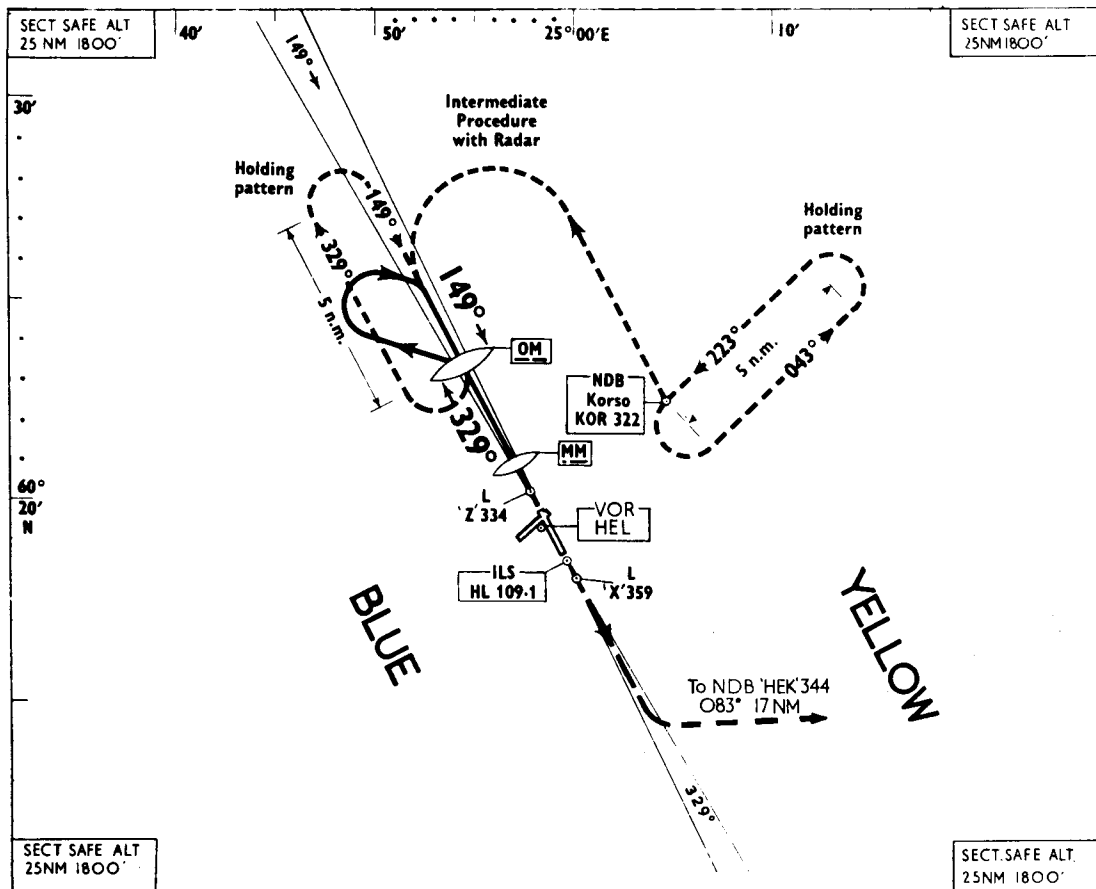
OFFICIAL AERODROME	167
LOWEST THRESHOLD (33)	144
LAT 60° 19' N, LONG 24° 58' E	
VARIATION 4° E	Bearings all magnetic
SERIAL No 50053/2	DATE 6-5-65

Reason for Re-issue:

VOR ADDED

AERODROME CHART
HELSINKI





HELSINKI ILS R/W 15

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APPROACH HELSINKI APP.

119.1 119.7 121.5

TOWER HELSINKI TWR

118.1 119.7 121.5

TRANS ALT 3000 FT.

G/S(KTS)	70	90	110	130	150
OM TO T/H	3 · 32	2 · 45	2 · 15	1 · 54	1 · 39
DESCENT ON G/P (FT/MIN)	371	477	583	689	796
G/S	170	190	210	230	250
OM TO T/H	1 · 27	1 · 17			
DESCENT ON G/P (FT/MIN)	902	1008			

OM TO T/H 3 · 32 2 · 45 2 · 15 1 · 54 1 · 39

DESCENT ON G/P (FT/MIN) 371 477 583 689 796

G/S 170 190 210 230 250

OM TO T/H 1 · 27 1 · 17

DESCENT ON G/P (FT/MIN) 902 1008

OVERSHOOT

CLIMB AHEAD TO 1800 /656 THEN TURN LEFT
HOMING TO NDB 'HEK' CONTACT APPROACH

ELEVATIONS (FT) :-

OFFICIAL AERODROME 167

LOWEST THRESHOLD 144

THRESHOLD R/W 15 162

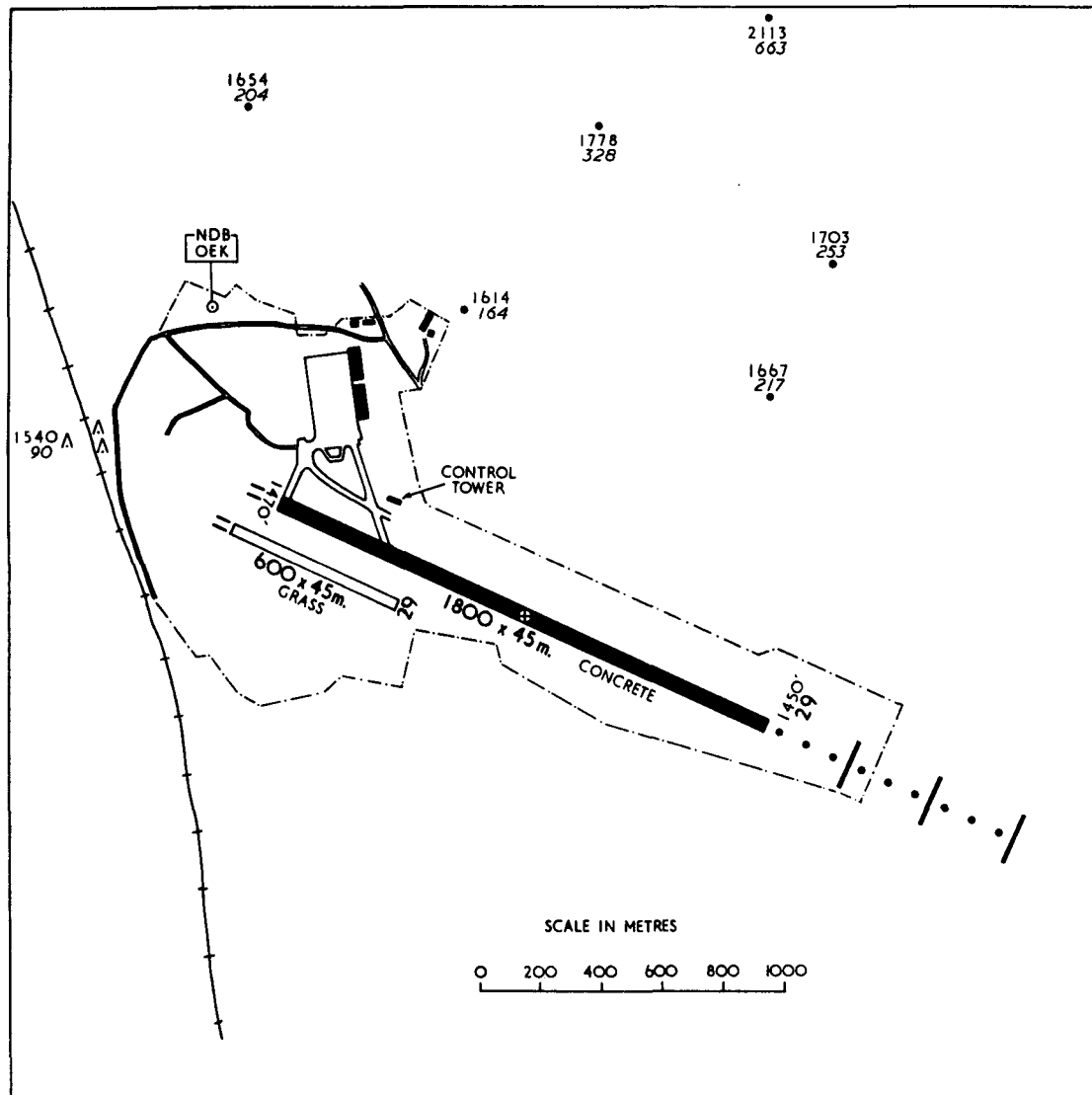
VARIATION 4° E Bearings all magnetic

SERIAL No 50276/2 DATE 6 5-65

Reason for Re-issue:

VOR ADDED

ILS R/W 15
HELSINKI



NOTE: GLIDER FLYING TO SOUTH OF GRASS RUNWAY.
R/W 11 RIGHT HAND CIRCUITS AT NIGHT.

KLAGENFURT AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
CAO
Type
Angle

R/W	APPROACH
29	VARIABLE H.I. CENTRE-LINE WITH 3 BARS

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
11 29	GREEN HIGH & LOW INTENSITY	VARIABLE H.I. BI-DIRECT. & WHITE L.I.

OBSTRUCTION, TAXIWAY (BLUE), SIGNAL AREA,
WIND INDICATOR, LANDING DIRECTION.

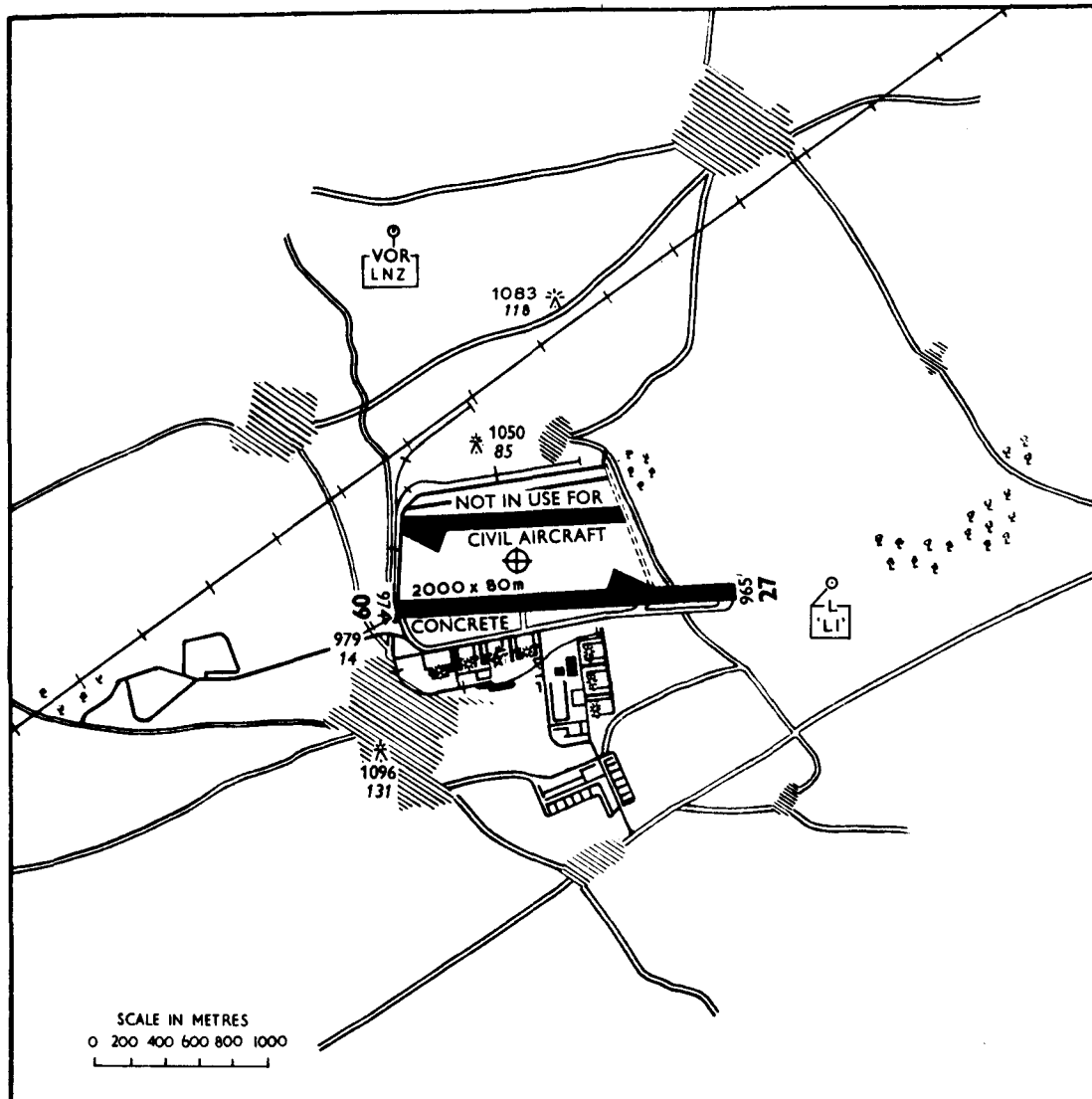
ELEVATIONS (FT) : —

OFFICIAL AERODROME	1470
LOWEST THRESHOLD (29)	1450
LAT 46° 39' N, LONG 14° 21'	
VARIATION 1° W	Bearings all magnetic
SERIAL No. 50022/1	DATE: 22.1.65

Reason for Re-issue:

LIGHTING

AERODROME CHART KLAGENFURT



LINZ AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH
09/27	NIL

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
09/27	H.I. & L.I. GREEN	H.I. & L.I. WHITE FIRST 500 M. R/W 27 UNLIT

OBSTRUCTION, TAXIWAY, ADME BEACON
FLOOD, ANGLE OF APP

ELEVATIONS (FT) :—

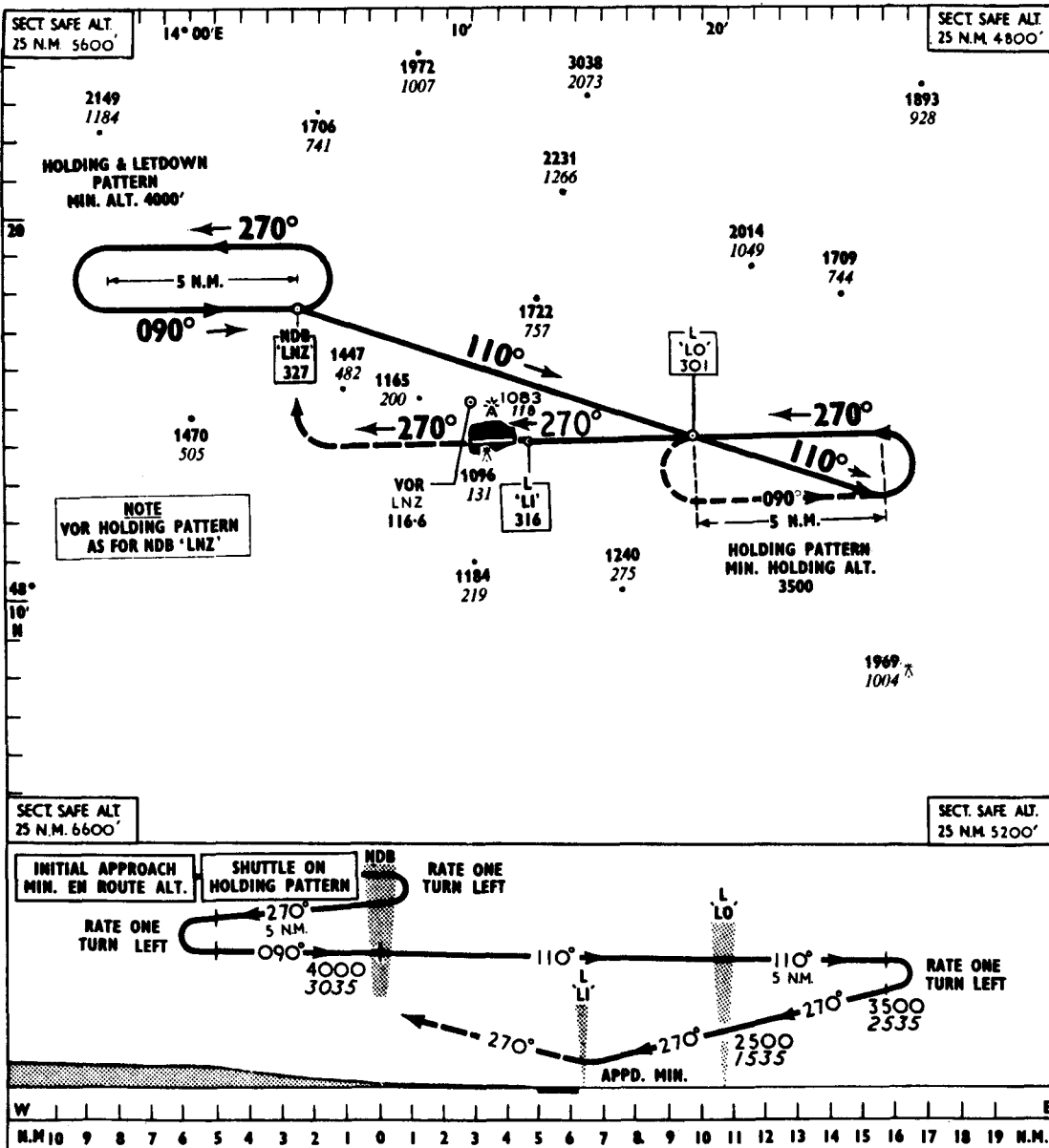
OFFICIAL AERODROME	968
LOWEST THRESHOLD (27)	965
LAT 48° 14' N, LONG 14° 11' E	
VARIATION 2° W	Bearings all magnetic
SERIAL No. 40221/1	DATE: 5.2.65

Reason for Re-issue:

VOR IDENT.

AERODROME CHART LINZ

- NOTE 1. DUE TO RESTRICTED VISUAL RANGE CAUSED BY REMOVAL OF THE CONTROL TOWER, NO AIR TRAFFIC CONTROL CAN BE PROVIDED FOR AIRCRAFT IN THE TRAFFIC CIRCUIT & NO LIGHT SIGNALS CAN BE GIVEN. ONLY TAKE OFF & LANDING INFORMATION WILL BE TRANSMITTED BY RADIO. ALL AIRCRAFT MOVEMENTS ARE CARRIED OUT UNDER SOLE RESPONSIBILITY OF THE PILOT.
- NOTE 2. CIRCUITS. R/W 09 LEFT HAND. R/W 27 RIGHT HAND.
- NOTE 3. JET BARRIERS R/W 09/27 40m. BEFORE THRESHOLD 13FT AGL WHEN RAISED, LIT.
- NOTE 4. TAXIWAY EXTENSION TO R/W 27 NOT AVAILABLE TO CIVIL AIRCRAFT.



LINZ
NDB/L

BRITISH EUROPEAN AIRWAYS

BE 1

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APPROACH LINZ APP

120.5E

TOWER LINZ TWR

119.5 121.5E

TRANS ALT 4000 FT.

G/S (KTS)	70	90	110	130	150
L'LO TO TURN	4.17	3.20	2.44	2.19	2.0
L'LO TO A/D	4.03	3.09	2.35	2.10	1.53
G/S	170	190			
L'LO TO TURN	1.46	1.35			
L'LO TO A/D	1.40	1.30			

CRITICAL HEIGHT (FT)

550

OVERSHOOT

CLIMB AHEAD FOR 5 N.M. THEN TURN RIGHT
CLIMBING TO 4000 3035 TO NDB 'LNZ'

ELEVATIONS (FT) :—

OFFICIAL AERODROME	968
LOWEST THRESHOLD	965
THRESHOLD R/W 27	965
VARIATION 2°W	Bearings all magnetic
SERIAL No. 40221/1	DATE: 5.2.65

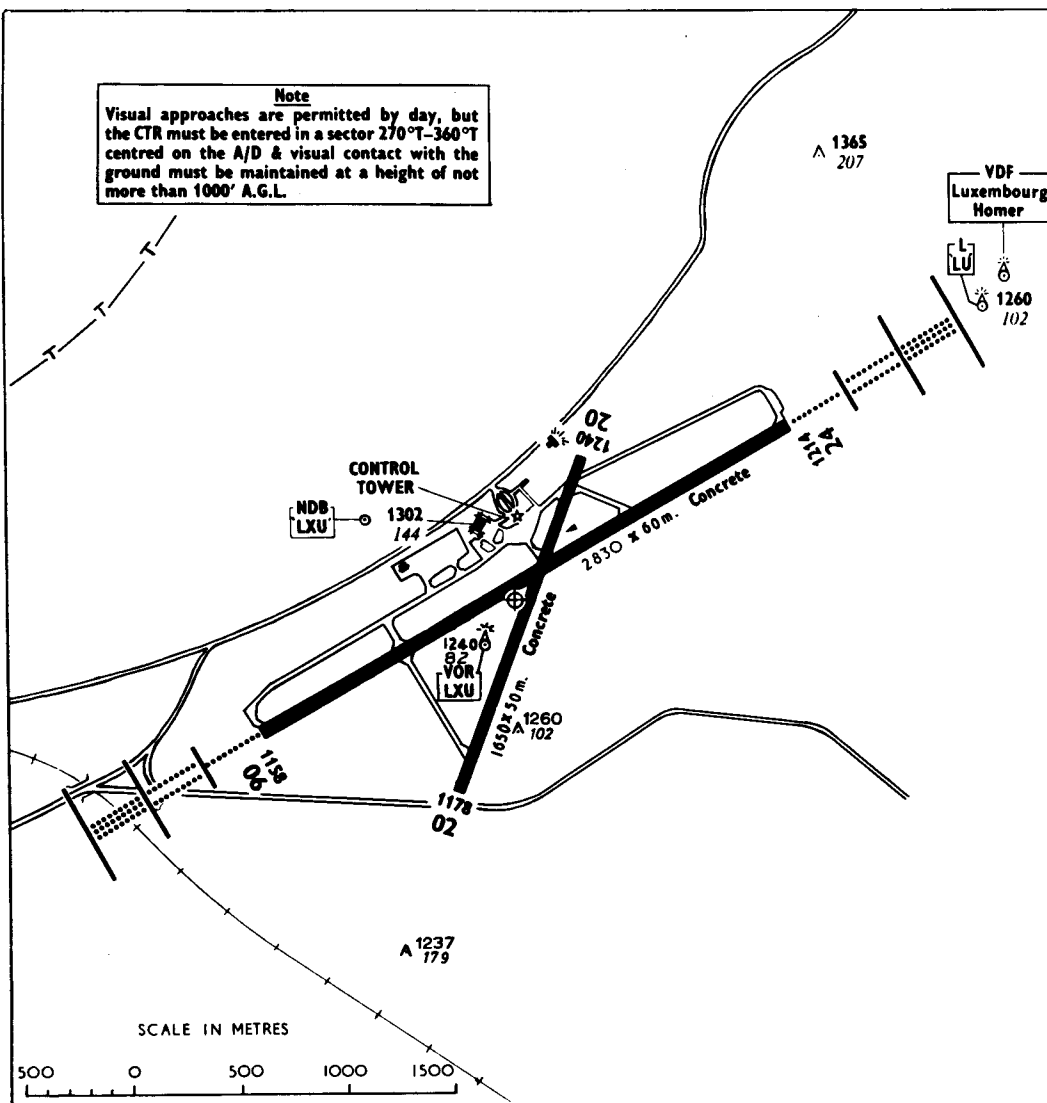
Reason for Re-issue:

VOR IDENT.

NDB/L
LINZ

Note

Visual approaches are permitted by day, but the CTR must be entered in a sector 270°T-360°T centred on the A/D & visual contact with the ground must be maintained at a height of not more than 1000' A.G.L.



LUXEMBOURG AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06	L.I. WHITE CENTRE-LINE & BARS	
24	H.I. WHITE CENTRE-LINE & BARS PLUS ELECTRONIC FLASH CENTRE-LINE	
02/20	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06	H.I. & L.I. GREEN	H.I. BI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL WHITE
24	GREEN	
02/20	GREEN	L.I. OMNI-DIRECTIONAL WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON
LANDING INDICATOR R/W INDICATORS.
(GREEN ARROW/RED CROSS) FOR R/Ws 02/20
ANGLE OF APPROACH INDICATORS.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	1240
LOWEST THRESHOLD (06)	1158
LAT 49° 38' N, LONG 06° 13' E	
VARIATION 4° W	Bearings all magnetic
SERIAL No. 40296/1	DATE : 12-11-64
Reason for Re-issue:	
R/W 06/24 AVAILABLE FULL LENGTH	

AERODROME CHART LUXEMBOURG

WARNING: R/W 06 APPROACH W.I.P.

SECT. SAFE ALT.
25 NM. 3500'

06° 00' E

110°

120°

SECT. SAFE ALT.
25 NM. 2900'

LUXEMBOURG ILS R/W 24

BRITISH EUROPEAN AIRWAYS

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APPROACH LUXEMBOURG APP

118.45 121.5E

TOWER LUXEMBOURG TWR

118.1 121.5E

TRANS ALT 3000 FT.

G/S (KTS)	90	110	130	150	170
ABM NDB TO TURN	1.40	1.22	1.09	1.00	0.53
OM TO ADME	2.34	2.06	1.47	1.32	1.22
DESCENT ON G/P (FT/MIN)	475	585	690	795	905
G/S (KTS)	190	210	230	250	
ABM NDB TO TURN	0.47	0.43	0.39	0.36	
OM TO ADME	1.13				
DESCENT ON G/P (FT/MIN)	1010	1115			

CRITICAL HEIGHTS (FT)

WITH G/P 250 NO G/P 350

OVERSHOOT

CLIMB ON 244° TO 3000 1842
AND CONTACT APP.

ELEVATIONS (FT) :—

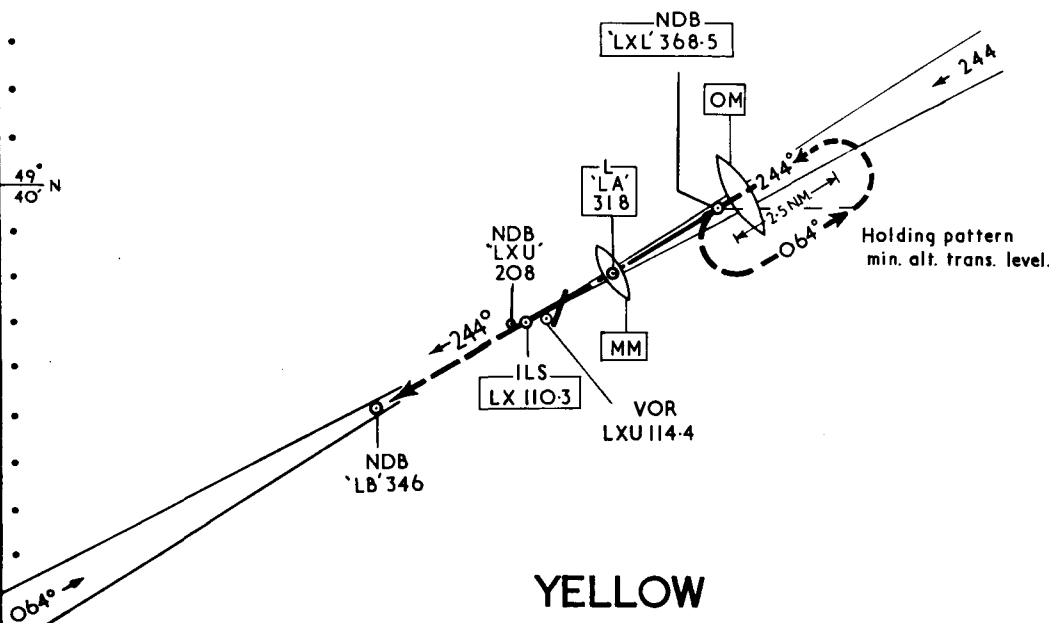
OFFICIAL AERODROME	1240
LOWEST THRESHOLD	1158
THRESHOLD R/W 24	1214
VARIATION 4° W	Bearings all magnetic
SERIAL No. 40345/2	DATE: 16.2.65
Reason for Re-issue:	

SECT. SAFE ALTS. & PROFILE

ILS R/W 24
LUXEMBOURG

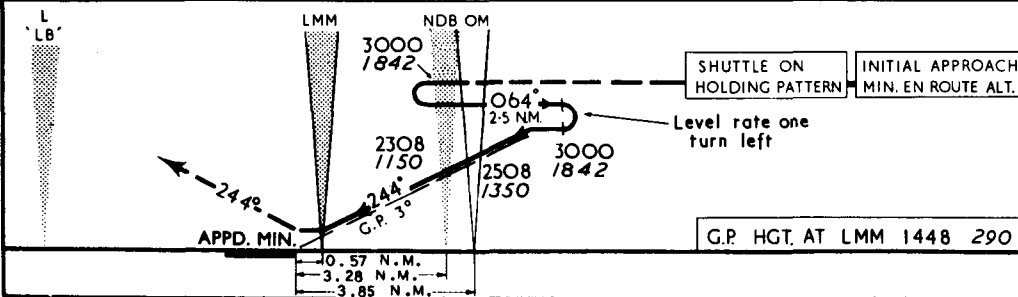
BLUE

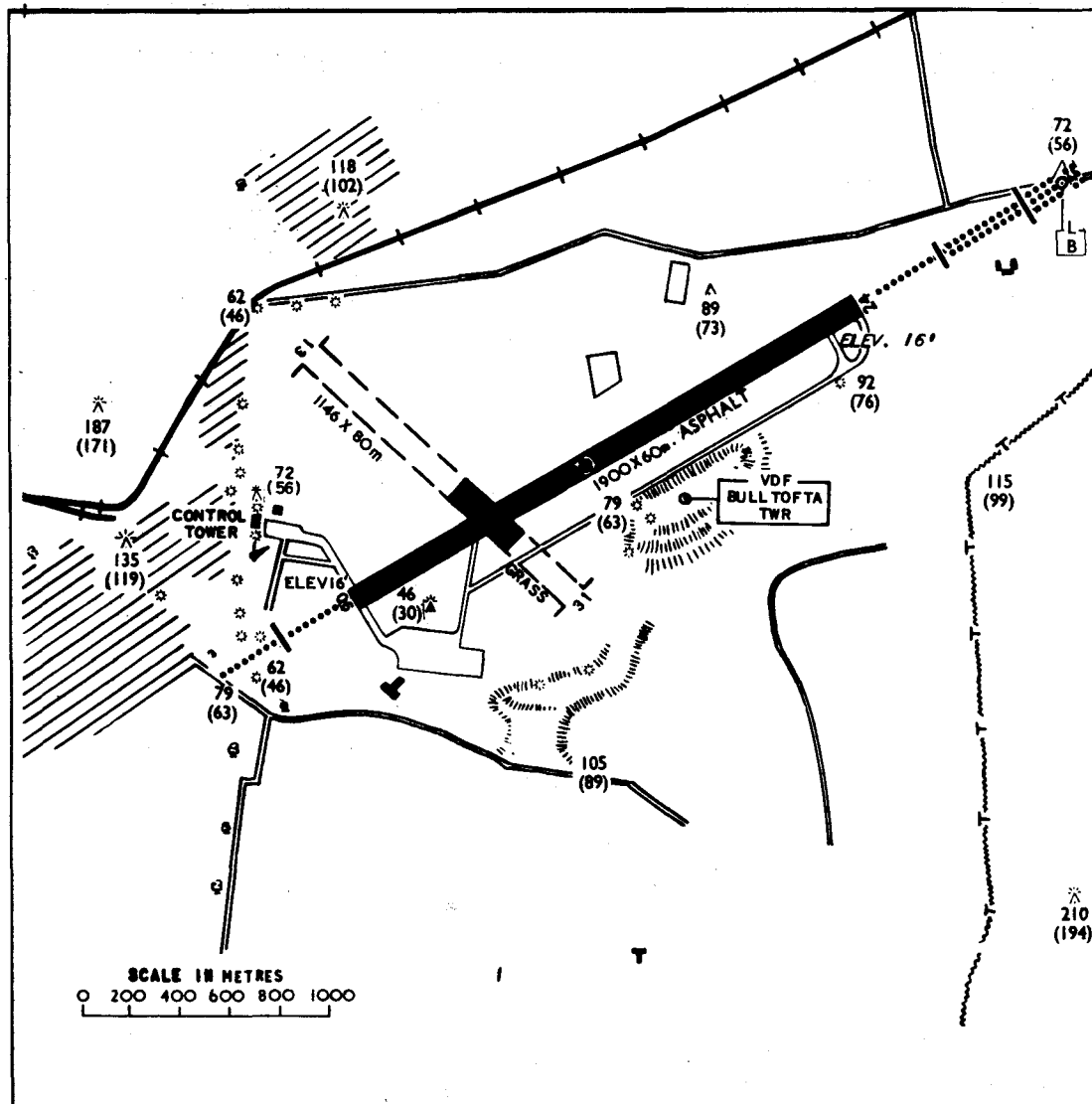
49°
40' N



SECT. SAFE ALT.
25 NM. 3200'

SECT. SAFE ALT.
25 NM. 3500'





MALMO (BULLTOFTA) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
24	H.I. WHITE & L.I. WHITE/GREEN CENTRE - LINE & BARS	
06	L.I. WHITE/GREEN CENTRE-LINE & BAR	
13/31	NIL	

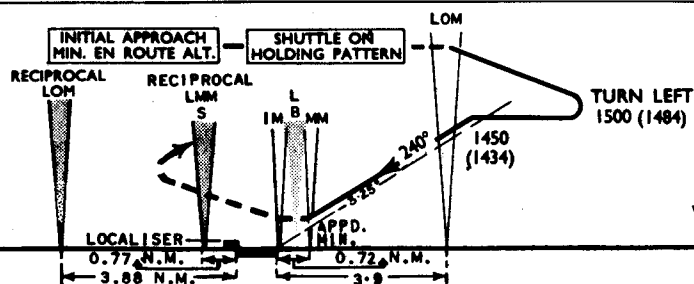
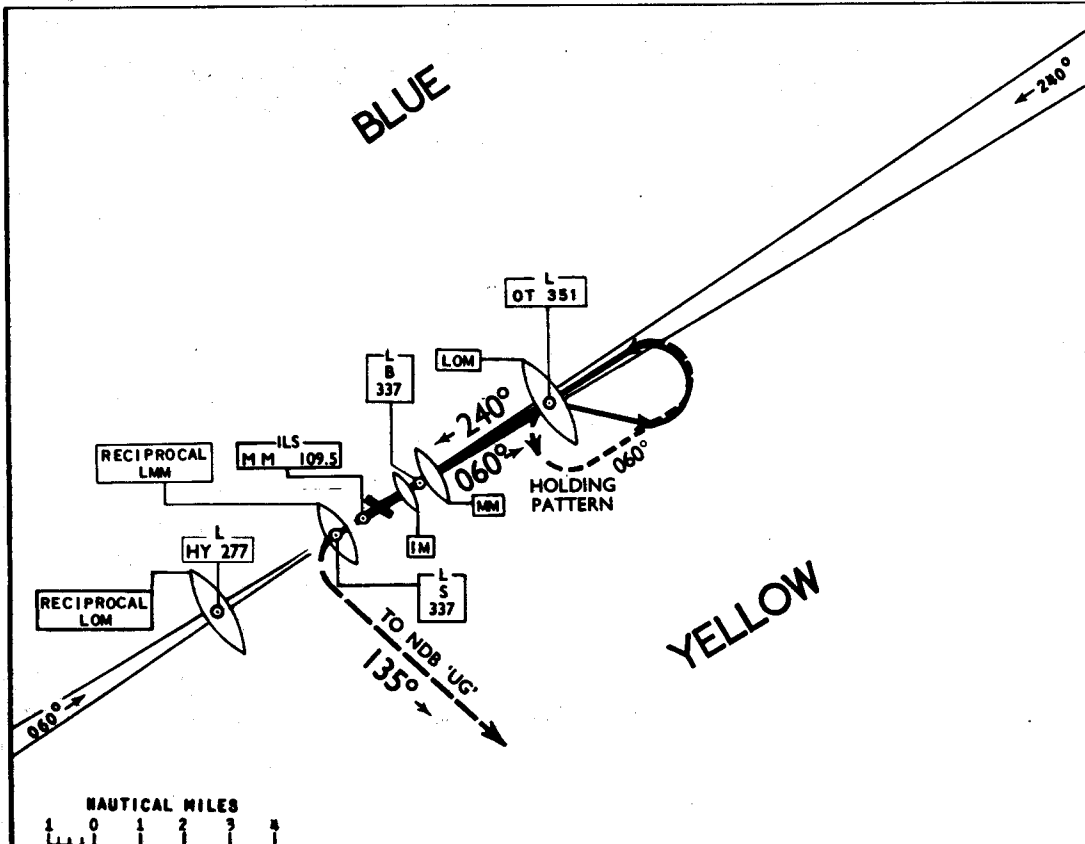
N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
24	H.I. & L.I. GREEN	H.I. & L.I. WHITE
06	, ,	, ,
13/31	L.I. GREEN	L.I. WHITE

OBSTRUCTION, TAXIWAY, FLOODLIGHTS

ELEVATIONS (FT)	a.m.s.l.	135
	above lowest T/H	119
OFFICIAL AERODROME		16
LOWEST THRESHOLD		16
LAT	55° 36' N,	LONG 13° 04' E
VARIATION	2° W	Bearings all magnetic
SERIAL No.	39339	DATE: 28.7.64
Reason for Re-issue:		

AERODROME CHART MALMO (BULLTOFTA)



HGT. AT MM 338 (322)
WITH G/P INOPERATIVE
HGT. AT MM 388 (372)

MALMO (BULLTOFTA)

ILS R/W 24

BRITISH EUROPEAN AIRWAYS

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APPROACH

TOWER

BULLTOFTA TWR.

118.7 119.7 122.6 121.5

TRANS ALT

2500 FT.

G/S (KTS)	105	115	125	135	145
LOM TO ADME	2m14s	2m02s	1m53s	1m44s	1m37s
	m s	m s	m s	m s	m s
DESCENT ON G/P (FT/MIN)					
G/S (KTS)	155	165			
LOM TO ADME	1m31s	1m25s	m s	m s	m s
	m s	m s	m s	m s	m s
DESCENT ON G/P (FT/MIN)					

CRITICAL HEIGHTS (FT)

TRIDENT AUTO MANUAL

OTHER TYPES WITH G/P 200 NO G/P 300

OVERSHOOT

TURN LEFT TO 'UG' & CLIMB TO 2500 2484

ELEVATIONS (FT) a.m.s.l. 1500
above lowest T/H 1484

OFFICIAL AERODROME 16

LOWEST THRESHOLD 16

THRESHOLD R/W 24 16

VARIATION 2°W Bearings all magnetic

SERIAL No. 45563 DATE: 28.7.64

Reason for Re-issue:

ILS R/W 24
MALMO (BULLTOFTA)

NOTE
A/C USING R/W 25 ARE TO USE
T/W 25A WHICH IS PARTLY INVISIBLE
FROM TOWER & IS TO BE KEPT
CLEAR NEAR THE RUN-UP END.

VOR
DDM 115.4
On test unreliable
Not to be used.

CONTROL
TOWER

CLOSED

GRASS
RUNWAYS

2600x61 M. CONCRETE

VDF
MUNICH
APP/TWR

SCALE IN METRES

0 200 400 600 800 1000

NOTES: 1. R/W LIGHTS ARE ELEVATED
2. T/W P/W 07 IS 5FT HIGHER THAN OFFICIAL AERODROME ELEVATION.

MUNICH AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
07 & 25	H.I. WHITE UNI-DIRECTIONAL & BAR WITH L.I. WHITE OMNI-DIRECTIONAL CENTRE-LINE & BAR, ALSO SEQUENCED FLASHERS	3°

NB NO ENTRY UNDER VASI. ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
07 & 25	H.I. GREEN & L.I. RED WITH ELECTRONIC FLASHERS	H.I. UNI-DIRECTIONAL L.I. OMNI-DIRECTIONAL WHITE (LAST 3 YELLOW)

OBSTRUCTION, TAXIWAY, (ELEVATED 24")
WIND INDICATOR, FLOODLIGHTS, ADME
BEACON

ELEVATIONS (FT) :—

OFFICIAL AERODROME 1732

LAT 48° 08' N, LONG 11° 42' E

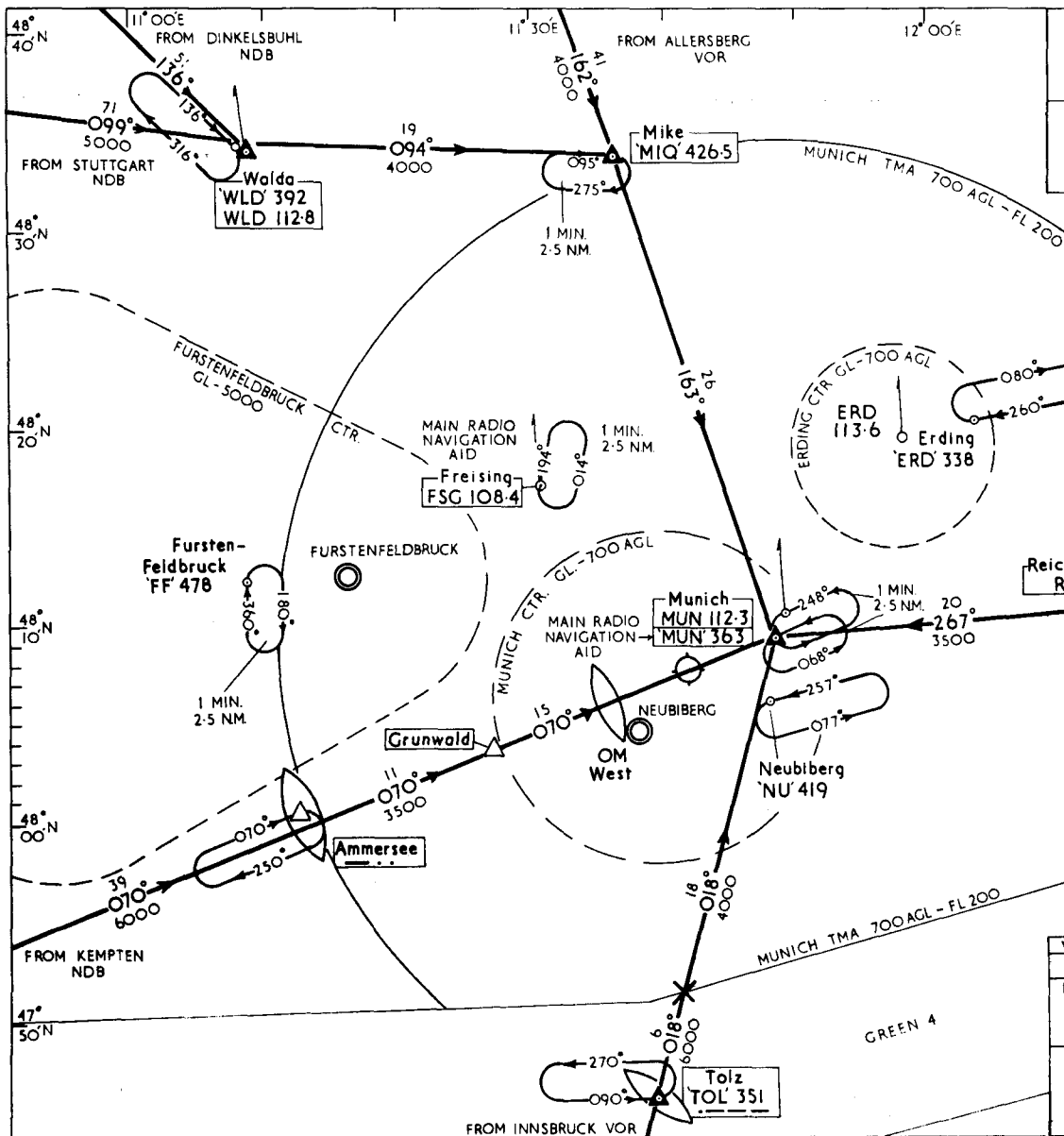
VARIATION 2° W | Bearings all magnetic

SERIAL No. 50235/3 | DATE 15.4.65

Reason for Re-issue:

VASI 07 ADDED

AERODROME CHART MUNICH



MUNICH

INBOUND ROUTES & HOLDINGS

BRITISH EUROPEAN AIRWAYS

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NOTES

All holding patterns
are 2 mins (5nm) unless
otherwise indicated
Min. holding alt. 4000'
except:

Munich NDB	3000'
Freising	3000'
Ammersee	5000'
Reichertsheim	5000'
Walda	5000'
Tolz	6000'

VARIATION 2°W Bearings all magnetic

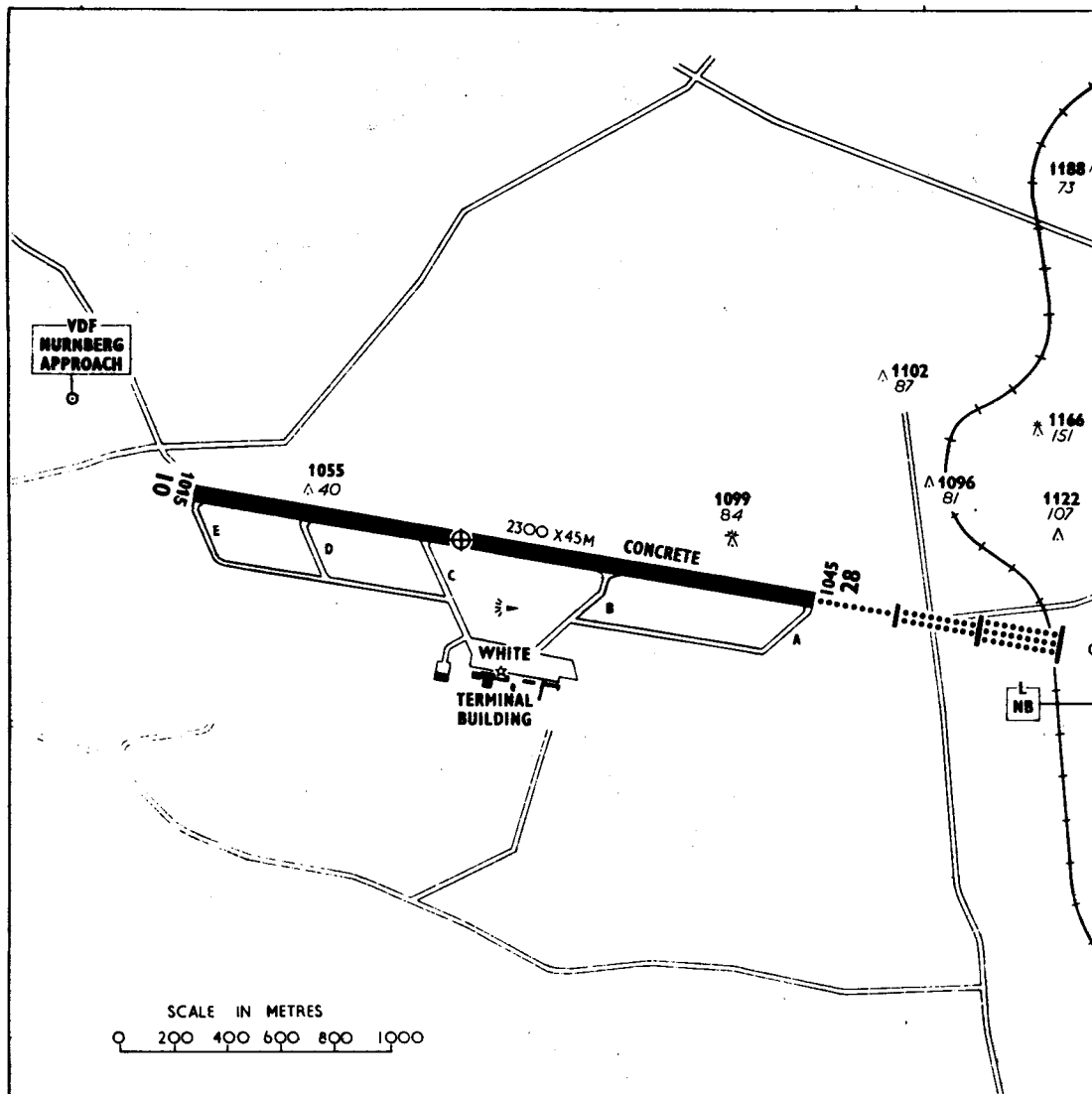
SERIAL No 50084/2 DATE 6.2.65

Reason for Re-issue:

ERDING VOR ADDED.

INBOUND ROUTES & HOLDINGS

MUNICH



NURNBERG AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI Type Angle
28	WHITE H.I. & L.I. CENTRE - LINE & BARS	
10	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
28 & 10	H.I. & L.I. GREEN	WHITE UNI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL WING BARS 30SM UP R/W

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR.

ELEVATIONS (FT)	a.m.s.l.	1099
	above lowest T/H	84
OFFICIAL AERODROME		1045
LOWEST THRESHOLD (10)		1015
LAT 49° 30' N. LONG 11° 05' E		
VARIATION 3° W Bearings all magnetic		
SERIAL No. 46948	DATE	5.8.64
Reason for Re-issue:		

1. AVOID OVERFLYING FURTH-ATZENHOF A/D 5NM. WEST OF NURNBERG.
2. THICKLY WOODED AREAS TO NORTH & EAST OF A/D.

AERODROME CHART NURNBERG

ILS R/W28 NURNBERG

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APPROACH NURNBERG APP

119-1 119-7 121-5

TOWER NURNBERG TWR

118-3 119-7 121-5

TRANS ALT 4000' FT.

G/S(KTS)	70	90	110	130	150
RTB TO TURN	4m17s	3m20s	2m44s	2m19s	2m00s
OM TO T/H	3m20s	2m36s	2m08s	1m46s	1m34s
DESCENT ON G/P(FT/MIN)	359	461	564	666	769
G/S	170	190	210	230	250
RTB TO TURN	1m46s	1m35s	m s	m s	m s
OM TO T/H	1m23s	1m14s	m s	m s	m s
DESCENT ON G/P(FT/MIN)	872	974			

CRITICAL HEIGHTS (FT)

WITH G/P 250 NO G/P 300

OVERSHOOT

CLIMB ON 281° TO 3000 /985 THEN TURN
RIGHT CLIMBING TO 3500 2485 TO NDB
'RTB' OR AS INSTRUCTED

ELEVATIONS (FT)

OFFICIAL AERODROME 1045

LOWEST THRESHOLD 1015

THRESHOLD R/W 28 1045

VARIATION 3°W Bearings all magnetic

SERIAL No. 40264/1 DATE 11 11 64

Reason for Re-issue:

REVISED CRIT. HGTS.

ILS R/W 28
NURNBERG

BLUE

YELLOW

ERLANGEN

RECIPROCAL OM

FURTH

ILS
DLN 109-1

L
NB 291-5

DUTZENOTEICH

NDB
RTB 415

HOLDING AND
LETDOWN
PATTERN
MIN. ALT. 3500

NAUTICAL MILES



INITIAL APPROACH
MIN. EN ROUTE ALT.

SHUTTLE ON
HOLDING PATTERN

LMM

OM

NDB

APPD. MIN.

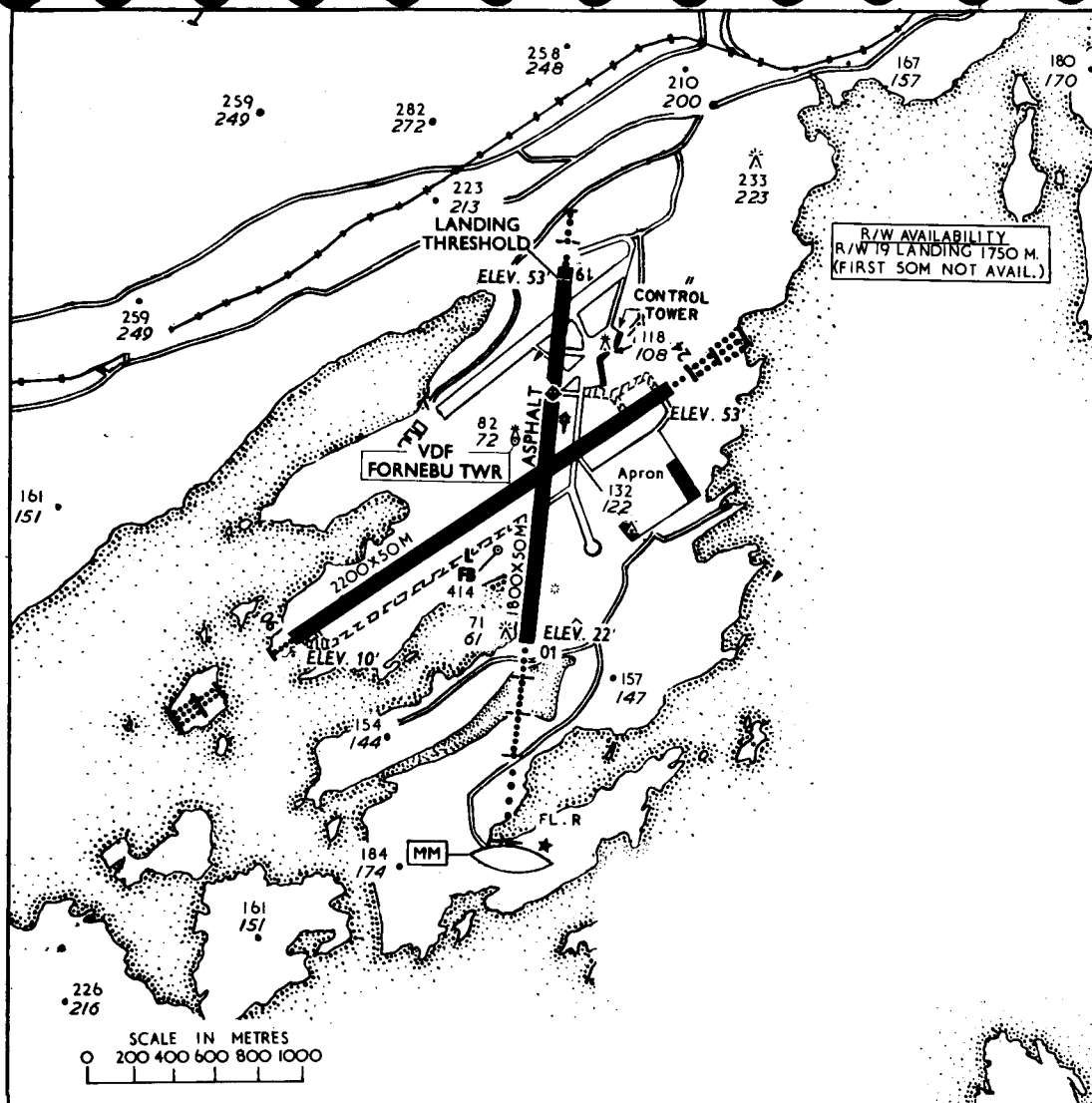
0-57 N.M.

3-9 N.M.

6-18 N.M.

G.P. HGT AT MM 1280 265

RATE ONE
TURN RIGHT
3100
2085



OSLO (FORNEBU) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
01	H.I. & L.I. CENTRE-LINE & BARS	3°
19	L.I. CENTRE-LINE & BARS	
06	H.I. & L.I. CENTRE-LINE & BARS	
24	H.I. & L.I. CENTRE-LINE & BARS	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
01	H.I. & L.I. (ELEV.)	H.I. & L.I. (ELEV.)
19	H.I. & L.I. (ELEV.)	L.I.
06 & 24	GREEN	H.I. DIRECTIONAL & L.I. OMNI-DIRECTIONAL WHITE

OBSTRUCTION, TAXIWAY, LANDING INDICATOR,
BOUNDARY, CIRCLING GUIDANCE (R/W 19)

ELEVATIONS (FT) : —

OFFICIAL AERODROME 56

LOWEST THRESHOLD (06) 10

LAT 59° 54' N, LONG 10° 37' E

VARIATION 3° W Bearings all magnetic

SERIAL No. 50255/2 DATE: 30.6.65

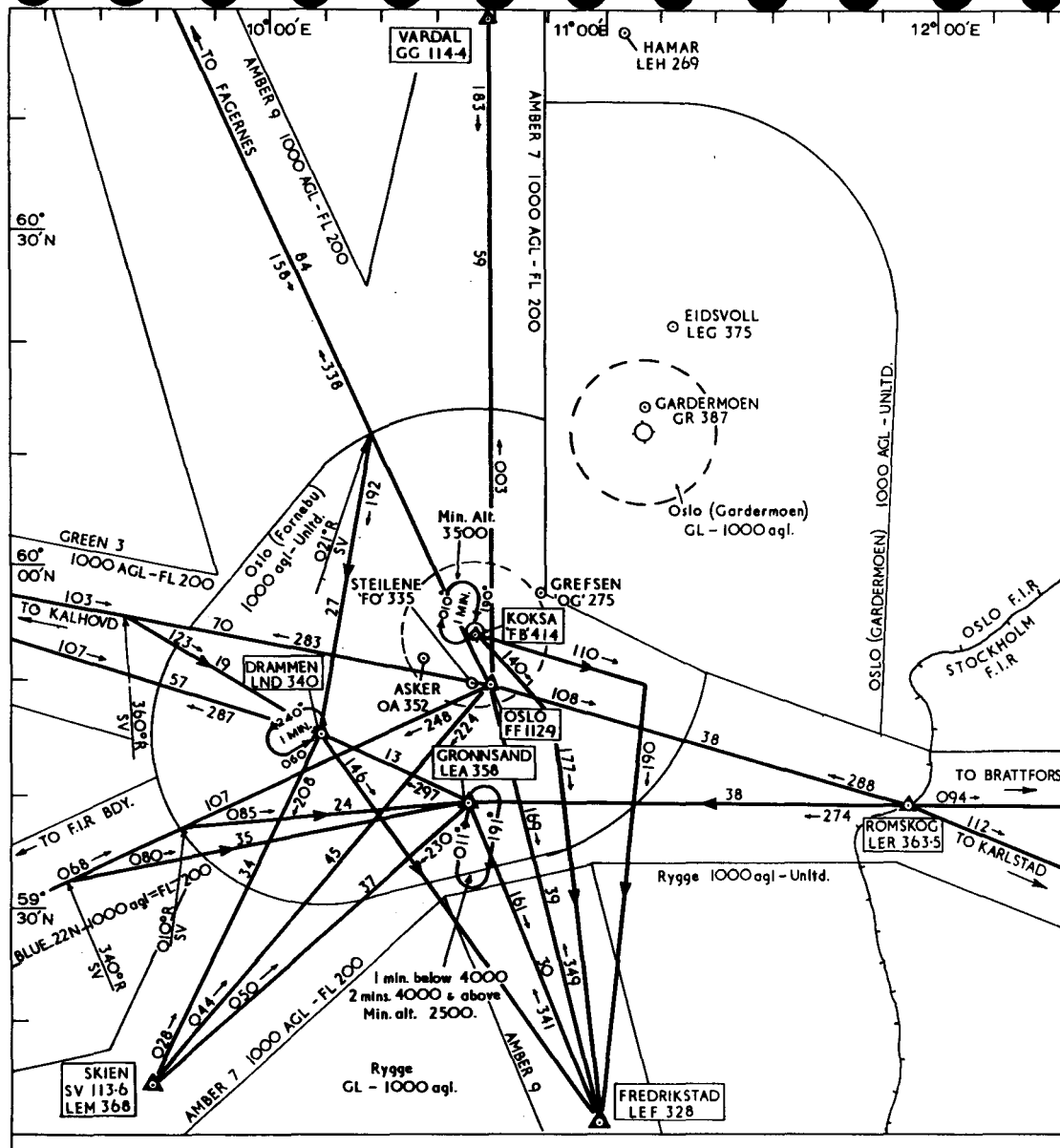
Reason for Re-Issue:
VASI R/W 01 ADDED

NOISE ABATEMENT PROCEDURES: LANDING ON R/W 19 & T/O R/W 01 ONLY WHEN USE OF OTHER R/W'S IMPRACTICABLE BUT PROHIBITED
ALL TIME TO JET A/C. TAKE-OFF R/W 19 FOR JET A/C PROHIBITED A/D STRICTLY PPO HN.

STOPWAYS: R/W'S 06/24: - 60M.

R/W MARKER BOARDS: INDICATE ONE THIRD & TWO THIRDS OF R/W 06/24.

AERODROME CHART OSLO (FORNEBU)



OSLO (FORNEBU)

INBOUND & OUTBOUND ROUTES

BRITISH EUROPEAN AIRWAYS

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VARIATION 3° W Bearings all magnetic

SERIAL No. 40355 / 5 DATE: 15.12.64

Reason for Re-issue:

TMA BDYS. & AWY. AMBER 7

INBOUND & OUTBOUND ROUTES

OSLO (FORNEBU)

OSLO (GARDERMOEN) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
20	H.I. & L.I. WHITE, 3 WHITE BARS	
02	H.I. & L.I. WHITE, WITH 1 WHITE & 2 SODIUM BARS	
11/29	ELEVATED L.I. WHITE.	

N.B. NO ENTRY UNDER VASI. ANGLE INDICATES NO INSTALLATION

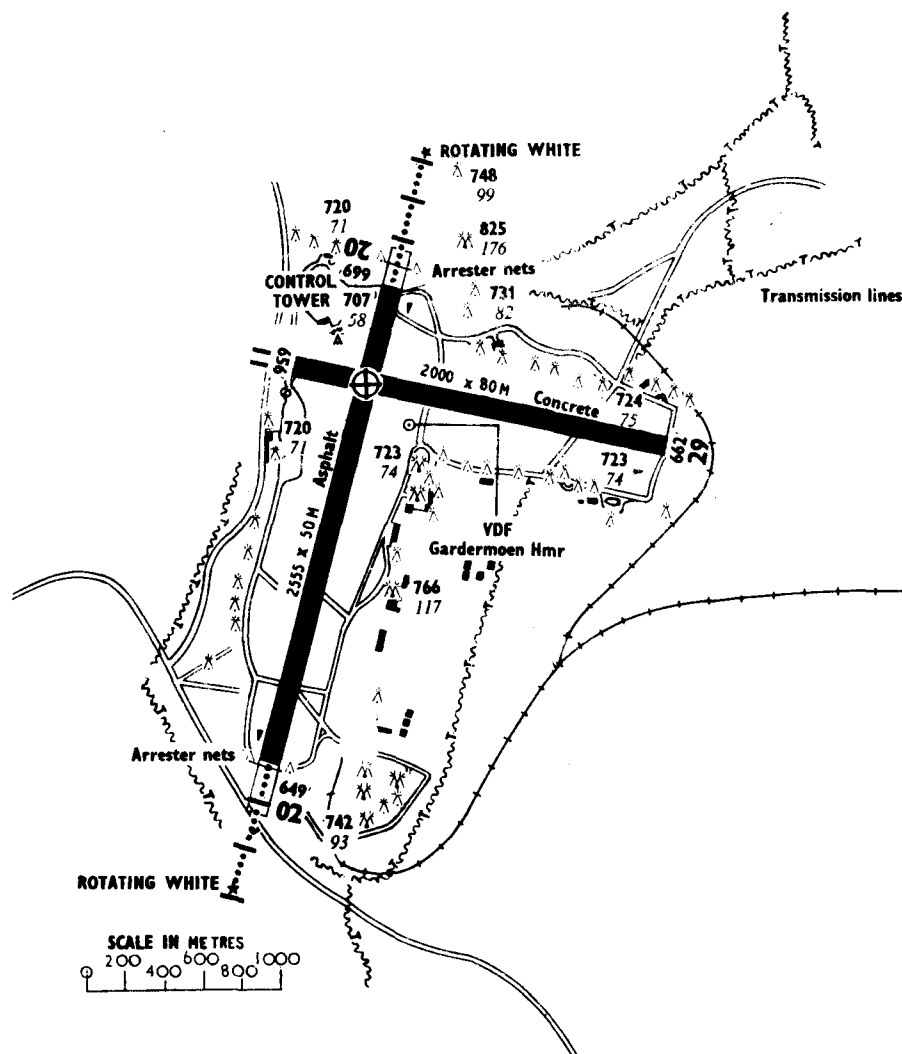
R/W	THRESHOLD	RUNWAY
20	H.I. GREEN WINGS	ELEVATED H.I. WHITE
02	H.I. GREEN WINGS	ELEVATED H.I. WHITE
11/29	L.I. GREEN	ELEVATED L.I. WHITE

OBSTRUCTION, TAXIWAY, (SCOTCH LITES IN
WINTER), ADME BEACONS, AUX. GLIM LAMPS
THRESHOLD 02/20.

ELEVATIONS (FT)	a.m.s.l. 766 above lowest T/H 117
OFFICIAL AERODROME	669
LOWEST THRESHOLD (02)	649
LAT 60° 12' N, LONG 11° 05' E	
VARIATION 3° W	Bearings all magnetic
SERIAL No. 39301	DATE: 5.8.64
Reason for Re-issue:	

AERODROME CHART

OSLO (GARDERMOEN)



STOPWAYS: R/W 02: 175M R/W 20: 260M (BOTH CONCRETE.)

ELEVATED: 1. R/W & T/H LIGHTS ELEVATED 17INS SET BACK 1M.

LIGHTING: FROM EDGES.

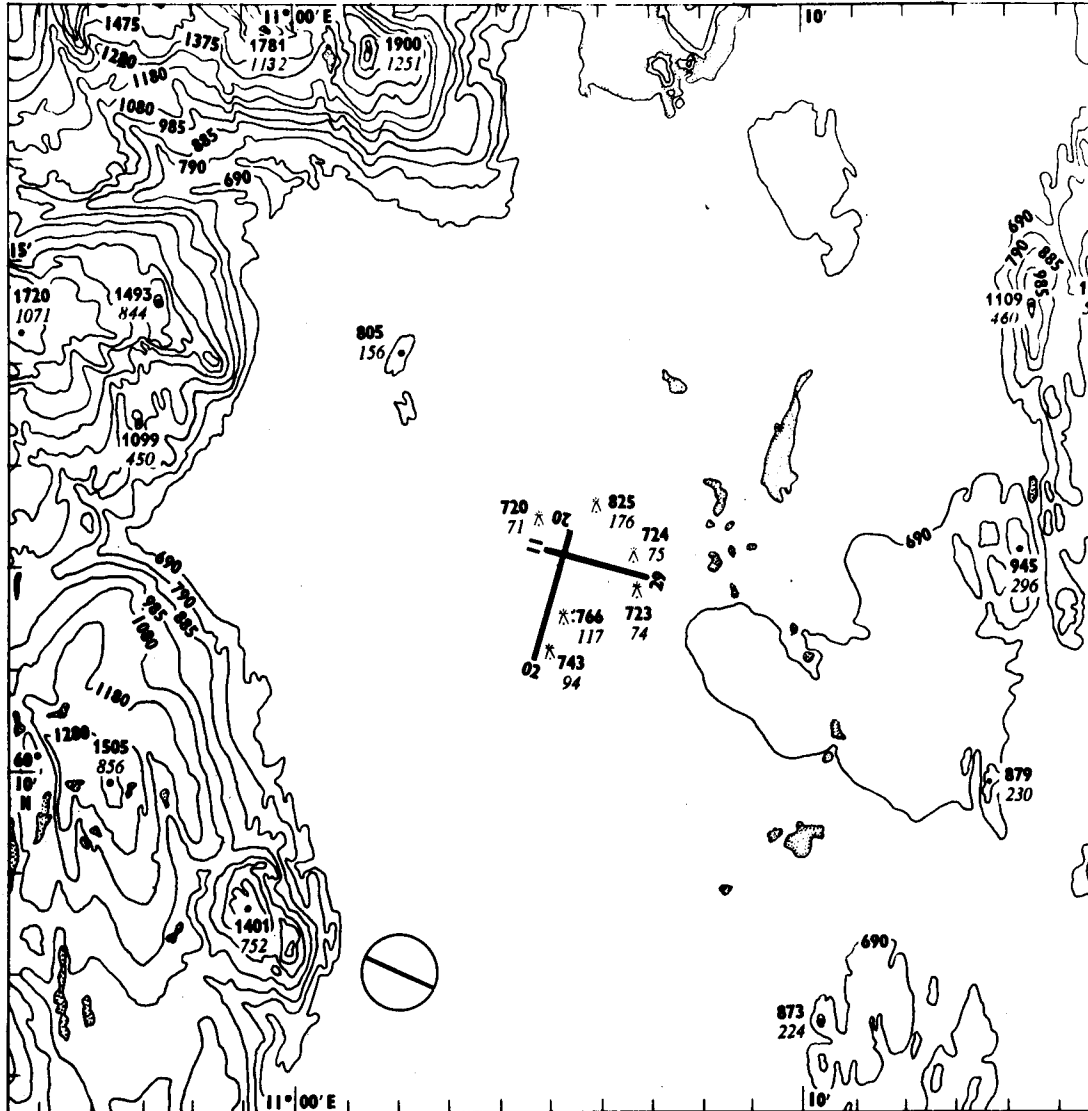
2. T/W LIGHTING ELEVATED 12INS, SET AT EDGES.

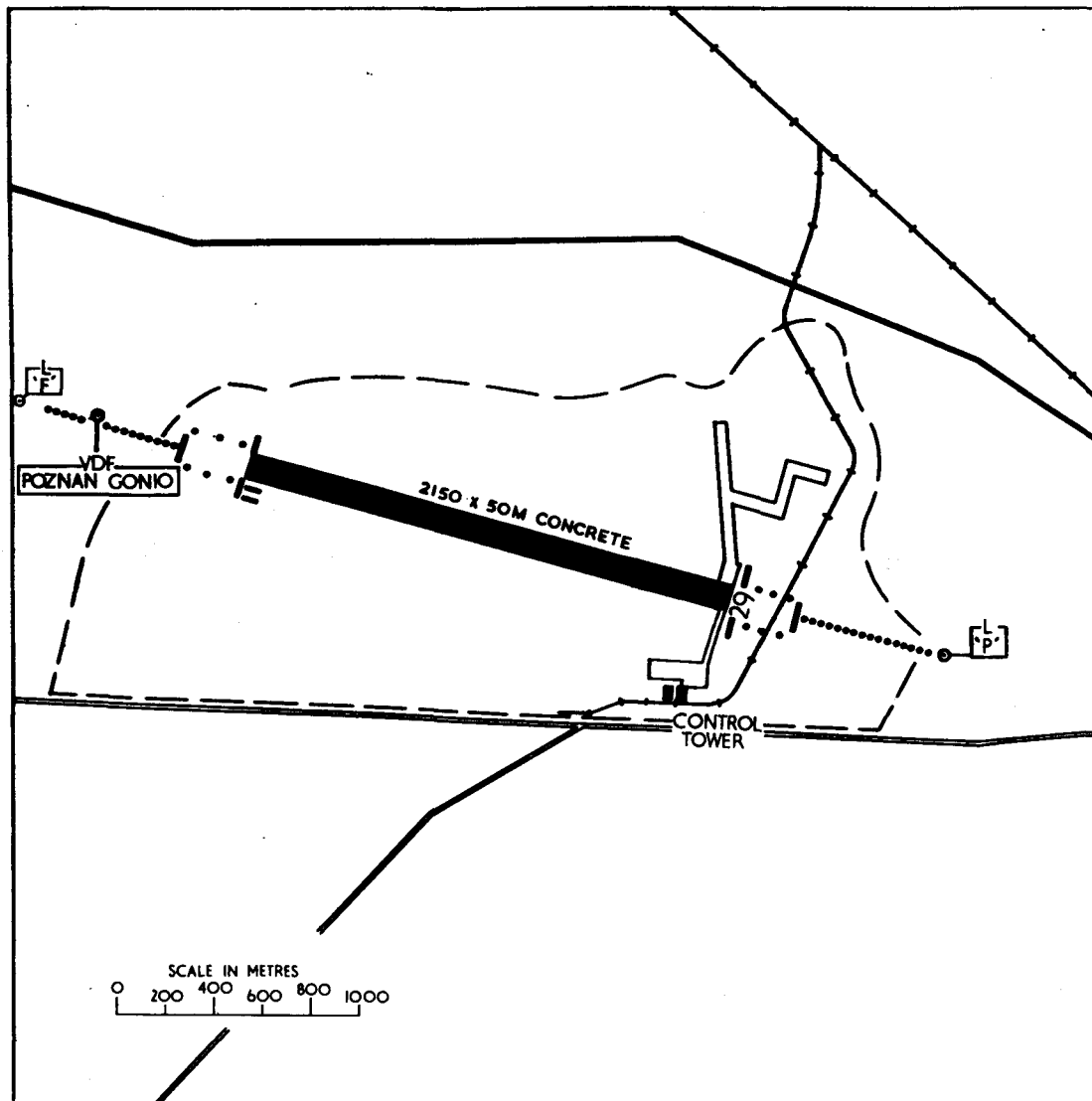
WARNING:

1. CAUTION WHEN LANDING AFTER PROLONGED RAINFALL
OR THAW.

2. ARRESTING GEAR ON STOPWAYS 34M. FROM R/W 02 &
81M. FROM R/W 20.

VISUAL APPROACH CHART OSLO (GARDERMOEN)





POZNAN (LAWICA) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
11/29	RED CENTRE-LINE & BAR	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
11/29	GREEN	L.L. WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND AND LANDING INDICATOR

ELEVATIONS (FT): —

OFFICIAL AERODROME 295

LOWEST THRESHOLD () 295

LAT 52° 25' N, LONG 16° 50' E

VARIATION 1° W Bearings all magnetic

SERIAL No. 40259/1 DATE: 12-2-65

Reason for Re-issue:

L.F. ADDED.

AERODROME CHART POZNAN (LAWICA)

Sect. Safe. Alt.
25 n.m. 1500'

40°

50'

17°00'E

Sect. Safe. Alt.
25 n.m. 1500'

POZNAN L/BCN. R/W II

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APPROACH POZNAN TWR

118.1 3023.5

TOWER POZNAN TWR

118.1 3023.5

TRANS ALT 1650 (QFE) FT.

G/S (KTS)	70	90	110	130	150
'LF TO TURN	2 09	1 40	1 22	1 09	1 10

G/S	170	190	210	230	250
'LF TO TURN	0 53	0 47	0 43	0 39	0 36

MISSED APPROACH

CLIMB AHEAD ON 108° TO 1295 /000
OR AS INSTRUCTED BY TWR

ELEVATIONS (FT)

OFFICIAL AERODROME 295

LOWEST THRESHOLD 295

THRESHOLD R/W II 295

VARIATION 1°W Bearings all magnetic

SERIAL No. 50398 DATE: 29.7.65

Reason for Re-issue:

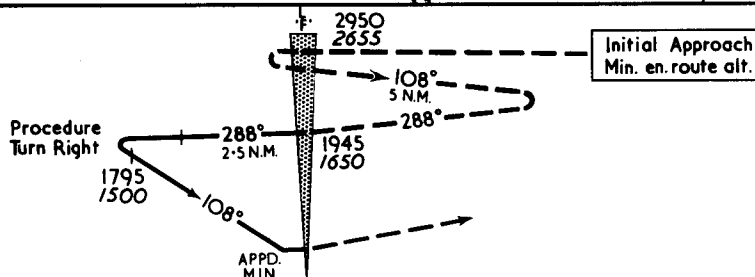
L/BCN R/W II
POZNAN

52°
30'N

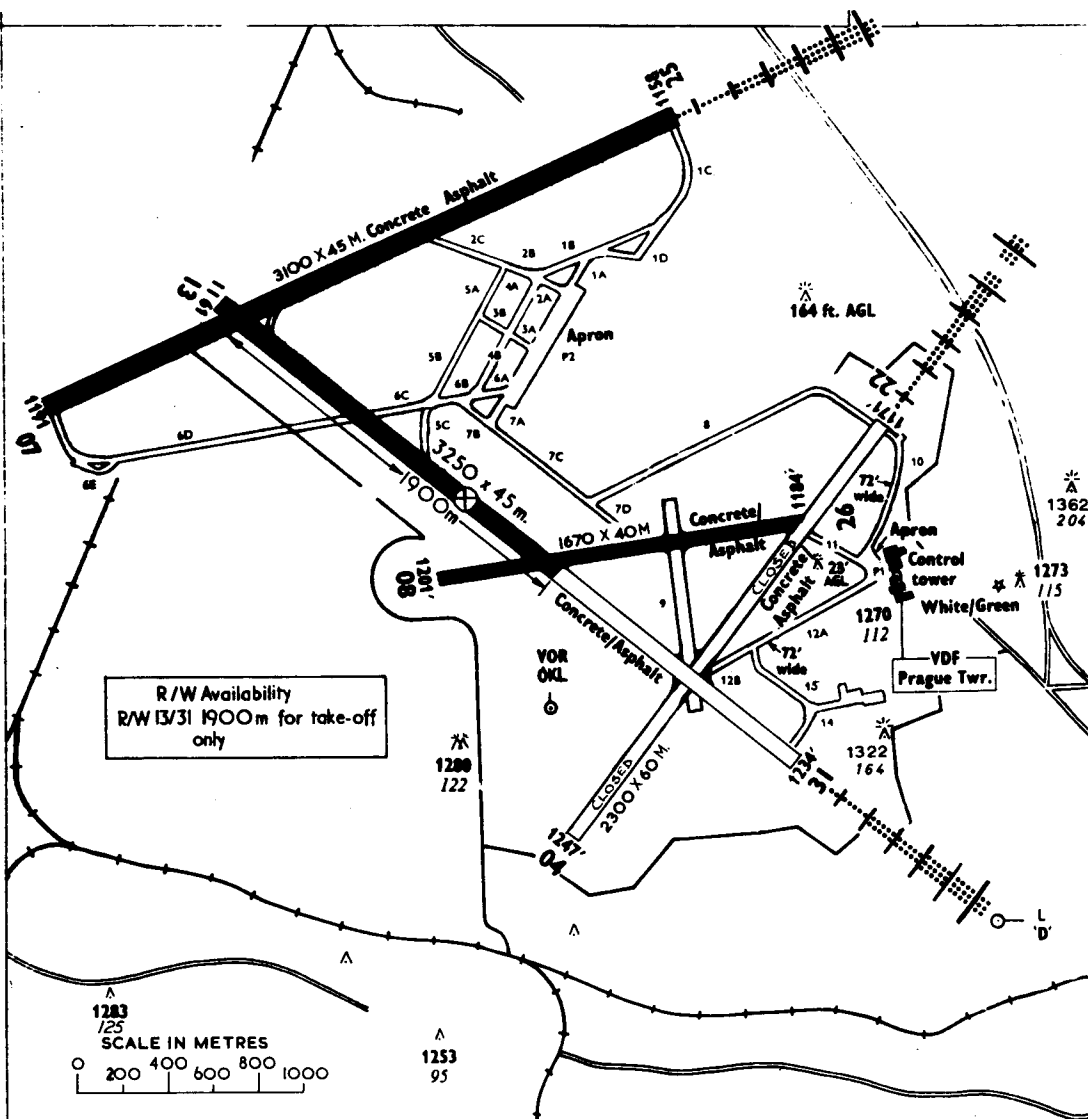
20'

Sect. Safe. Alt.
25 n.m. 1500'

Sect. Safe. Alt.
25 n.m. 1500'



WNW N.M. 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 ESE NM



- NOTES**
1. R/W 13/31 NOT AVAILABLE FOR TAKE-OFF BY JET A/C.
 2. TAXIWAY 14 UNLIT.
 3. TAXIWAYS 9 & 12 B MAY BE CLOSED DUE WIP.

PRAGUE (RUZYNE) **AERODROME CHART**

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
22	H.I. WHITE CENTRE-LINE & SODIUM BARS RED MED. INT. CENTRE-LINE	
04	NIL	
31	H.I. WHITE CENTRE-LINE & SODIUM BARS LOW INT. CENTRE-LINE AND BAR	
13	NIL	
08/26	NIL	
07	LOW INT. CENTRE-LINE (1000 M) WITH CROSSBARS AT 300 & 600 M	
25	H.I. WHITE CENTRE-LINE & SODIUM BARS MED. INT. CENTRE-LINE & BARS	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
22	GREEN	H.I. & LOW INT. WHITE
04	GREEN	H.I. & LOW INT. WHITE
31	GREEN	H.I. & LOW INT. WHITE TOUCHDOWN ZONE 300M H.I. WHITE SODIUM WINGBARS
13	GREEN	H.I. & LOW INT. WHITE
08/26	GREEN	LOW INT. WHITE
07	GREEN	H.I. & LOW WHITE
25	GREEN & GREEN WING BARS	H.I. & LOW INT. WHITE. TOUCHDOWN ZONE 910M WITH CROSSBARS AT 300M UP RUNWAY

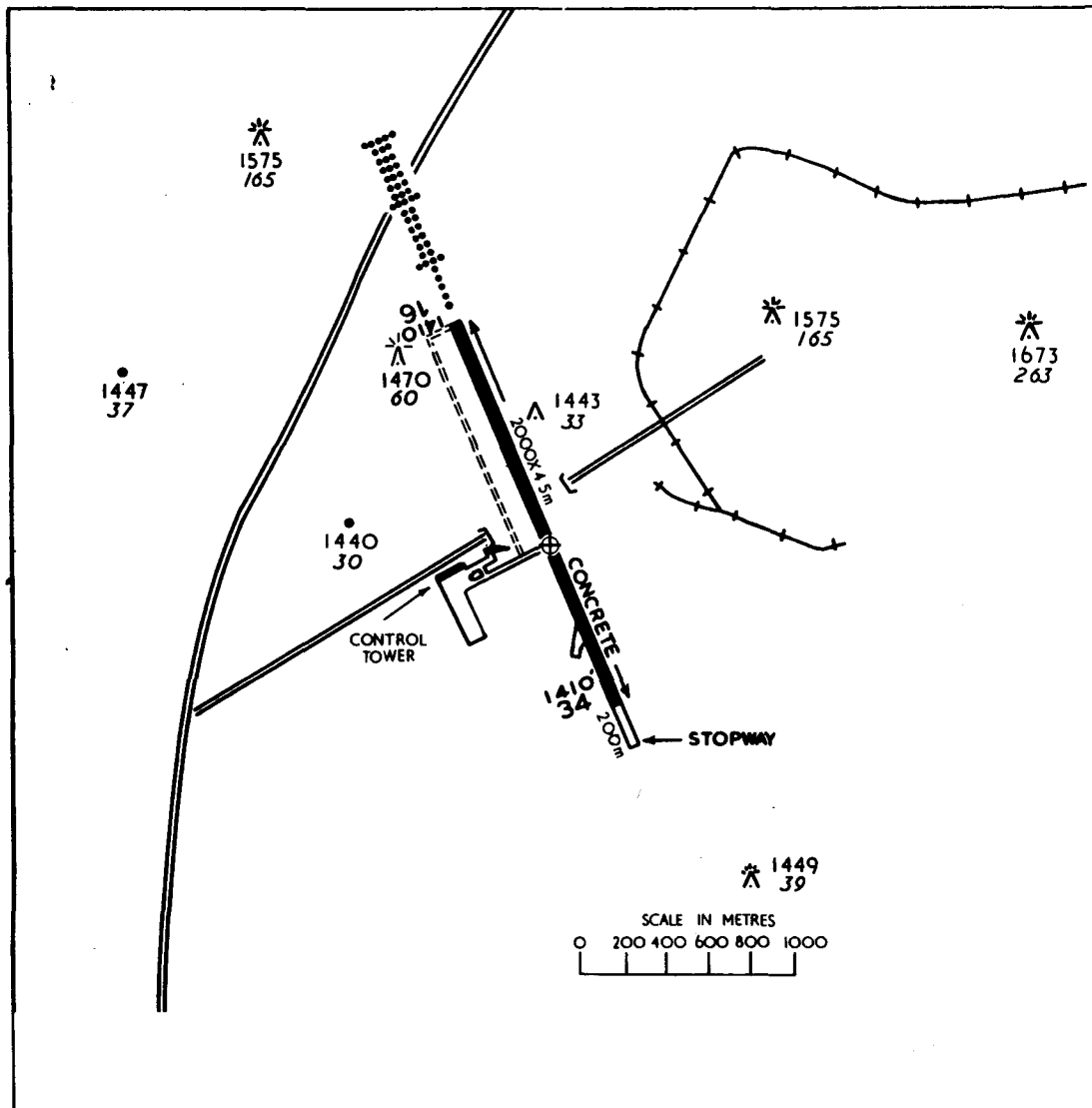
OBSTRUCTION, TAXIWAY, A'DME BEACON
WIND INDICATOR. TAXIWAY NO.15 - NO
LIGHTS-REFLECTIVE GLASSES ONLY

ELEVATIONS (FT) :-

OFFICIAL AERODROME	1247
LOWEST THRESHOLD (25)	1158
LAT 50° 06' N, LONG 14° 16' E	
VARIATION 1° W	Bearings all magnetic
SERIAL No. 50206/2	DATE 28.4.65

Reason for Re-issue:
R/W 13/31 AVAILABILITY

AERODROME CHART **PRAGUE (RUZYNE)**



SALZBURG AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI/ ICAO Type Angle
16	H.I. WHITE CENTRE-LINE 915M. & 3 BARS	X
34	NIL	X

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
16	H.I. GREEN	H.I. WHITE & YELLOW
34	H.I. GREEN	H.I. WHITE & YELLOW

OBSTRUCTION, TAXIWAY, ADME BEACON
LANDING INDICATOR, APRON FLOODS,
ANGLE OF APPROACH INDICATORS.

ELEVATIONS (FT)

OFFICIAL AERODROME 1410

LAT 47°48'N, LONG 13°00'E

VARIATION 2°W Bearings all magnetic

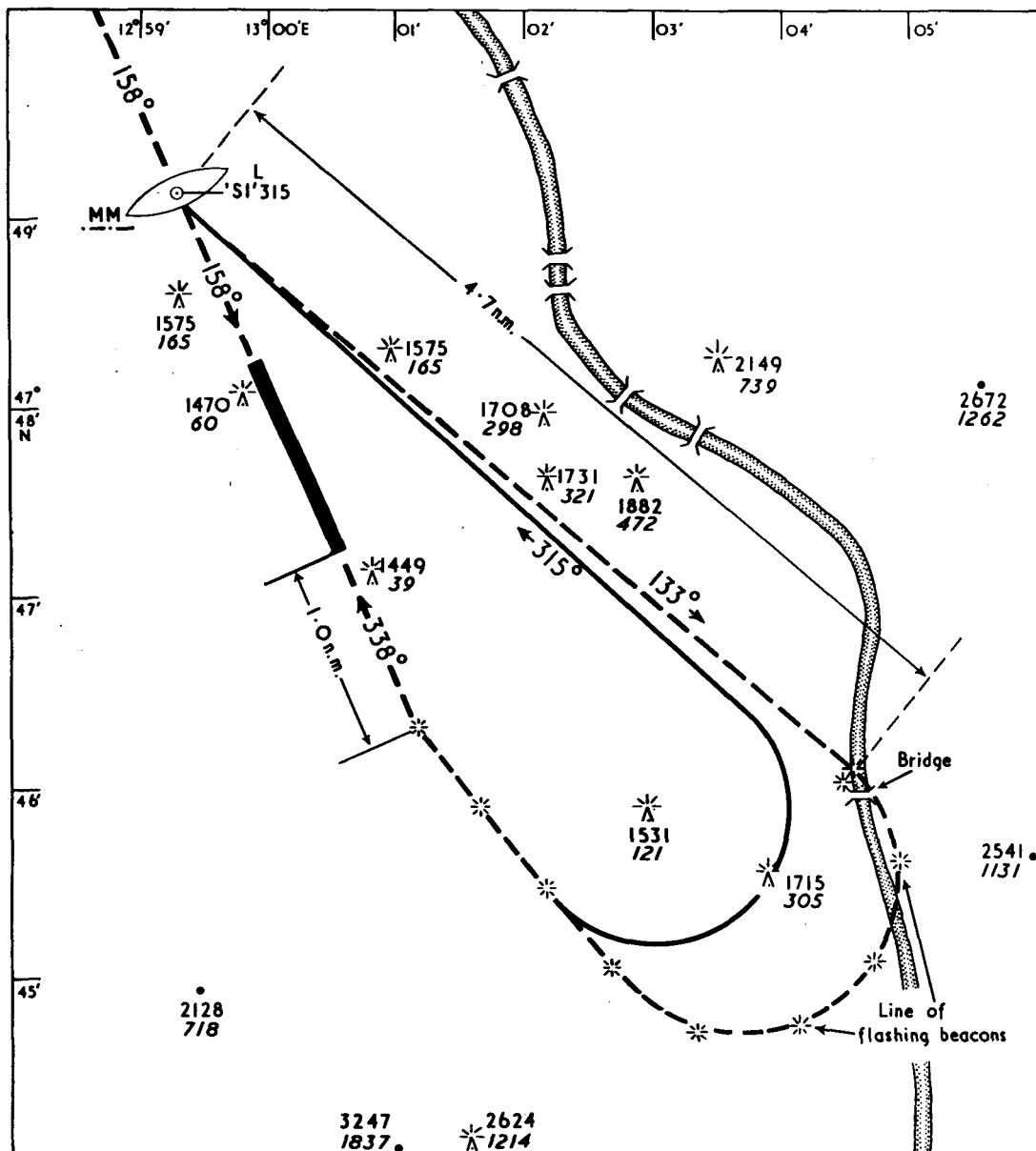
SERIAL No. 50366/1 DATE: 10. 6. 65.

Reason for Re-Issue:

OBSTRUCTIONS, VASI & TAXIWAY.

AERODROME CHART SALZBURG

NOTE: STOPWAY OF 200M. AVAILABLE FOR T/O R/W 34 ON SPECIAL REQUEST TO CTL.



SALZBURG

SPECIAL PROCEDURES R/W 16/34

BRITISH EUROPEAN AIRWAYS

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ATTENTION:— All flights south of aerodrome to be carried out within pattern marked by broken line.

APPROACH LIMITATIONS:— Airspeed Max 150 Kts.

LANDING R/W 34:— After passing locator (SI) proceed visually on 133° for 4.7 nm to join special traffic pattern marked by 9 flashing yellow beacons.

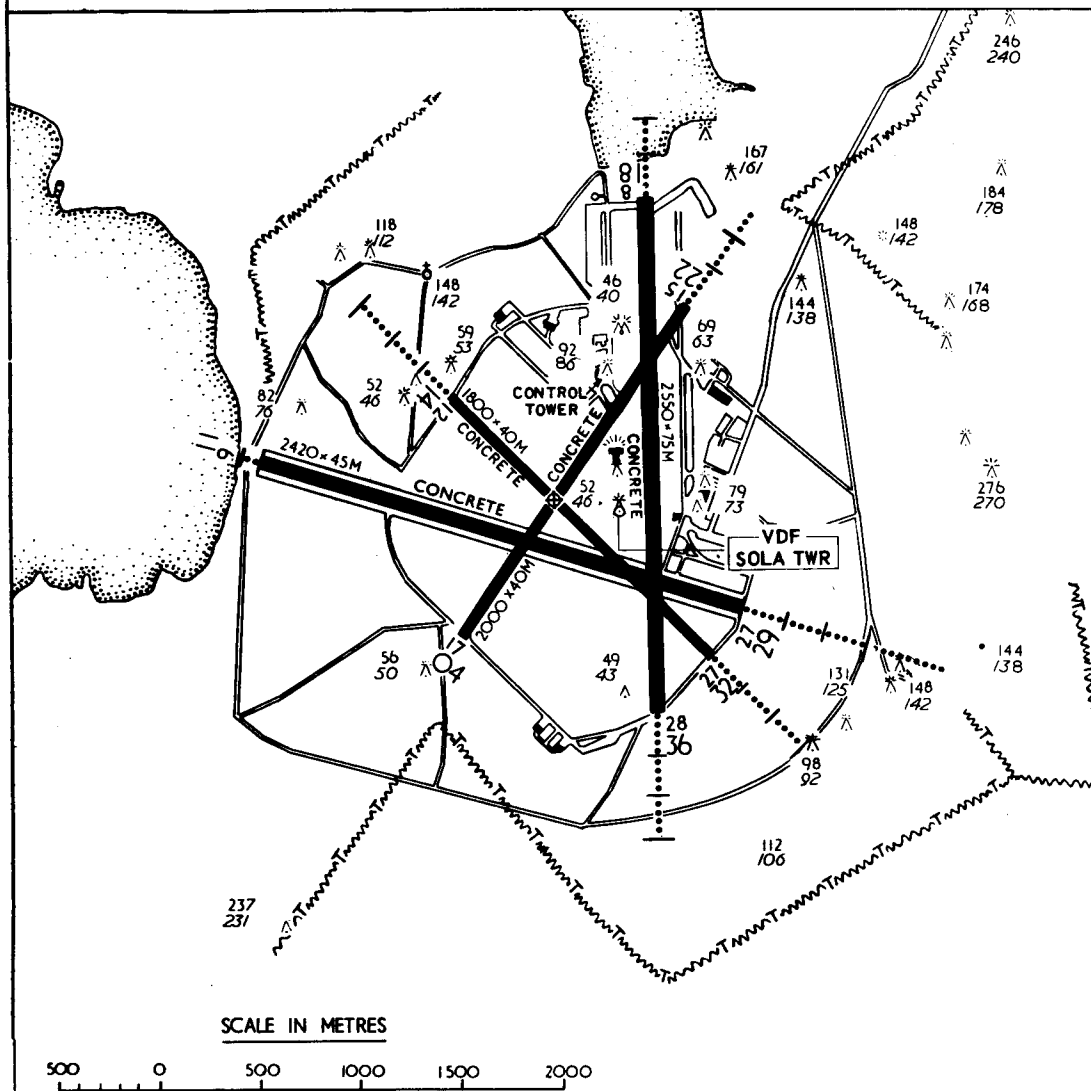
TAKE-OFF R/W 16:— After take-off proceed visually on left turn out according to outbound pattern marked by solid line to locator (SI)

ELEVATIONS (FT)

OFFICIAL AERODROME	1410
LOWEST THRESHOLD	
THRESHOLD R/W	
VARIATION 2°W	Bearings all magnetic
SERIAL No. 50366/1	DATE: 10. 6. 65.
Reason for Re-issue:	
Presentation	

SPECIAL PROCEDURES R/W 16/34

SALZBURG



DISTANCE TO GO MARKERS:-

STOPWAY :- R/W 36,90 M.

SECT. SAFE ALT.
25 NM. 3100'

05° 20'

MIN. HOLDING
ALT. 3000'

NDB
RENNESØY
LII 398

SECT. SAFE ALT.
25 NM. 4800'

STAVANGER
ILS R/W 18

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH

TOWER

SOLA TWR.

119.6 118.1

TRANS ALT

4000 FT.

G/S (KTS)	70	90	110	130	150
LOM TO ADME.	3.16	2.32	2.04	1.45	1.31
DESCENT ON G/P (FT/MIN)	370	475	585	690	795
G/S	170	190	210	230	250
LOM TO ADME.	1.20	1.12			
DESCENT ON G/P (FT/MIN)	905	1010			

OVERSHOOT

CLIMB AHEAD TO 600 594
TURN RIGHT ONTO 270° 6
CLIMB TO 2500 2494

ELEVATIONS (FT):-

OFFICIAL AERODROME	28
LOWEST THRESHOLD	6
THRESHOLD R/W 18	8
VARIATION 5°W	Bearings all magnetic
SERIAL No. 50155/2	DATE: 14.6.65
Reason for Re-issue:	
APPROACH DELETED	

ILS R/W 18
STAVANGER

BLUE

YELLOW

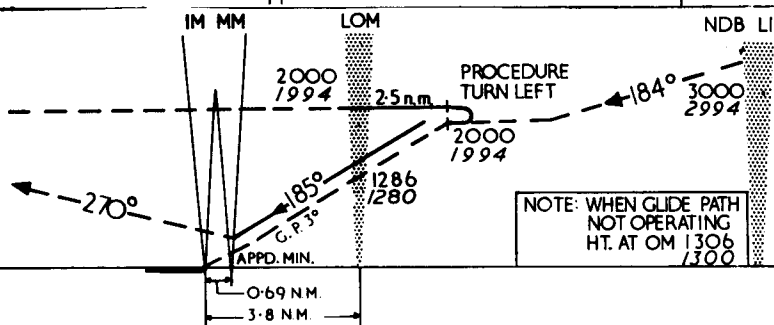
59
00N

50'

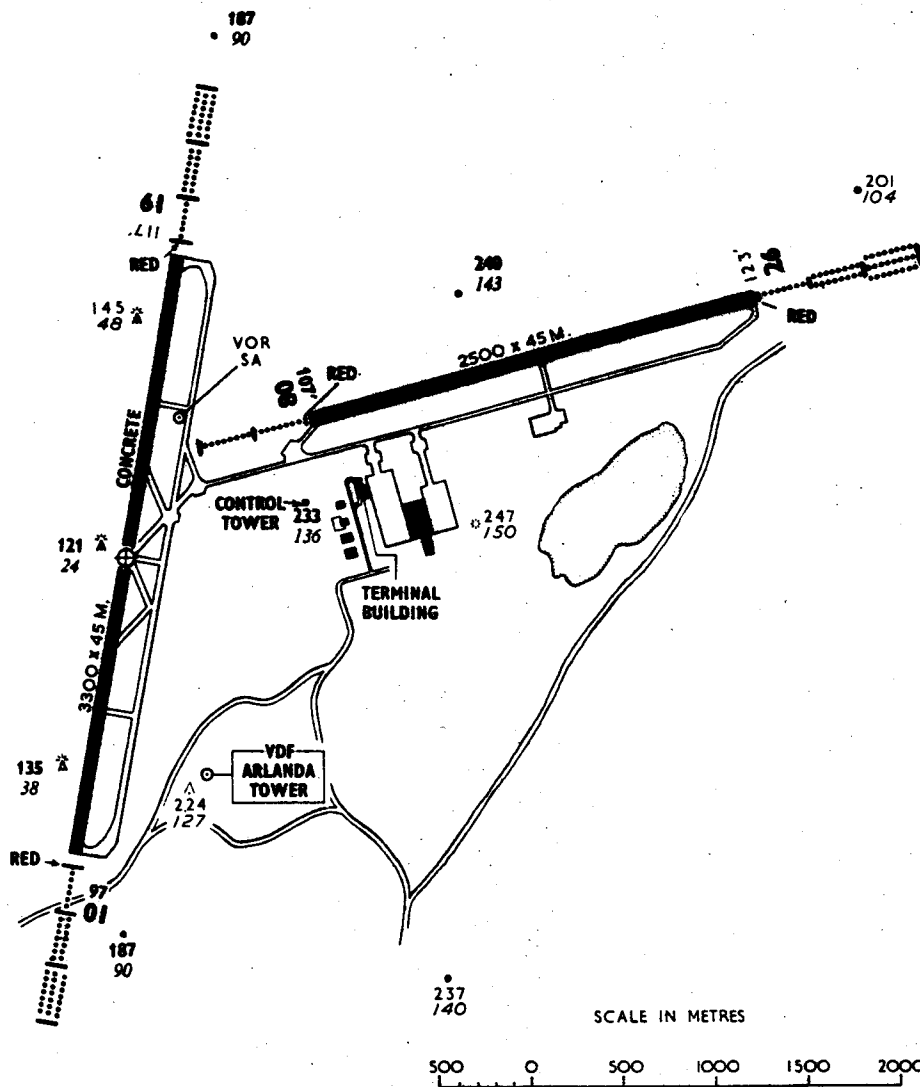
SECT. SAFE ALT.
25 NM. 2200'

SECT. SAFE ALT.
25 NM. 4800'

INITIAL APPROACH
MIN. EN ROUTE ALT.



NOTE: WHEN GLIDE PATH
NOT OPERATING
HT. AT OM 1306
1300



STOCKHOLM(ARLANDA)

AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
01/19	WHITE H.I. CENTRE-LINE & BARS WHITE & GREEN L.I. CENTRE-LINE	
08	WHITE & GREEN L.I. CENTRE-LINE, WHITE L.I. BARS	
26	WHITE H.I. CENTRE-LINE & BARS, WHITE & GREEN L.I. CENTRE-LINE	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
01/19	H.I. & L.I. GREEN WITH H.I. & L.I. WHITE WING BARS AT 143 M. INTERVALS ON FIRST 915 M. OF R/W.	WHITE H.I. CENTRE-LINE & WHITE H.I. & L.I. EDGE LIGHTS WITH LAST 900 M. AMBER.
08 26	H.I. & L.I. GREEN	WHITE H.I. & L.I. WITH LAST 600 M. AMBER

OBSTRUCTION, TAXIWAY.

ELEVATIONS (FT) : —

OFFICIAL AERODROME 113

LOWEST THRESHOLD (01) 97

LAT 59° 39' N. LONG 17° 55' E

VARIATION 1° E Bearings all magnetic

SERIAL No. 40351/1 DATE: 18-12-64

Reason for Re-issue:

LIGHTING REVISED.

SPECIAL LIGHTING : H.I. & L.I. RED WARNING BAR 60 M. BEFORE THRESHOLDS OF 01, 19, & 26.
L.I. RED WARNING BAR 60 M. BEFORE THRESHOLD OF 08.
WHITE LANDING ZONE ILLUMINATING LIGHTS ON FIRST 900 M. OF 01 & 19.

AERODROME CHART

STOCKHOLM(ARLANDA)

CONTROL ZONES OF MILITARY A/D's & APPROACH SECTOR OF BARKARBY
MAY BE FLOWN THROUGH ONLY WITH A CLEARANCE FROM A.T.C. FLIGHTS
THROUGH BARKARBY CTR. SHOULD BE MADE S.W. OF A LINE THROUGH
SPANGA RAILWAY STATION (59°23'N 17°54'E) & THE ISLE OF FODERHOLMEN
(59°27'N 17°46'E).

17°30'E 18°100'E 18°30'E

ARLANDA
OA 404
GL - 1700'

ARLANDA
SA 112-7

FLORA
'FA' 258

ARLANDA
OH' 370

KARBO
'KAR' 534

BARKARBY
GL - 1700'

BROMMA
OB' 364

BROMMA
TA' 322

NACKA
'SN' 335

HILL
SEC 388

AKER
SIB 309-5

ECHO
SEK 330

TROSA
SIT 381
TR 114-2

STOCKHOLM TMA 1000 AGL - FL 200

STOCKHOLM TMA 1000 AGL - FL 200

KLACKEN IN

R5 UNLTD.

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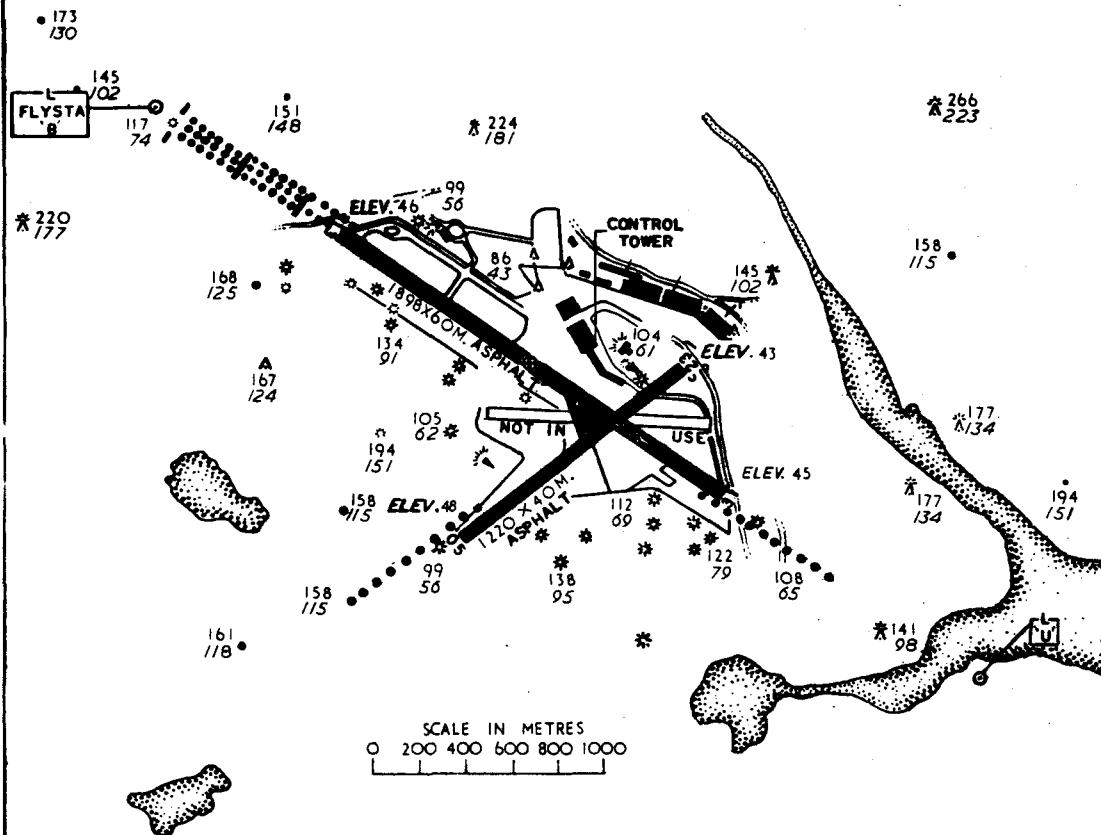
HOLDING PATTERNS

HOLDING POINT	MIN ALT. FT.	TIME MINS	INBOUND HEAD. °M	TURN	MAX TAS KTS
BROMMA NDB OB	2500	1	009	LEFT	200
BROMMA NDB OB (HINGED TO HILL NDB)	FL 150	2	360	LEFT (LEFT 1/2)	280
ECHO NDB	2500	1	211	LEFT	200
FLORA NDB	2500	1	255	RIGHT	200
HILL NDB	2500	1	026	LEFT	150
KARBO NDB	2500	1	240	RIGHT	150
NACKA NDB	2500	1	295	RIGHT	150

OUTBOUND

TROSA VOR & REVISED PRESENTATION

IN & OUTBOUND ROUTES
(ARLANDA)
STOCKHOLM



STOCKHOLM (BROMMA) AERODROME CHART

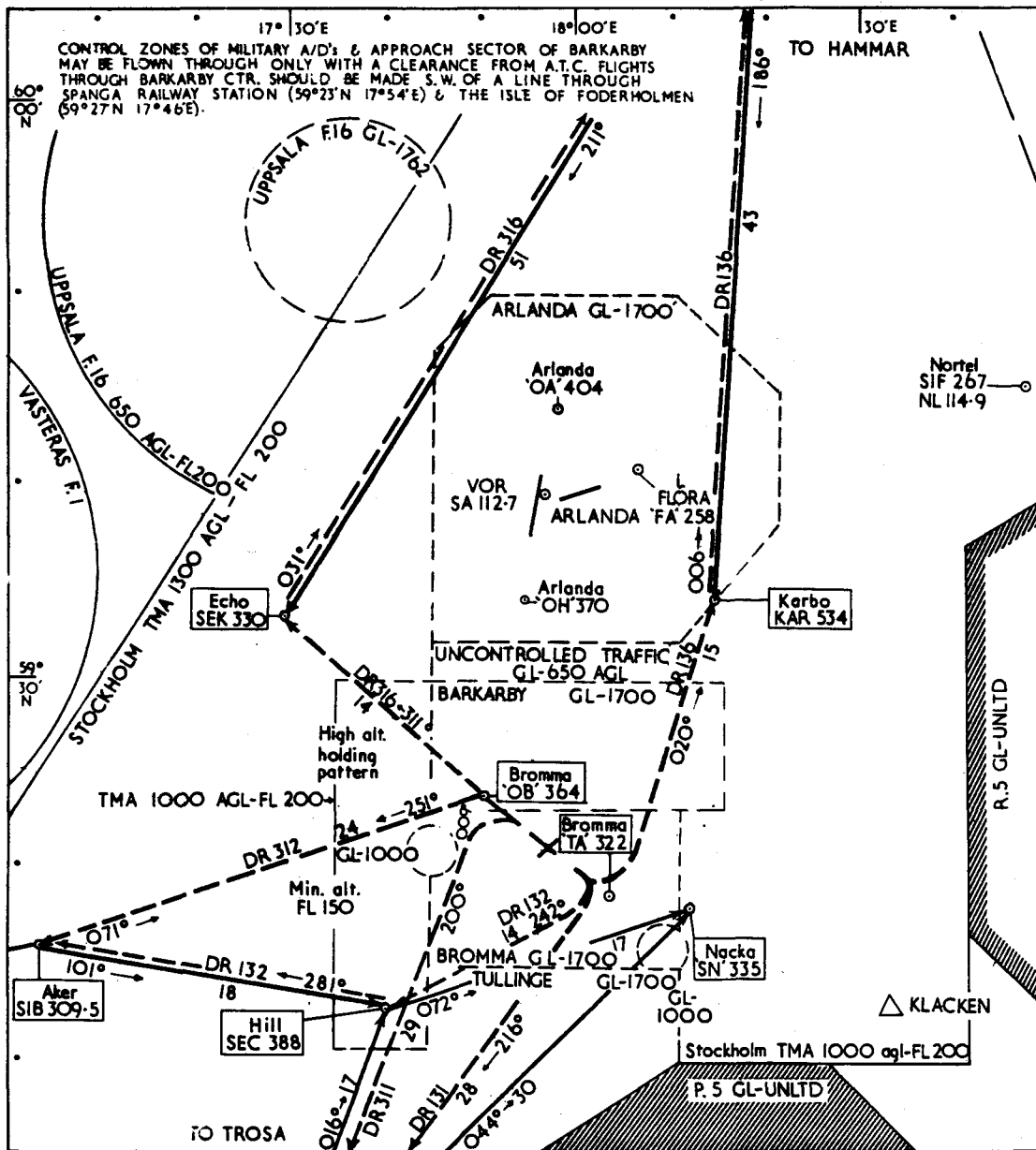
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R/W	APPROACH	Type Angle
13	H.I. WHITE C/L & BARS L.I. WHITE & GREEN LEFT HAND	
31	L.I. WHITE & GREEN LEFT HAND	
05, 23	L.I. WHITE & GREEN LEFT HAND	

R/W	THRESHOLD	RUNWAY
13 31	H.I. & L.I. GREEN WINGBARS	H.I. & L.I. WHITE
05, 23	L.I. GREEN WINGBARS	L.I. WHITE

ELEVATIONS (FT):—

OFFICIAL ELEVATIONS



STOCKHOLM BROMMA INBOUND & OUTBOUND ROUTES & HOLDING PATTERNS

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HOLDING PATTERNS

HOLDING POINT	MIN. ALT. FT.	TIME MINS	INBOUND HEADING °M	TURN	MAX TAS KTS
BROMMA NDB OB	2500	1	009	LEFT	200
BROMMA NDB OB (HINGED TO HILL NDB)	FL 150	2	360	LEFT	280
ECHO NDB	2500	1	211	LEFT	200
FLORA NDB	2500	1	255	RIGHT	200
HILL NDB	2500	1	026	LEFT	150
KARBO NDB	2500	1	240	RIGHT	150
NACKA NDB	2500	1	295	RIGHT	150

LEGEND

INBOUND ROUTES: ———
OUTBOUND ROUTES: - - - - -

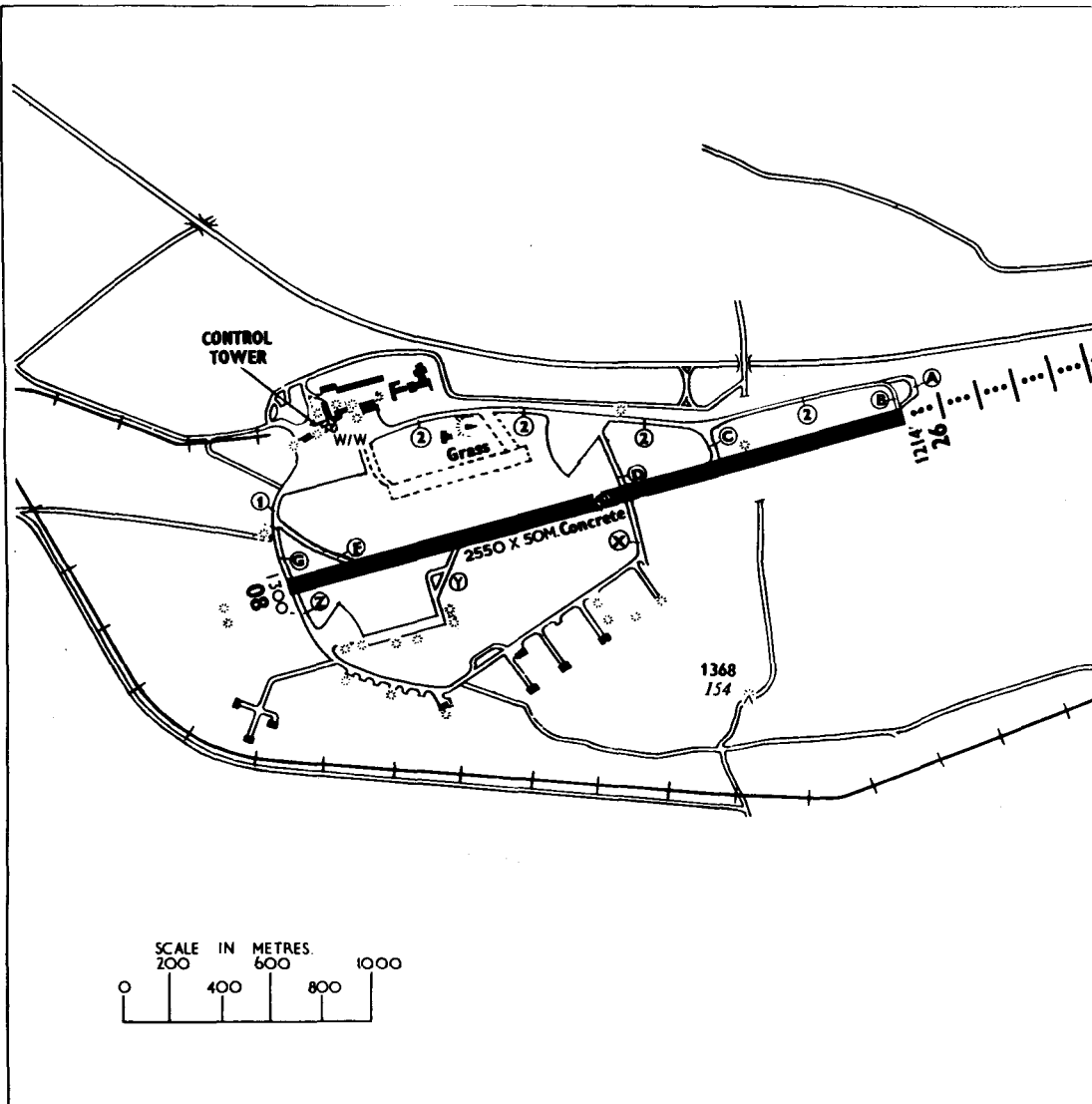
VARIATION 1° E Bearings all magnetic

SERIAL No. 50496/3 DATE 12.8.65

Reason for Re-issue:

NOTE DELETED

INBOUND & OUTBOUND ROUTES
& HOLDING PATTERNS
STOCKHOLM BROMMA



- NOTES: 1 R/W & T/WAY LIGHTS ELEVATED.
2. T/WAY WIDTHS 1 & 2 G-75M., 2, B, C, E & F-22½M., D-18M.

STUTTGART AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
26	WHITE H.I. UNI-DIR. CENTRE LINE 915M WITH 6 CROSSBARS AND SEQUENCED FLASHERS	
08	NIL	

N.B. NO ENTRY UNDER VASI: ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
26 & 08	GREEN H.I. & L.I. WINGBARS, WITH 2 WHITE FLASHING LIGHTS ON OUTER ENDS	WHITE H.I. UNI-DIR. AND L.I. OMNI-DIR. WITH WHITE WINGBAR 265M. DOWN R/W 26

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	1300
LOWEST THRESHOLD (26)	1214
LAT 48° 41' N, LONG 09° 13' E	
VARIATION 3° W	Bearings all magnetic
SERIAL No. 50131/2	DATE: 23.2.65

Reason for Re-Issue:

ADME. ELEV.

AERODROME CHART STUTTGART

SECT. SAFE ALT.
25 N.M. 4500'

LUDWIGSBURG

120'
FROM
LUBURG

SECT. SAFE ALT.
25 N.M. 3600'

STUTTGART

ILS R/W 26

BRITISH EUROPEAN AIRWAYS

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APPROACH	STUTTGART	APP
191.2	119.7	121.5E
TOWER	STUTTGART	TWR
118.8	119.7	121.5E
TRANS ALT	4000 FT.	

G/S (KTS)	70	90	110	130	150
LBU TO TURN	8.04	6.16	5.08	4.20	3.45
LOM TO T/H	3.33	2.46	2.15	1.55	1.39
DESCENT ON G/P (FT/MIN)	322	414	506	598	690
G/S	170	190			
LBU TO TURN	3.19	2.58			
LOM TO T/H	1.28	1.18			
DESCENT ON G/P (FT/MIN)	781	873			

OVERSHOOT

CLIMB ON 257° TO 3000 1786 TURN LEFT
TO TANGO CLIMBING TO 4000 2786

ELEVATIONS (FT) :—

OFFICIAL AERODROME	1300
LOWEST THRESHOLD	1214
THRESHOLD R/W 26	1214
VARIATION 3°W	Bearings all magnetic
SERIAL No. 50378/2	DATE: 16.6.65
Reason for Re-Issue: NEW HOLDING ON L'SG'	

EFFECTIVE 1.7.65

ILS R/W 26
STUTTGART

SECT. SAFE ALT.
25 N.M. 4500'

SECT. SAFE ALT.
25 N.M. 4000'

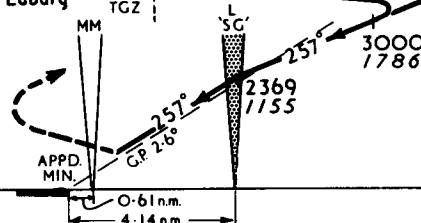
INITIAL APPROACH
MIN. EN ROUTE ALT.
SHUTTLE ON
HOLDING PATTERN

077° FROM Tango
160° FROM Luburg

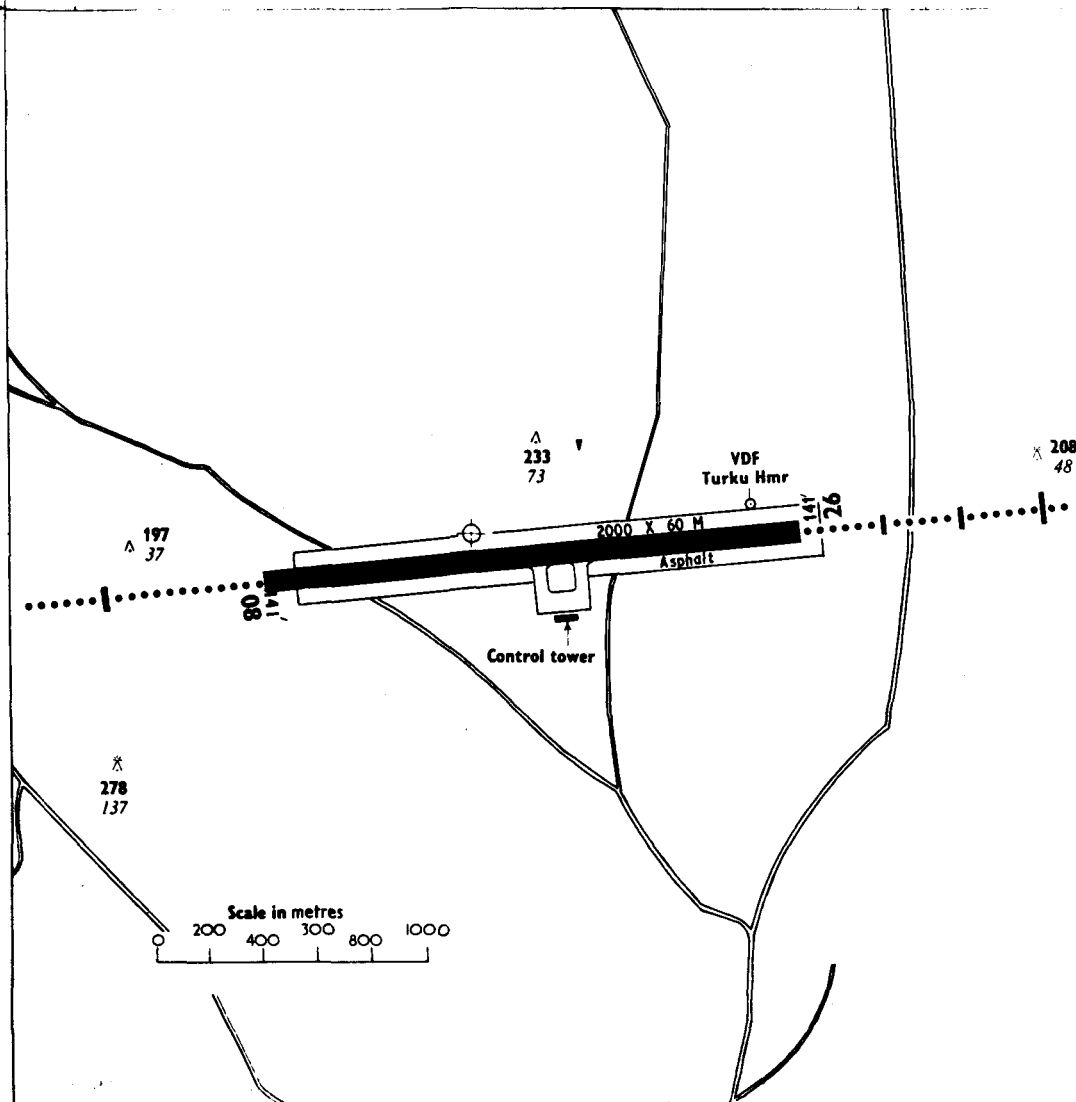
170°R LBU 3500
040°R TGZ 2286

TRANSITION POINT

FROM JAGST



Due to curved surface
G.P. angle varies from
OM to Adme. between
2.4° and 2.8°



WARNING : ON APPROACH TO R/W 26 TREE TOPS ARE 6-10 FT ABOVE APPROACH LIGHTS, BETWEEN MIDDLE AND INNERMOST CROSS BARS.

TURKU AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
26	H.I. UNI-DIRECTIONAL WHITE, L.I. OMNI-DIRECTIONAL RED CENTRE-LINE AND CROSSBARS	
08	L.I. RED CENTRE-LINE & CROSSBAR (FLASHING WHITE LIGHT ON OUTERMOST CENTRE-LINE LIGHT)	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08/ 26	H.I. BI-DIRECTIONAL & L.I. OMNI-DIRECTIONAL GREEN.	H.I. BI-DIRECTIONAL WHITE (LAST 600M YELLOW) L.I. OMNI-DIRECTIONAL WHITE.

OBSTRUCTION, TAXIWAY, APRON LIGHTS.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	161
THRESHOLD	141
LAT 60° 31' N, LONG 22° 16' E	
VARIATION 4° E	Bearings all magnetic
SERIAL No. 50253/1	DATE : 5.5.65

Reason for Re-issue:

WARNING NOTE ADDED

AERODROME CHART TURKU

SECT. SAFE ALT.
25 NM. 1700'

22°00'E

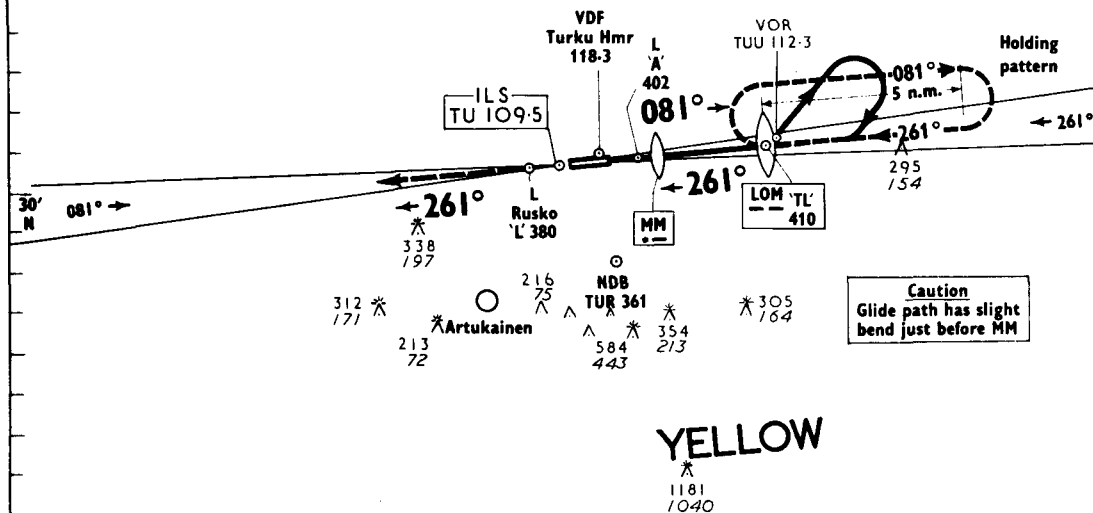
10'

20'

22°30'E

SECT. SAFE ALT.
25 NM. 1700'

BLUE

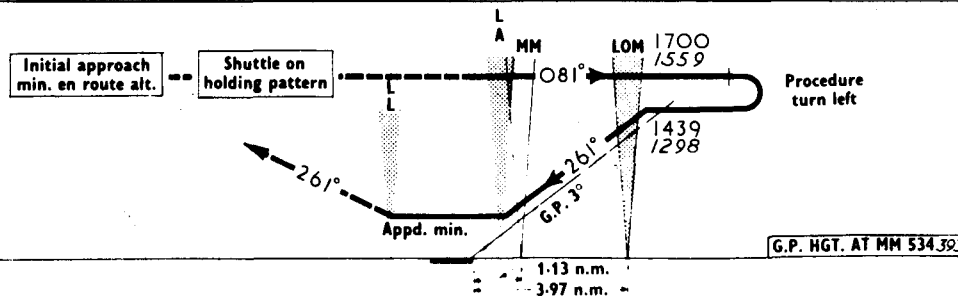


YELLOW

20'

SECT. SAFE ALT.
25 NM. 2200'

SECT. SAFE ALT.
25 NM. 2200'



TURKU
ILS R/W 26

BRITISH EUROPEAN AIRWAYS

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APPROACH

TOWER

TURKU TWR.

118.3 121.1 119.7 121.5 E

TRANS ALT

3000

FT.

G/S (KTS)	70	90	110	130	150
LOM TO T/H	3 - 25	2 - 39	2 - 10	1 - 50	1 - 36
DESCENT ON G/P (FT/MIN)	371	477	583	689	796
G/S	170	190			
LOM TOT/H	1 - 24	1 - 15			
DESCENT ON G/P (FT/MIN)	902	1008			

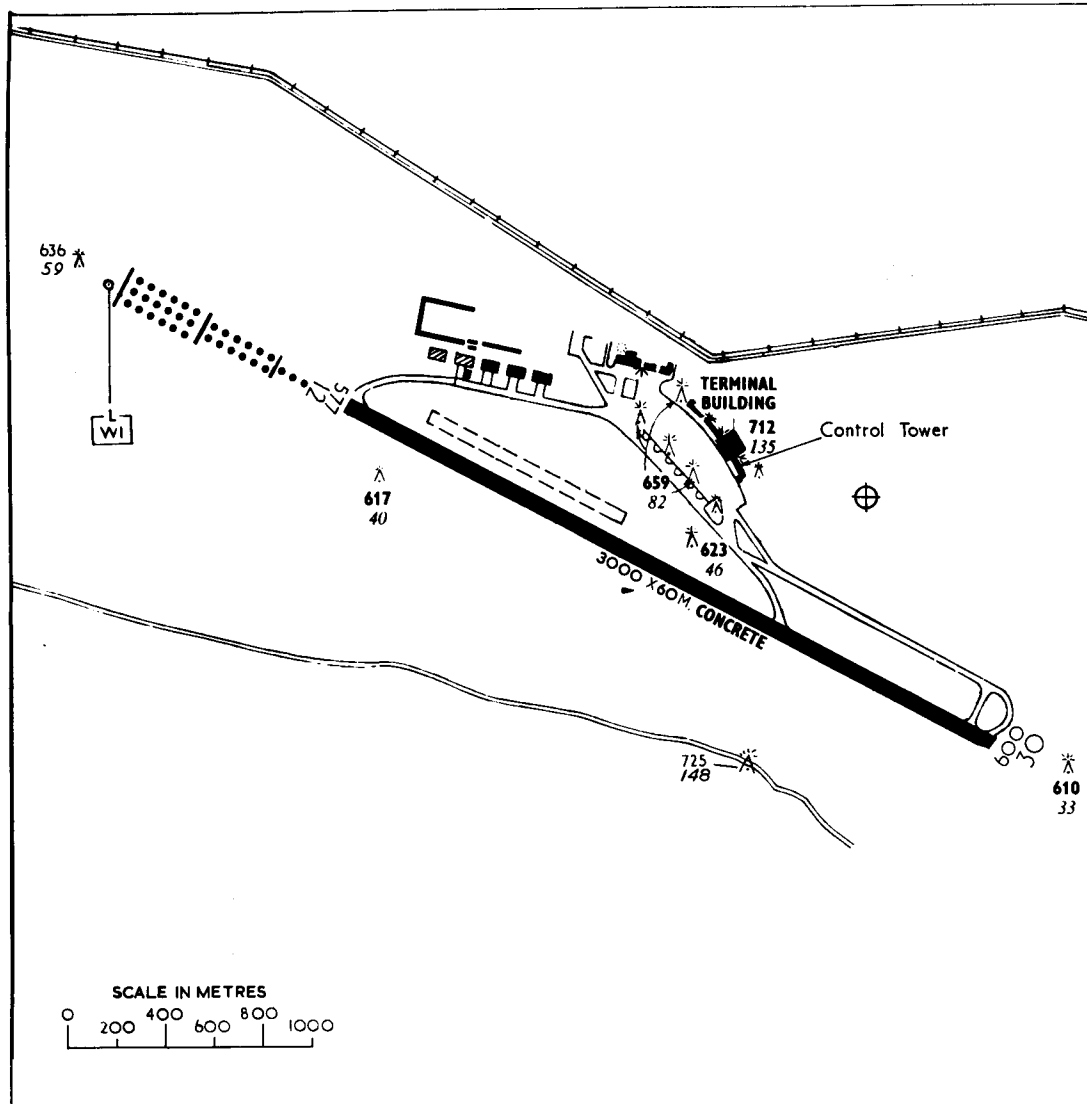
OVERSHOOT

CLIMB AHEAD TO 1700 /559 AND CONTACT TWR.

ELEVATIONS (FT) :-

OFFICIAL AERODROME	160
LOWEST THRESHOLD	141
THRESHOLD R/W 26	141
VARIATION 4°E	Bearings all magnetic
SERIAL No. 50275/2	DATE 5.5.65
Reason for Re-issue:	
IDENT & NEW TWR FREQ.	

ILS R/W 26
TURKU



VIENNA (SCHWECHAT) AERODROME CHART

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
12	HIGH INT. CENTRE-LINE & 3 BARS LOW INT. CENTRE-LINE	
30	NIL	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
12	GREEN	WHITE HIGH INT. & LOW INT. (LAST THIRD YELLOW)
30	GREEN	

OBSTRUCTION, TAXIWAY, APRON FLOODLIGHTS,

ELEVATIONS (FT) : —

OFFICIAL AERODROME	600
LOWEST THRESHOLD (12)	577
LAT 48° 07' N, LONG 16° 34' E	
VARIATION 0	Bearings all magnetic
SERIAL No. 50087/1	DATE: 22.4.65

Reason for Re-issue:

OBSTRUCTIONS

AERODROME CHART
VIENNA (SCHWECHAT)

SECT. SAFE ALT.
25 NM. 4000'

20°

16° 30' E

SECT. SAFE ALT.
25 NM. 4000'

VIENNA (SCHWECHAT)

ILS R/W 12

WARNING
ILS glide path not to be
used below 850 273

NDB/VOR
WAGRAM
WGMWGM
340 113.5

Holding pattern
min. alt. 4000'

YELLOW

STEINHOF
'STE'293

MKR

WO378

VIENNA
ASPERN

WI365

LOM

LMM

BI388

ILS
OEW 110.3

LMM

BO398

LOM

BLUE

48° N
10° Holding pattern
min. alt. 4000'

SECT. SAFE ALT.
25 NM. 5500'

SECT. SAFE ALT.
25 NM. 4000'

GOTZENDORF

OVERSHOOT

TURN LEFT TO NDB/VOR WAGRAM CLIMBING TO
4000 3423

ELEVATIONS (FT) : —

OFFICIAL AERODROME 601

LOWEST THRESHOLD 577

THRESHOLD R/W 12 577

VARIATION 0° Bearings all magnetic

SERIAL No. 50128/2 DATE: 23.4.65

Reason for Re-Issue:

CRIT. HGT. DELETED

ILS R/W 12
VIENNA(SCHWECHAT)

Initial approach
min. en route alt.

shuttle on
holding

STE
4000
3423

LOM

LMM

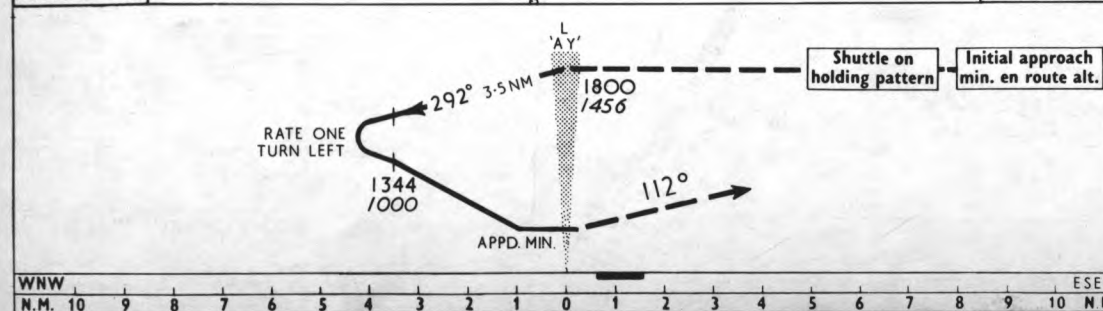
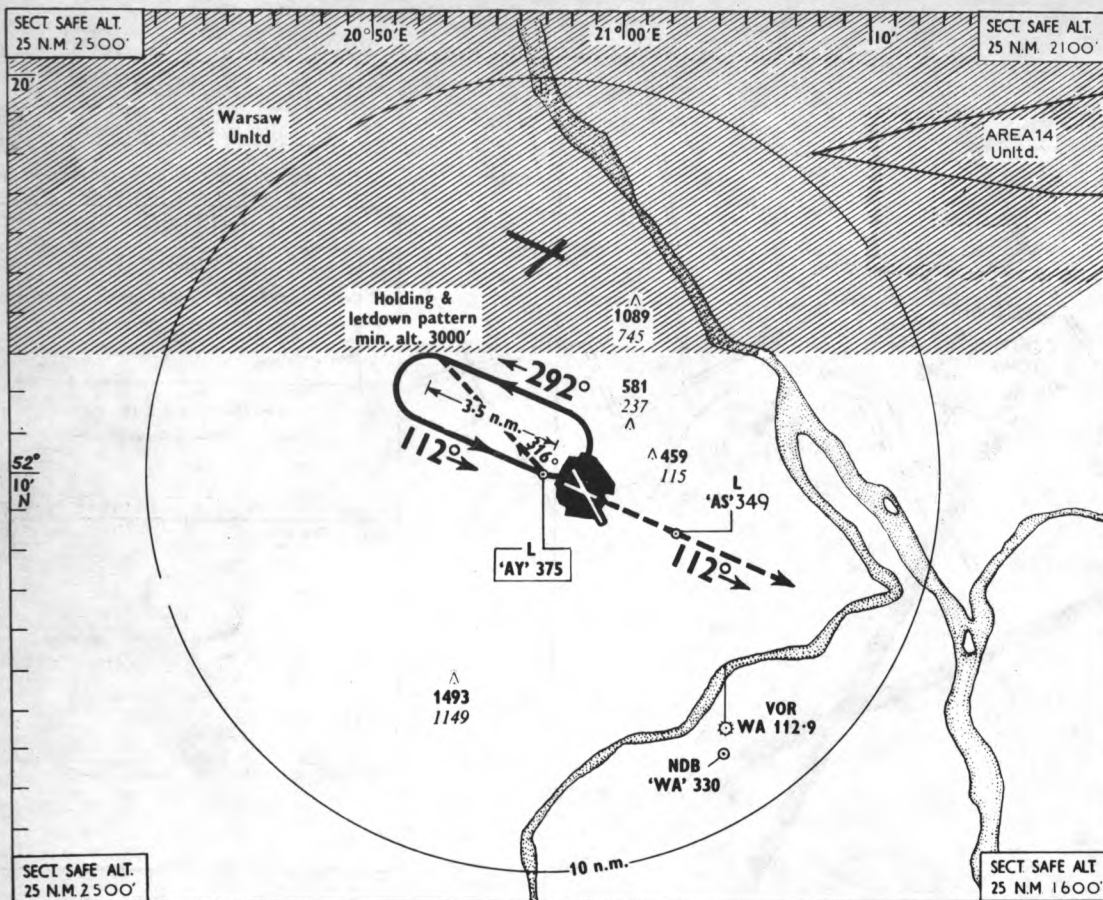
BI

1850
1273

APPD. MIN.

0.6 N.M.
3.7 N.M.

G.P. HGT. AT LMM 850 273



L/BCN R/W 11 WARSAW (OKECIE)

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APPROACH WARSAW TOWER

118-1

TOWER WARSAW TOWER

118-1

TRANS. ALT 1500 A.A.L. FT.

TRANS. LEVEL FL 30

G/S (KTS)	70	90	110	130	150
ABM 'AY' TO TURN	3.0	2.20	1.55	1.37	1.24

G/S	170	190	210	230	250
ABM 'AY' TO TURN	1.14	1.06	1.0	0.55	0.51

OVERSHOOT

CLIMB TO 1844 / 500 ON
112° OR AS INSTRUCTED

ELEVATIONS (FT) :-

OFFICIAL AERODROME 344

LOWEST THRESHOLD

THRESHOLD R/W

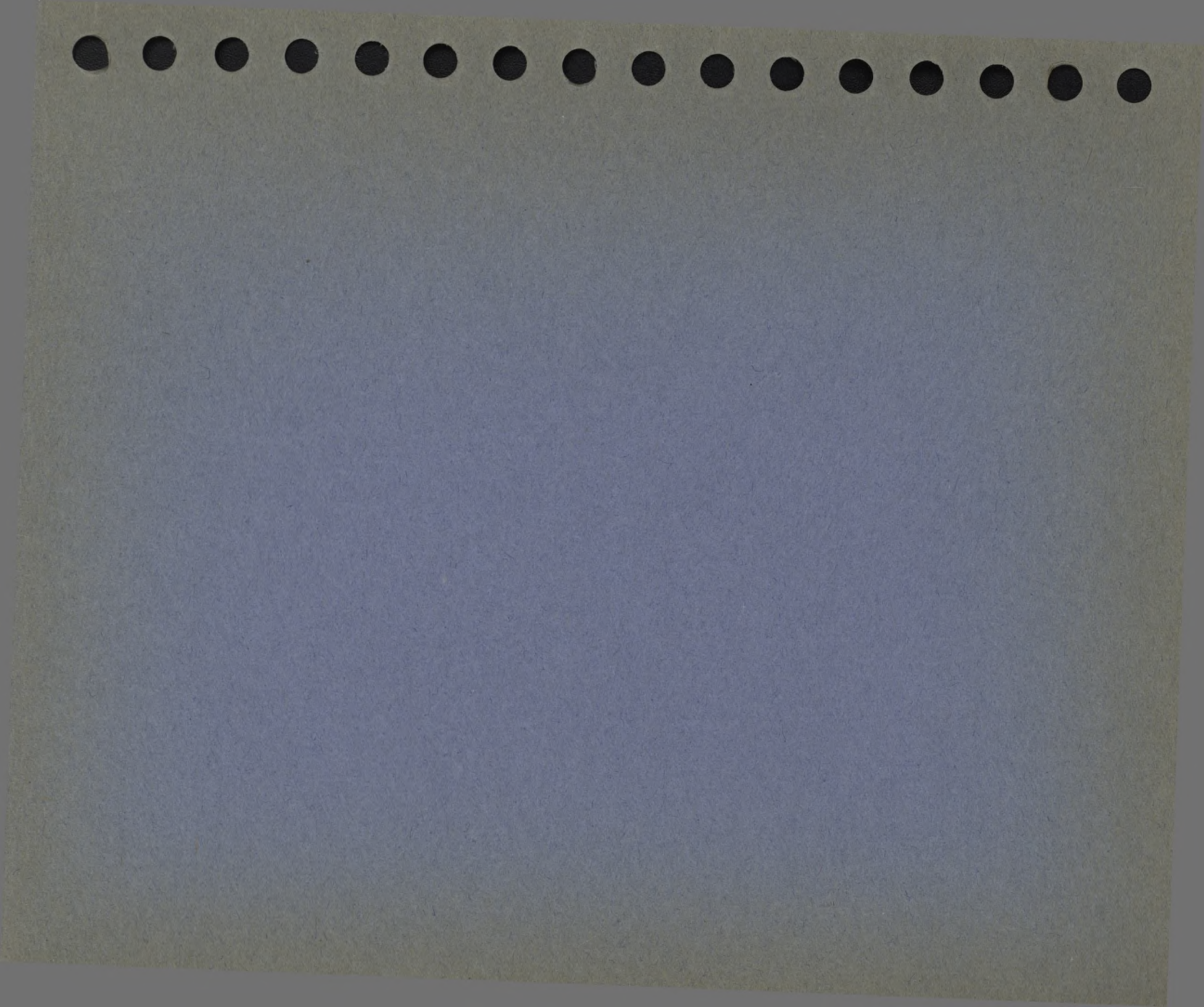
VARIATION 2°E Bearings all magnetic

SERIAL No. 50128/3 DATE: 18.3.65

Reason for Re-Issue:

AS FREQUENCY

L/BCN R/W 11 WARSAW (OKECIE)



BRITISH EUROPEAN AIRWAYS

BEA

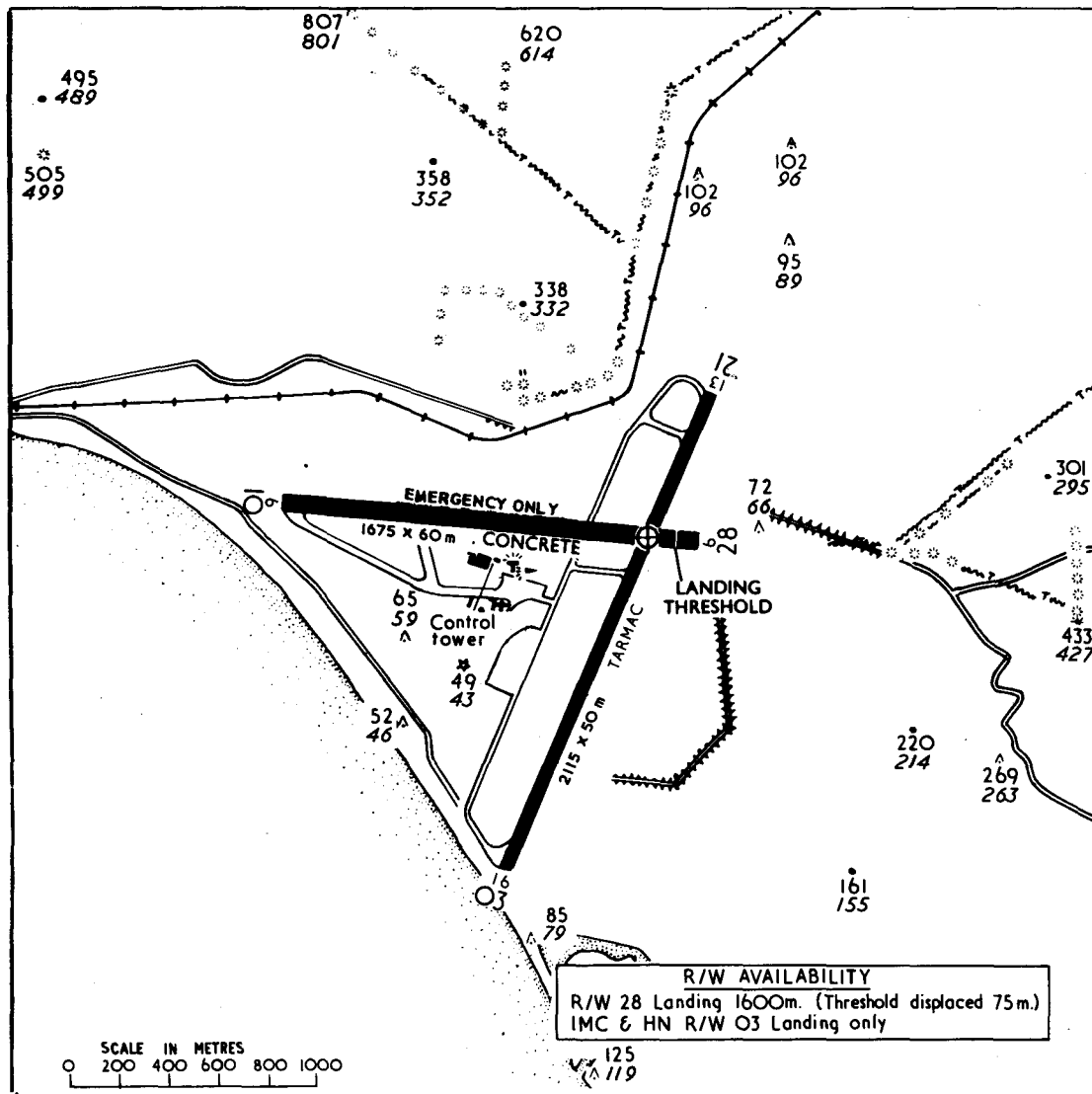
OPERATIONS MANUAL VOL. IV. A.

AERODROME INFORMATION

AREA 3 SOUTHERN EUROPE

The information contained in this part of the Operations Manual is for use in connection with BEA operations in accordance with the standards, limitations and procedures established by BEA. The information should not therefore be used in connection with the operations of any other person or organization.

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AJACCIO AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
03	NIL	*
21	NIL	
10	NIL	
28	NIL	
* FRENCH 'EQUAL BRIGHTNESS' VASI 24°		

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
03	L.I. GREEN WINGBARS. RED BAR 60m. BEFORE THRESHOLD	L.I. WHITE WITH WHITE WINGBARS 300M. UP R/W
21	L.I. GREEN. RED BAR 60m. BEFORE THRES- HOLD	
10	L.I. GREEN	L.I. WHITE
28	L.I. GREEN	L.I. WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, LANDING INDICATOR.

ELEVATIONS (FT) :-

OFFICIAL AERODROME	19
LOWEST THRESHOLD (10)	6
LAT 41° 55' N, LONG 08° 48' E	
VARIATION 3° W Bearings all magnetic	
SERIAL No. 50448/2	DATE: 26 7 65

Reason for Re-Issue:

PRESENTATION

AERODROME CHART AJACCIO

STOPWAY R/W 21-50m. HARD EARTH. **CIRCUITS** :- R/W 10 RIGHT HAND
N.B. EASTERN 600M. OF R/W 10/28 IN GOOD CONDITION, REMAINDER VERY POOR

R/W AVAILABILITY
R/W 28 Landing 1600m. (Threshold displaced 75m.)
IMC & HN R/W 03 Landing only

SCALE IN METRES

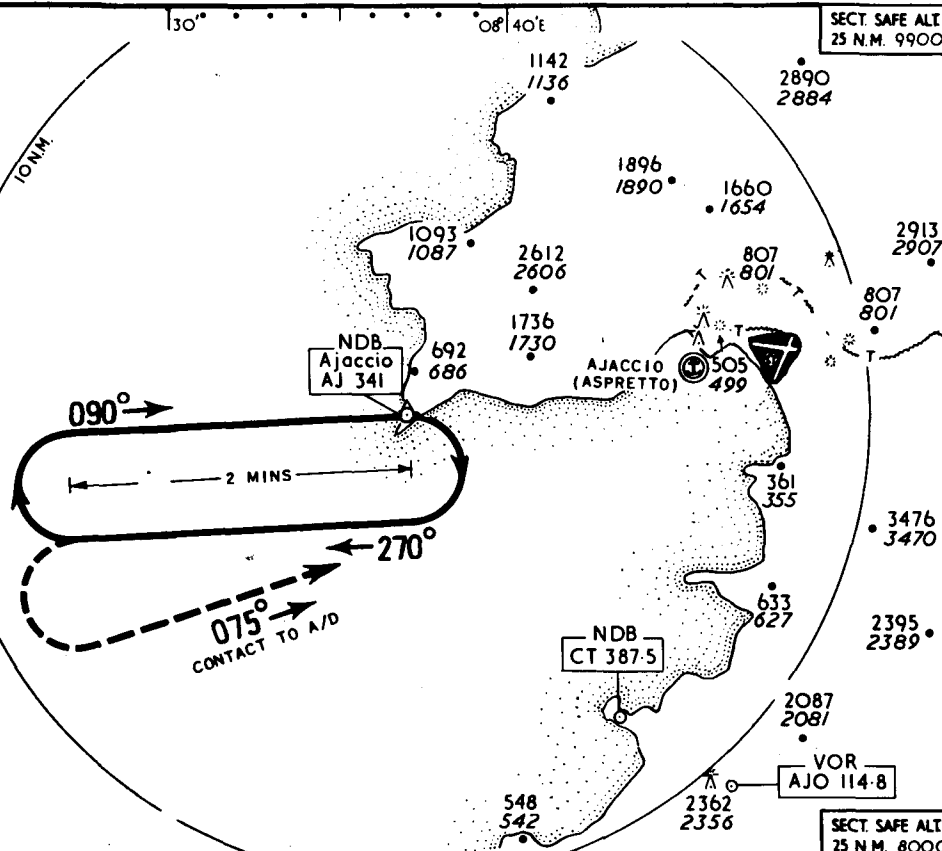
0 200 400 600 800 1000

SECT. SAFE ALT.
25 N.M. 9300

42°
00' N

50'

SECT. SAFE ALT.
25 N.M. 5500



SECT. SAFE ALT.
25 N.M. 9900

AJACCIO NDB AJ (B.C.P.)

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APPROACH	AJACCIO APP.
	120° 0
TOWER	AJACCIO TWR.
	118° 1
TRANS ALT	4900 FT.

G/S (KTS)	70	90	110	130	150
G/S	170	190	210	230	250

OVERSHOOT

IF CONTACT AT APPD. MIN. RATE ONE TURN LEFT
ONTO 075° TO A.D.
IF NOT CONTACT AT APPD. MIN. CLIMB TO SAFE
CRUISING ALT. IN THIS PATTERN.

ELEVATIONS (FT) : —

OFFICIAL AERODROME	19
LOWEST THRESHOLD	6

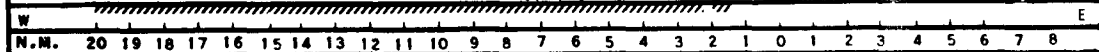
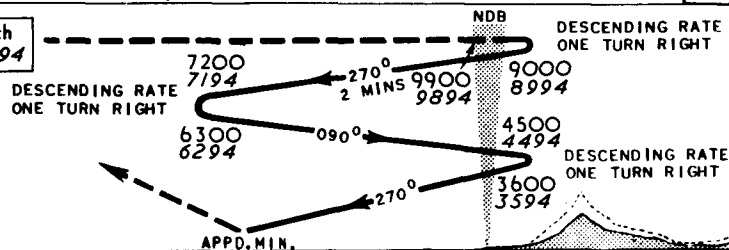
VARIATION 3°W | Bearings all magnetic

SERIAL No. 50448/2 | DATE: 26.7.65

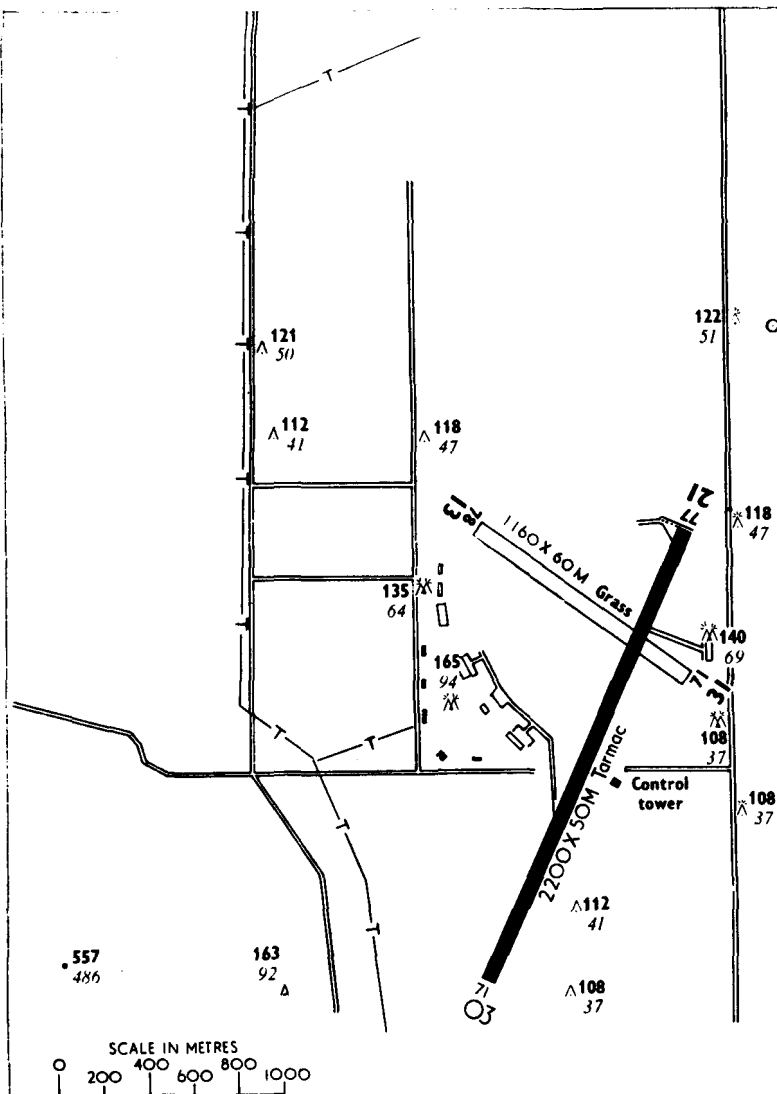
Reason for Re-issue:

OBSTRUCTIONS / NDB CT / FIXER

Initial approach
25 N.M. 9900 9894



NDB AJ (B.C.P.)
AJACCIO



Runway Availability
R/W 03/21 Landing & Take-off
2200m by day
1600m by night

ALGHERO AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI (CAO Type Angle
03/ 21	NIL	

N.B. NO ENTRY UNDER VASI. ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
03/ 21	YES	YES

OBSTRUCTION, TAXIWAY, ADME BEACON
FLOODLIGHTS, LEAD IN LIGHTS.

ELEVATIONS (FT)

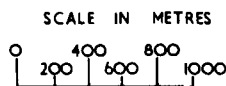
OFFICIAL AERODROME	77
LOWEST THRESHOLD (31)	71
LAT 40° 38' N, LONG 08° 17' E	
VARIATION 3° W	Bearings all magnetic
SERIAL No 50308/1	DATE 26.5.65
Reason for Re-issue:	

RUNWAY LENGTH

AERODROME CHART ALGHERO

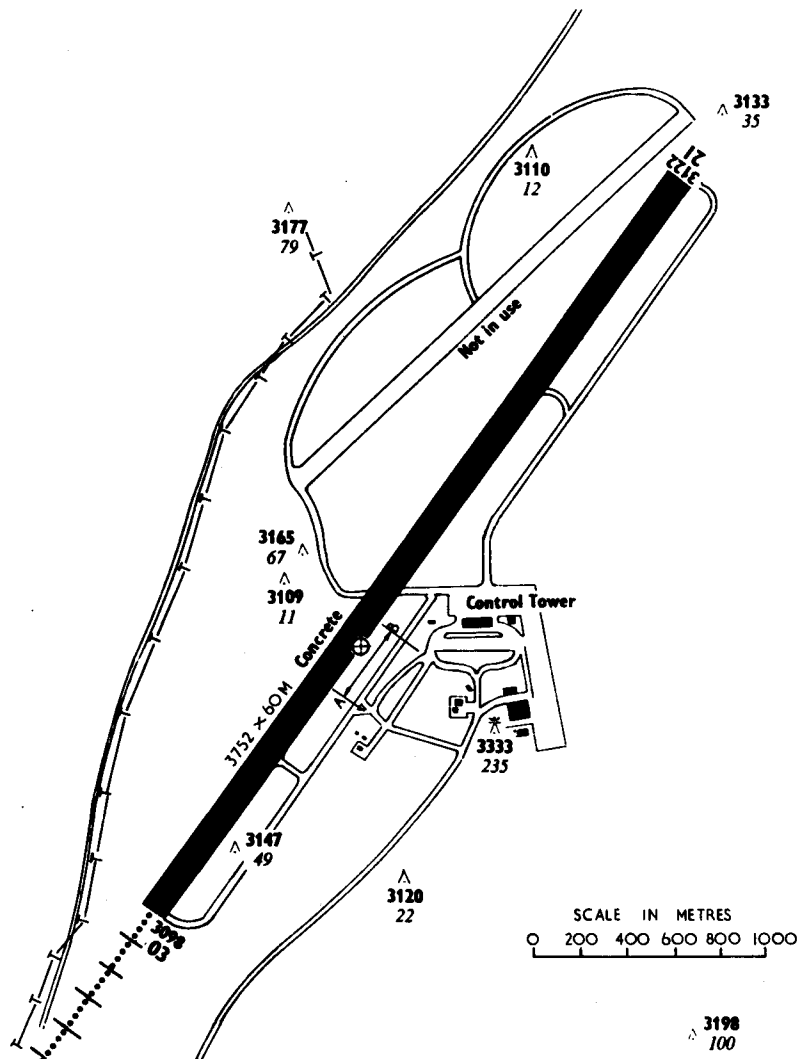
CIRCUITS

R/W 03, 31 right-hand



AERODROME CHART ANDRAVIDA

3162
64
△



NB. Portion of runway & associated taxiway between points A & B is not visible from Control Tower.

ANKARA (ESENBOGA) AERODROME CHART

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH
03	HL CALVERT WHITE 940 WITH 5 BARS

21	NIL
----	-----

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
03	GREEN HL	VAR. INT. WHITE
21		

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, APRON

ELEVATIONS (FT)

OFFICIAL AERODROME 3128

LOWEST THRESHOLD (03) 3098

LAT 40° 07' N, LONG 33° 00' E

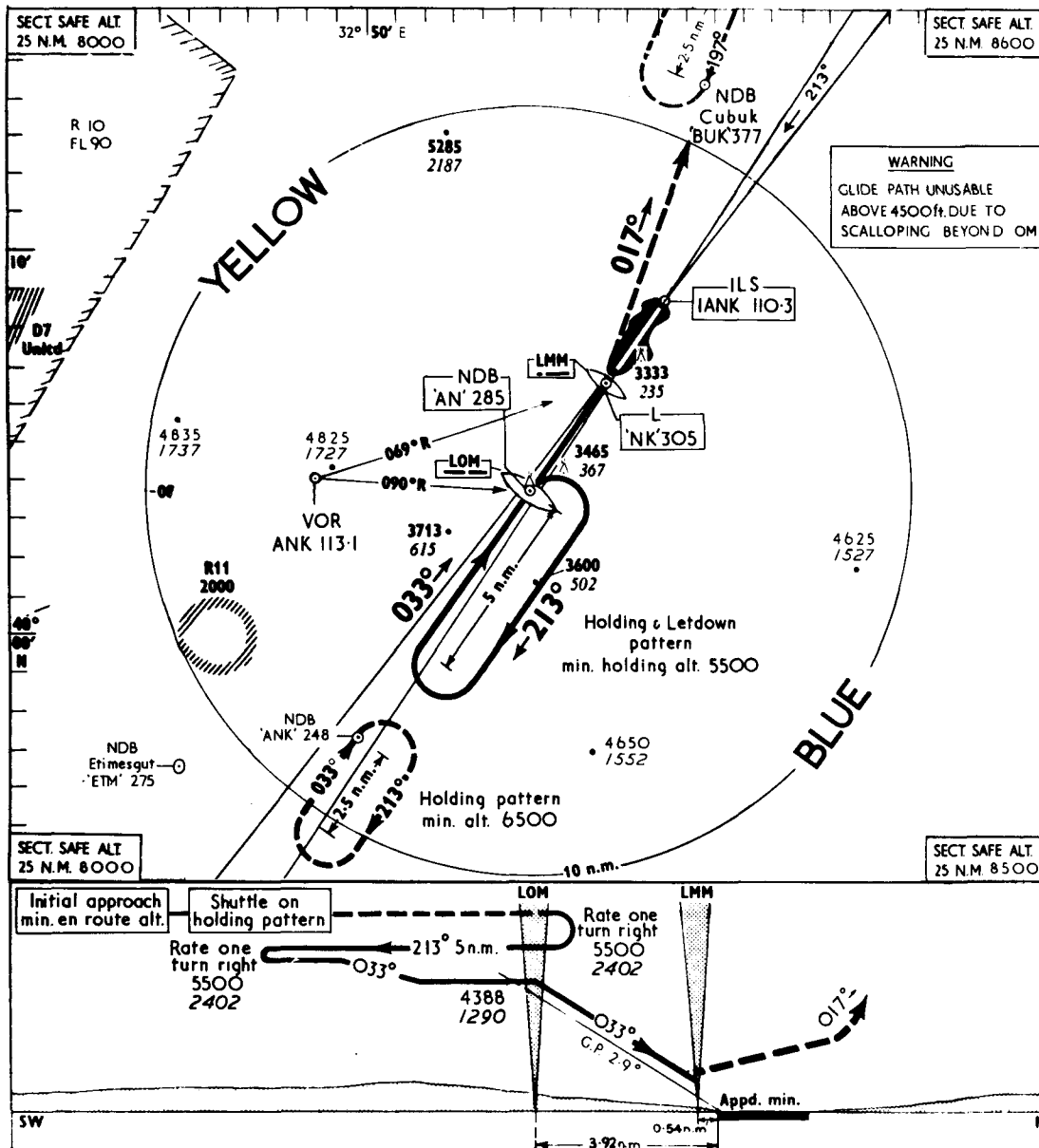
VARIATION 3° E Bearings all magnetic

SERIAL No. 50337/3 DATE: 4. 8. 65.

Reason for Re-issue:

LIGHTING & R/W LENGTH

AERODROME CHART ANKARA (ESENBOGA)



ANKARA (ESENBOGA)

1 L S R/W O 3

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APPROACH	ESENBGA	APP
119.1	121.5	3023.5
TOWER	ESENBGA	TWR
118.1	121.5	3023.5
TRANS ALT	6500	FT

G/S(KTS)	70	90	110	130	150
ABM LOM TO TURN			2 - 44	2 - 19	2 - 0
LOM TO A/D	3 - 22	2 - 37	2 - 08	1 - 49	1 - 34
DESCENT ON G/P (FT/MIN)	359	461	564	666	769
G/S	170	190	210	230	250
ABM LOM TO TURN	1 - 46	1 - 35	1 - 26	1 - 19	1 - 12
LOM TO A/D	1 - 23	1 - 14			
DESCENT ON G/P (FT/MIN)	872	974			

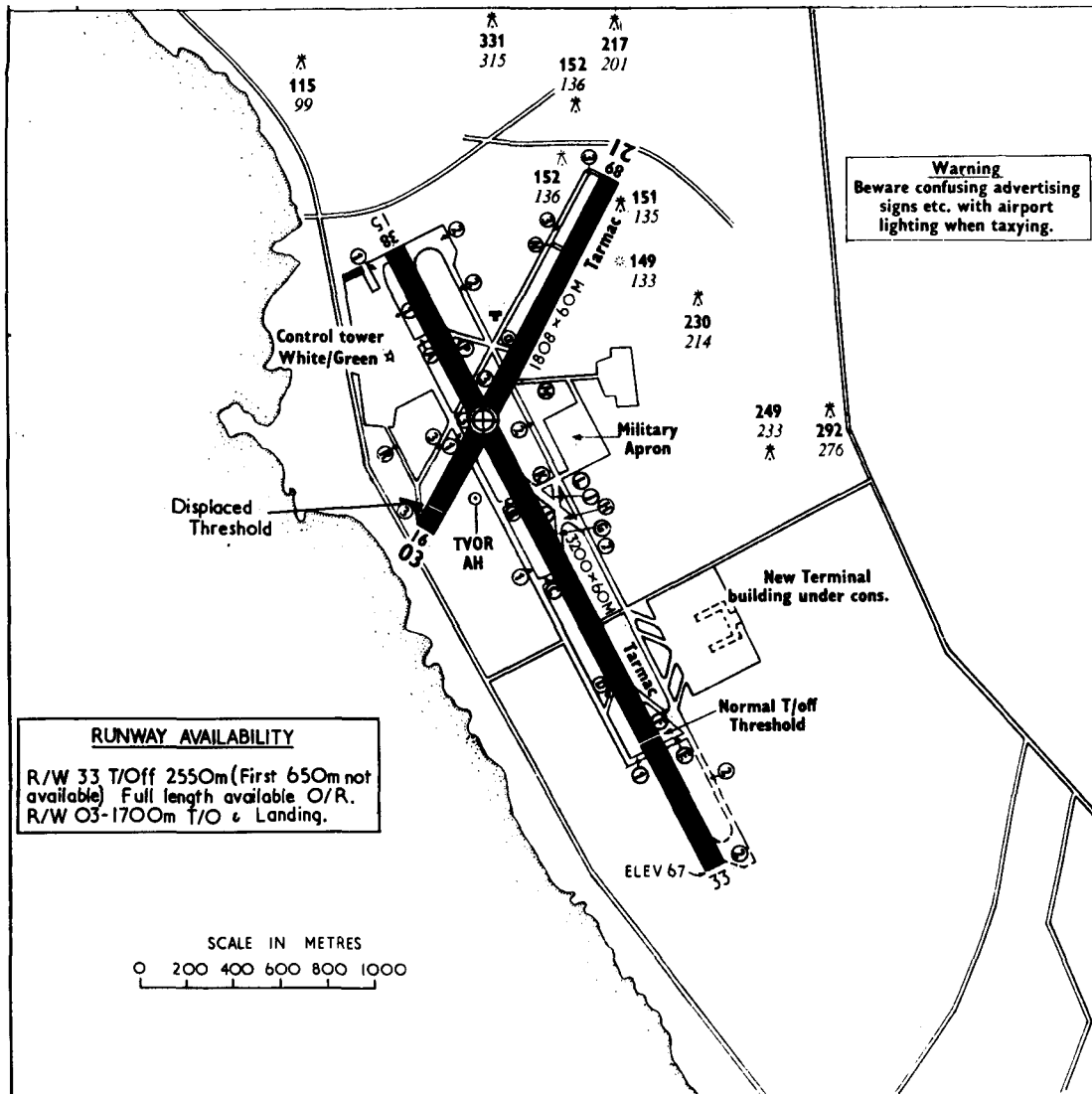
OVERSHOOT CLIMB TO NDB 'BUK' AT 6000
2902 OR ABOVE. (A/C CLIMBING AT LESS THAN
550F/MIN TURN LEFT CLIMBING TO RE-CROSS
NDB 'NK' AT 5000 /902 AND NDB 'BUK' AT
6000 2902)

ELEVATIONS (FT)		
OFFICIAL AERODROME		3128
LOWEST THRESHOLD		3098
THRESHOLD R/W 03		3098
VARIATION 3 °E	Bearings all magnetic	
SERIAL No 50451/4	DATE	4.8.65
Reason for Re-Issue:		

ILS IDENT.

ILSR/WO3

ANKARA (ESENBOGA)



ATHENS CENTRAL AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
15/33	NIL	
03/21	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
15/33	GREEN H.I. BL-DIRECTIONAL (ELEVATED 16")	WHITE H.I.
03/21	GREEN LOW INT. OMNI-DIRECTIONAL	WHITE L.I.

OBSTRUCTION, TAXIWAY, ADME BEACON
LANDING INDICATOR, FLOODLIGHTS.

ELEVATIONS (FT)

OFFICIAL AERODROME	90
LOWEST THRESHOLD (16)	16
LAT 37° 54' N, LONG 23° 44' E	
VARIATION 1° E Bearings all magnetic	
SERIAL No. 40154/2	DATE: 26.4.65

Reason for Re-Issue: R/W 03 AVAILABILITY.

MINOR LIGHTING

AERODROME CHART ATHENS CENTRAL

RUNWAY AVAILABILITY

R/W 33 T/Off 2550m (First 650m not available) Full length available O/R.
R/W 03-1700m T/O & Landing.

SCALE IN METRES
0 200 400 600 800 1000

CAUTION: LAND ON R/W 03 AFTER YELLOW LINE

RUN-UP RESTRICTIONS—RUN-UP ON 03 PROHIB.

A/C RUNNING-UP ON R/W 15 ARE TO FACE 340°

NOTE: CIRCUITS R/W 15 & 21 RIGHT HAND.

ATHENS (ELEFSIS) AERODROME CHART

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LIGHTING*

VASI
ICAO
Type
Angle

R/W	APPROACH
36	H.I. WHITE CENTRE-LINE & BARS
18	H.I. WHITE CENTRE-LINE & BAR

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
36	H.I. GREEN	H.I. WHITE
18	H.I. GREEN	H.I. WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON
LANDING INDICATOR, FLOODLIGHTS.

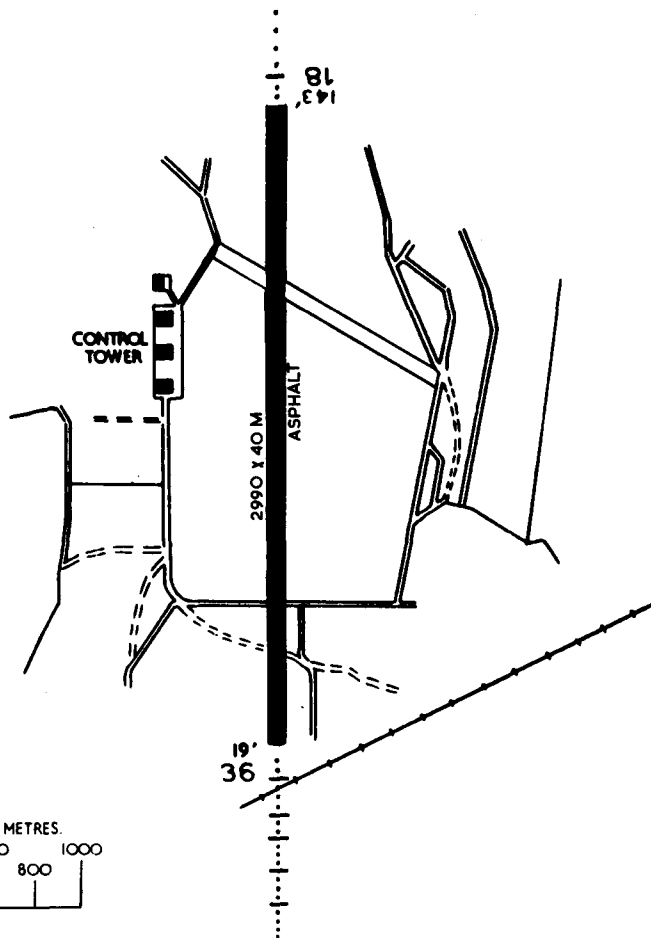
ELEVATIONS (FT) :—

OFFICIAL AERODROME	70
LOWEST THRESHOLD (36)	19
LAT	38° 04' N, LONG 23° 33' E
VARIATION	1° E Bearings all magnetic
SERIAL No	50023/1
DATE	11.1.65

Reason for Re-Issue:

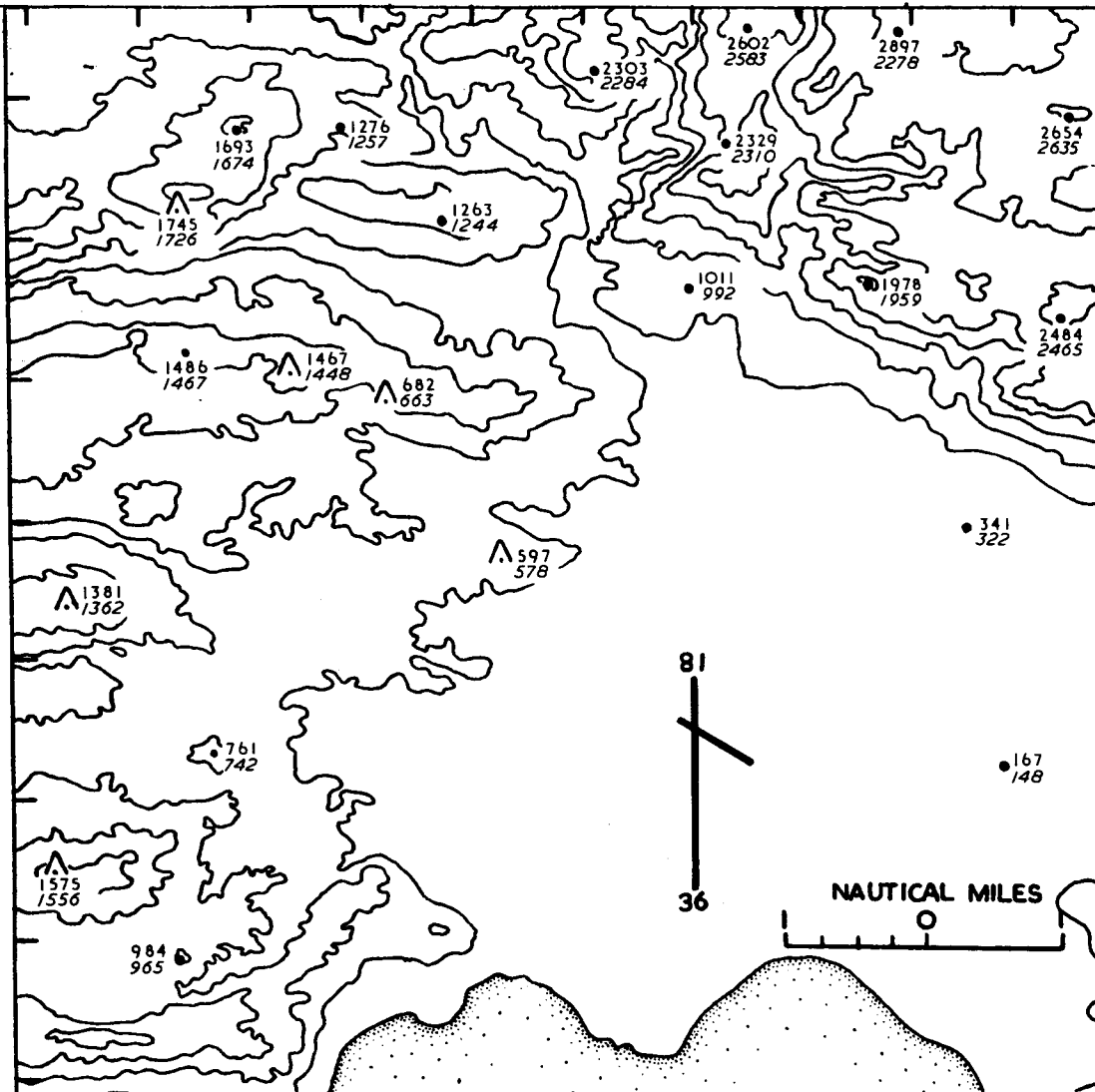
NOTE ADDED.

AERODROME CHART ATHENS (ELEFSIS)



SCALE IN METRES.
0 200 400 600 800 1000

* NB :- AERODROME ONLY APPROVED
FOR DAYLIGHT USE



ATHENS (ELEFSIS) VISUAL APPROACH CHART

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APPROACH

TOWER

ELEFSIS TOWER

117.9 118.3 121.5

TRANS ALT

FT.

ELEVATIONS (FT) : —

OFFICIAL AERODROME

70

LOWEST THRESHOLD

19

VARIATION 1° E Bearings all magnetic

SERIAL No. 50023/1

DATE : 11.1.65

Reason for Re-issue:

PRESENTATION

VISUAL APPROACH CHART ATHENS (ELEFSIS)

BARCELONA AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
02	MED. INT. CENTRE LINE & BAR	
07	HI. WHITE CENTRE - LINE & BAR ALSO RED BAR AT 60M & RED WINGBARS AT 30M	
25	MED. INT. CENTER-LINE & BAR	
17/ 35 20	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
07	HI. GREEN L.I. GREEN & RED	HI. WHITE LAST 610M AMBER L.I. WHITE (LAST 2 LIGHTS GREEN)
25	MED. I. GREEN L.I. GREEN & RED	MED. INT. WHITE (LAST 610M AMBER) L.I. WHITE
02		
20 17/ 35	NIL	NIL

OBSTRUCTION, TAXIWAY, ADME BEACON,
WIND INDICATOR, ANGLE OF APPROACH

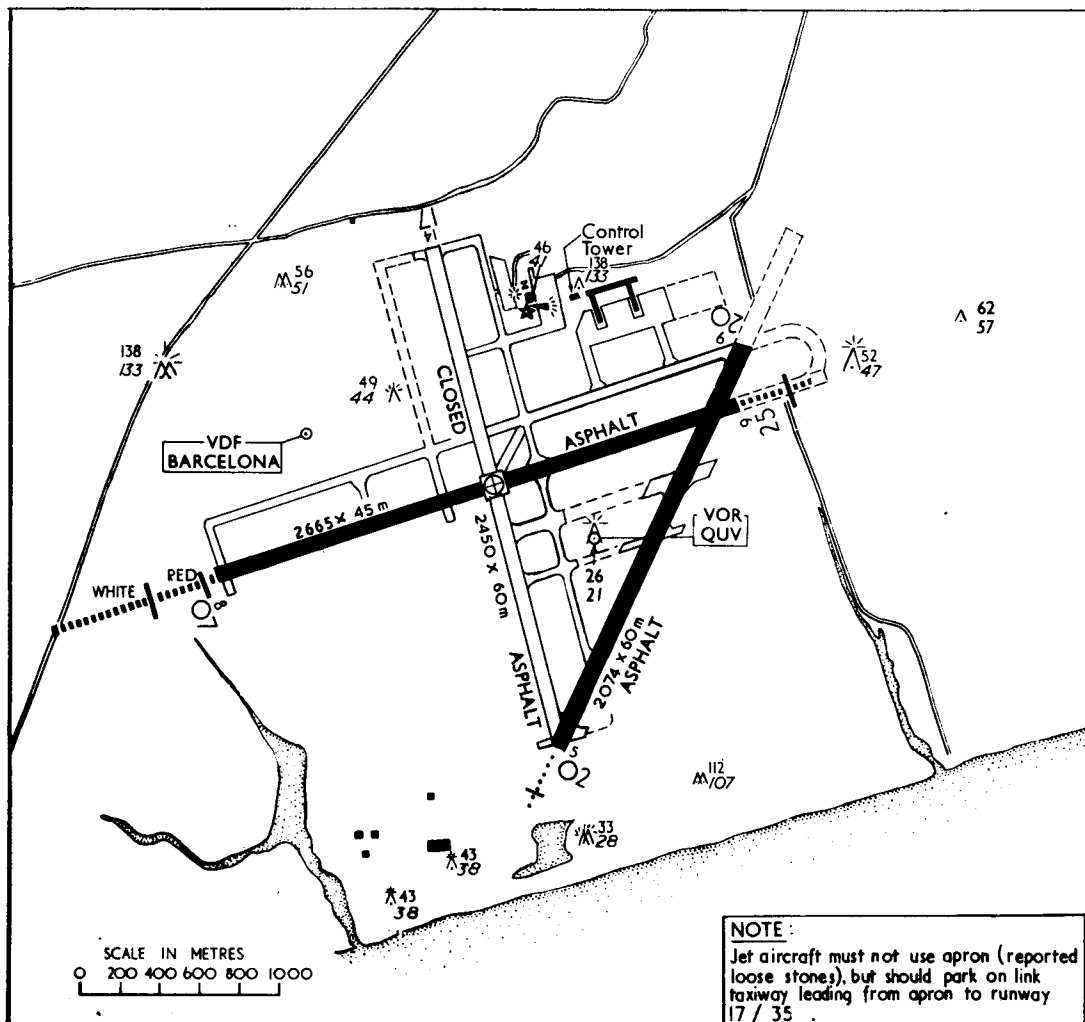
ELEVATIONS (FT)

OFFICIAL AERODROME	13
LOWEST T/H (02 & 35)	5
LAT 41° 18' N, LONG 02° 05' E	
VARIATION 5° W Bearings all magnetic	
SERIAL No. 50256/2	DATE: 31.5.65

Reason for Re-issue:

OBSTRUCTIONS LIGHTING & A/F LAYOUT

AERODROME CHART BARCELONA



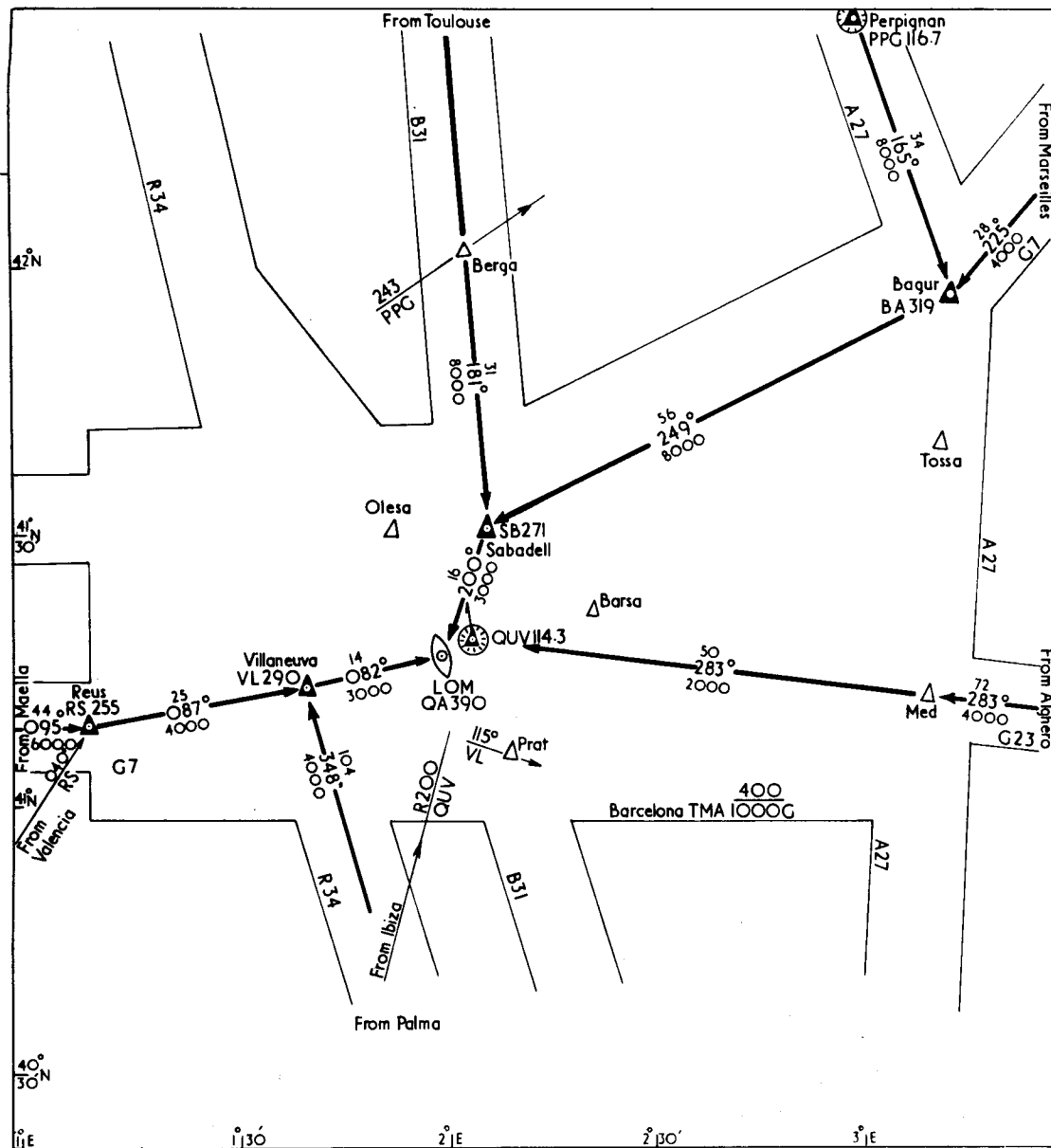
NOTE:

Jet aircraft must not use apron (reported loose stones), but should park on link taxiway leading from apron to runway 17/35.

WARNINGS:

1. R/W and Taxiway and Markers 17 to 28ins. high at distances 2/3 to 1 1/2m from edges.
2. Threshold lights 28ins. high across thresholds.
3. Increased separation of R/W lights at intersection - R/W 07/25, 185m spacing; R/W 17/35, 120m spacing. Control must be advised when R/W circuits and R/W procedures are intended.

AERODROME CIRCUITS:



BARCELONA INBOUND ROUTES & HOLDING PATTERNS

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BARCELONA TMA HOLDING PATTERNS

HOLDING POINT	MIN ALT FEET	TIME MINS	INBOUND HEAD °M	TURN
BARCELONA LOM QA	3000	1	072	RIGHT
VILLANEUVA NDB VL	4000	1	087	RIGHT
REUS NDB RS	6000	1	095	RIGHT
SABADELL NDB SB	6000	1	250	LEFT
SABADELL NDB SB	5000	1	188	LEFT

VARIATION 5°W Bearings all magnetic

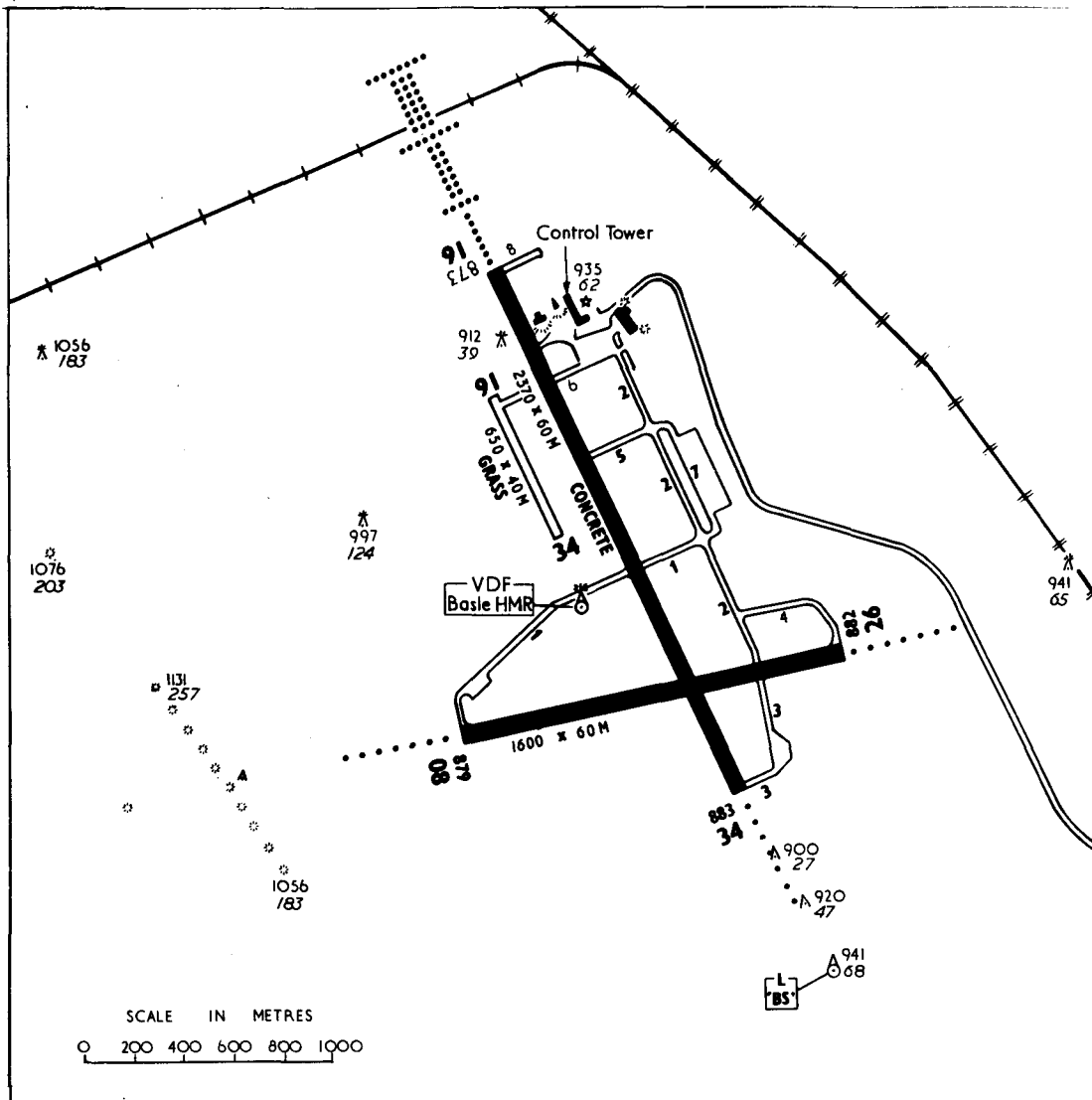
SERIAL No. 50340

DATE 26.5.65

Reason for Re-Issue:

EFFECTIVE 3.6.65

INBOUND ROUTES &
HOLDING PATTERNS
BARCELONA



WARNING

TAXIWAY LIGHTS JOINS. AGL POSITIONED APPROX.
14.2 M FROM TAXIWAY CENTRE LINE. A/C WITH LOW PROP.
CLEARANCE MUST KEEP TO YELLOW CENTRE LINE

CIRCUITS

R/Ws 16/08 RIGHT HAND

BASLE (MULHOUSE) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
16	H.I. CALVERT L.I. CENTRE-LINE	
34	L.I. CENTRE-LINE	
08, 26	L.I. CENTRE-LINE	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
16		H.I. & L.I. WHITE (LAST 600M AMBER)
34	GREEN	
08, 26		L.I. WHITE (LAST 600M AMBER)

OBSTRUCTION, TAXIWAY, IDENT BEACON 'B' (WHITE)
WIND INDICATOR, & ILLUMINATED 'T'

ELEVATIONS (FT) :-

OFFICIAL AERODROME	886
LOWEST THRESHOLD (16)	873
LAT 47° 35' N, LONG 07° 32' E	
VARIATION °W	Bearings all magnetic
SERIAL No. 50296/1	DATE : 7-5-65

Reason for Re-Issue:

OBSTRUCTIONS / LOWEST THRESHOLD

AERODROME CHART BASLE (MULHOUSE)

BEIRUT AERODROME CHART

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
18	WHITE VAR. INT. CENTRE-LINE & CROSSBARS.	
36	NIL	
03	NIL	
21	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
18	GREEN HIGH INT.	WHITE H.I. (LAST 850M. RED)
36	GREEN WINGS	WHITE H.I. RED BEFORE THRESHOLD
03	GREEN MED. INT.	WHITE H.I. (LAST 450M. RED)
21	GREEN WINGS	WHITE H.I. RED BEFORE THRESHOLD

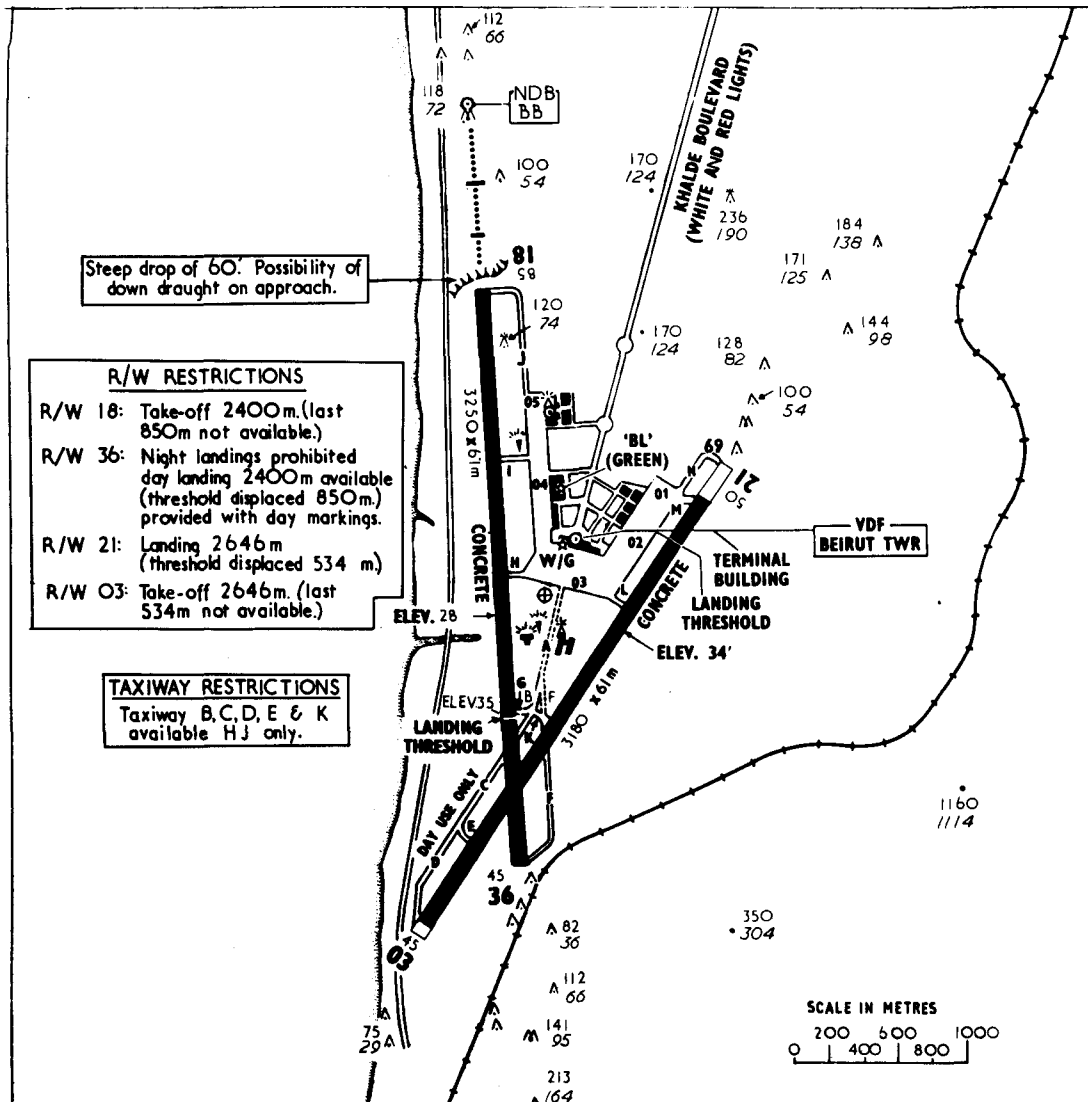
OBSTRUCTION, TAXIWAY, ADME BEACON
WIND AND LANDING INDICATOR, IDENT. BEACON
'BL' (GREEN)

ELEVATIONS (FT) :-

OFFICIAL AERODROME	86
LOWEST THRESHOLD	03.21 46
LAT	33° 49' N, LONG 35° 29' E
VARIATION	2° E Bearings all magnetic
SERIAL No.	40389/3
DATE	14 4 65
Reason for Re-Issue:	

MINOR CORRECTIONS

AERODROME CHART BEIRUT



Steep drop of 60'. Possibility of
down draught on approach.

R/W RESTRICTIONS

- R/W 18: Take-off 2400m. (last
850m not available.)
R/W 36: Night landings prohibited
day landing 2400m available
(threshold displaced 850m.)
provided with day markings.
R/W 21: Landing 2646m
(threshold displaced 534 m.)
R/W 03: Take-off 2646m. (last
534m not available.)

TAXIWAY RESTRICTIONS

Taxiway B, C, D, E & K
available HJ only.

STOPWAYS R/W 36 60m: R/W 21 38m

NOTES: 1. TAXIWAY & R/W LIGHTS ARE 20" ELEVATED PEDESTAL TYPE
THRESHOLD LIGHTS R/W 03 18 & 21 ARE 20" HIGH, R/W 36
ARE 16" HIGH (R/W 03 36 FRANGIBLE)
2. HIGHWAY LIGHTS ON KHALDE BOULEVARD 150FT APART
ALTERNATE LIGHTS CARRY OBSTRUCTION LIGHTS
BOULEVARD 2-2 N.M. LONG.

CIRCUITS: R/W 18 & 21 RIGHT HAND.

SECT. SAFE ALT.
25 N.M. 6000'

20°

35° 30'E

40°

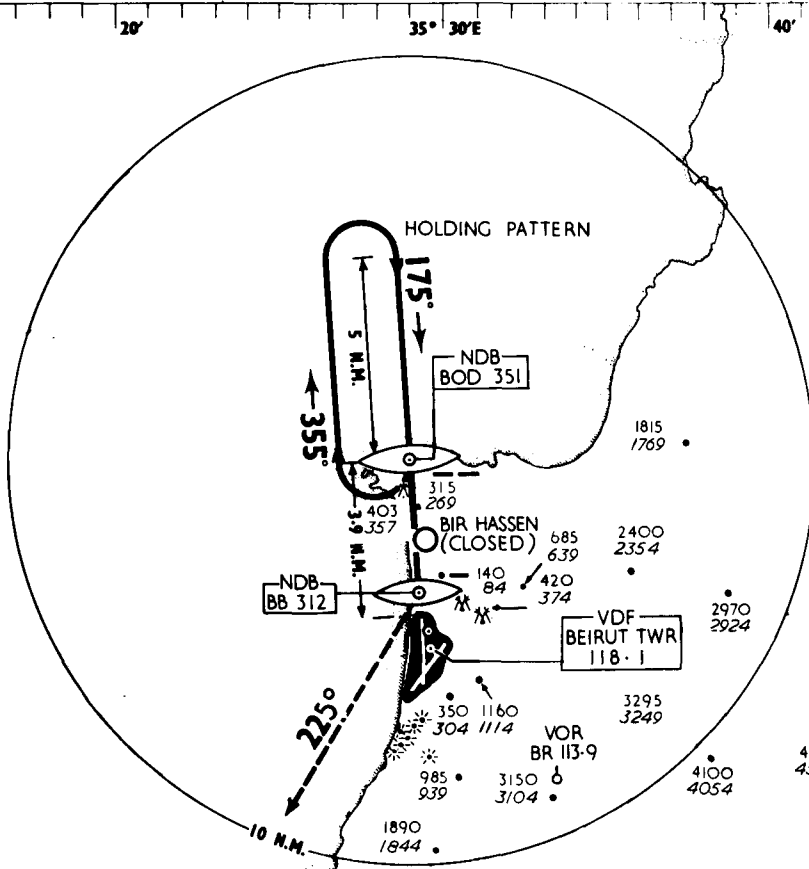
SECT. SAFE ALT.
25 N.M. 9700'

34°
00' N

50'

SECT. SAFE ALT.
25 N.M. 6000'

SECT. SAFE ALT.
25 N.M. 11300'



INITIAL APPROACH
MIN. EN ROUTE ALT.

SHUTTLE ON
HOLDING PATTERN

NDB. BOD

NDB
B B

RATE ONE
TURN RIGHT

RATE ONE
TURN RIGHT

2500
2454

4000 3954

355° 5 N.M.

175°

1300
1214

175°

APPD. MIN

N.M. 12 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 11 12 N.M.

BEIRUT NDB R/W 18

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH BEIRUT APP.

120.3 3023.5

TOWER BEIRUT TWR.

118.1 116.1 121.5

TRANS ALT 10000 FT.

G/S (KTS) 70 90 110 130 150

ABM NDB TO TURN

NDB TO ADME 3.20 2.36 2.08 1.48 1.34

G/S 170 190 210 230 250

ABM NDB TO TURN

NDB TO ADME 1.46 1.35 1.25 1.18 1.12

OVERSHOOT

RIGHT TURN ONTO 225° CLIMBING TO 2000
1954 & CALL APPROACH

ELEVATIONS (FT) :-

OFFICIAL AERODROME 86

LOWEST THRESHOLD 46

THRESHOLD R/W 18 86

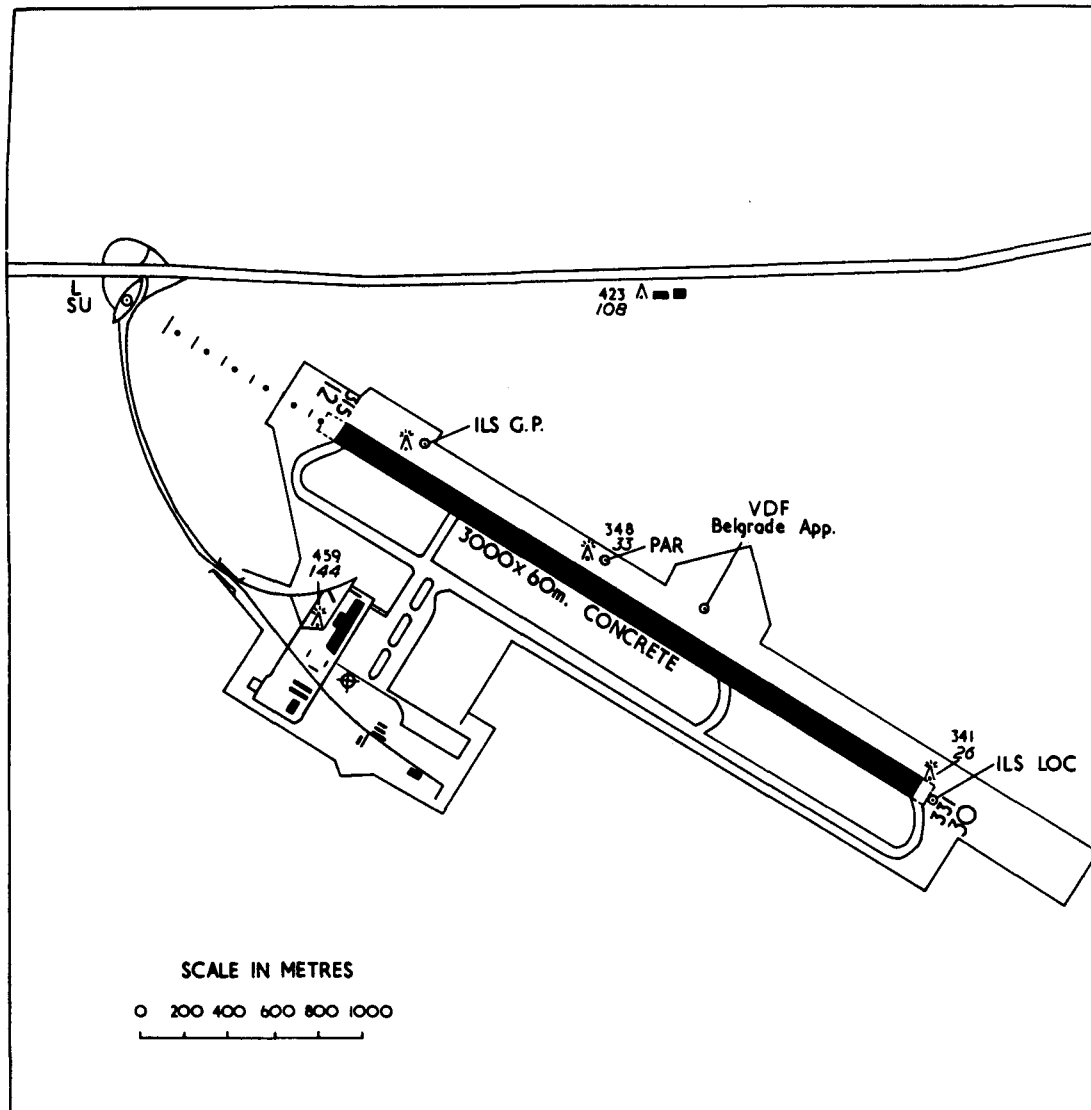
VARIATION 2°E Bearings all magnetic

SERIAL No. 50211/2 DATE 14.4.65

Reason for Re-issue:

OVERSHOOT HEIGHT

NDB R/W 18
BEIRUT



BELGRADE AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
12	H.I.(variable) white uni-directional centre-line and bars. L.I. red omni-directional centre-line and bars.	2.5
30	NIL	2.5

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
12 30	Green, H.I. variable bi-directional	White, H.I. variable bi-directional, at 30m intervals last 750m yellow.

OBSTRUCTION, TAXIWAY, LANDING TEE.

ELEVATIONS (FT) :—

OFFICIAL AERODROME 331

LOWEST THRESHOLD (12) 315

LAT 44° 49' N, LONG 20° 18' E

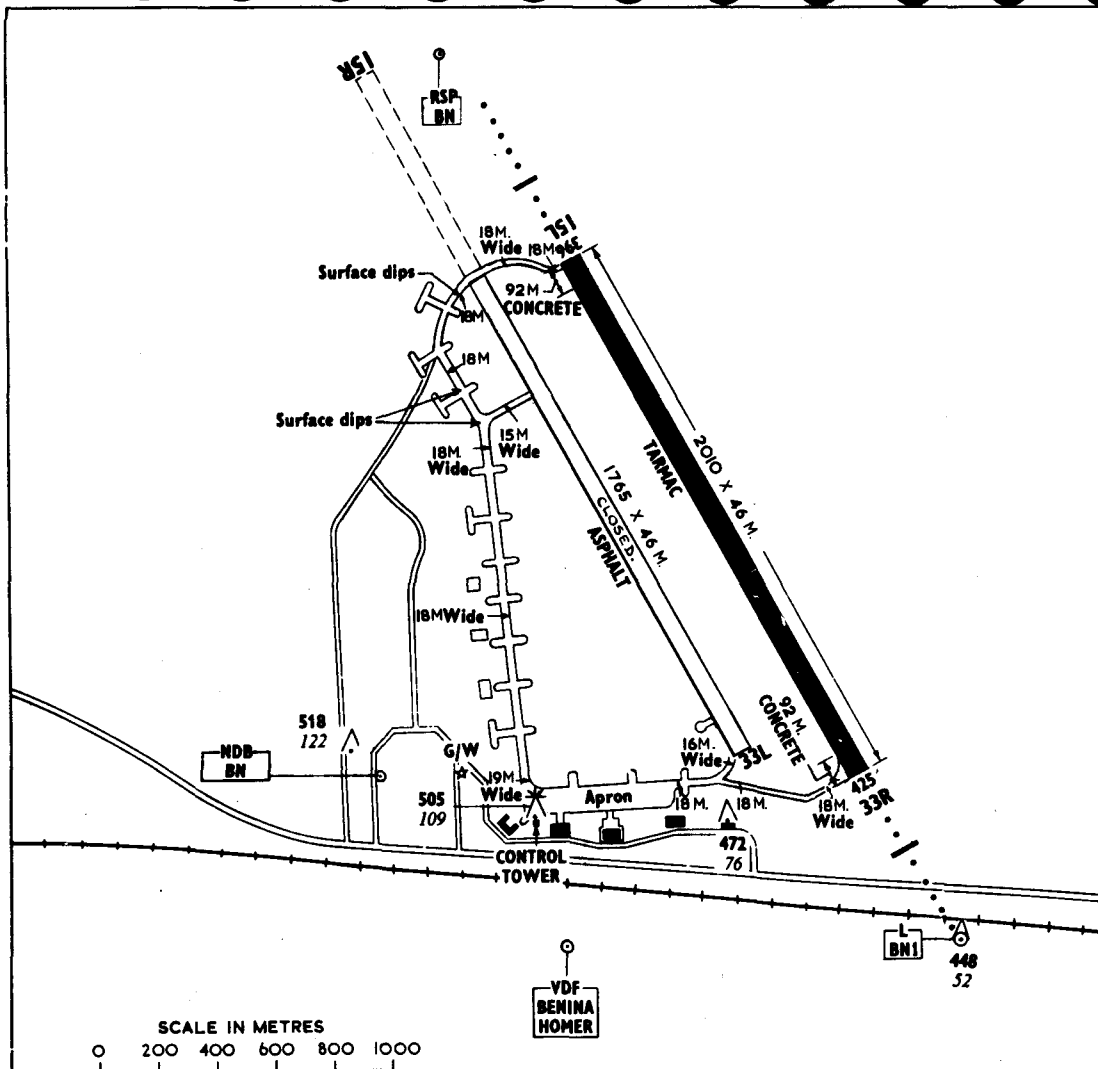
VARIATION 'W' Bearings all magnetic

SERIAL No. 50157 DATE : 2.3.65

Reason for Re-issue:

NEW ISSUE

AERODROME CHART
BELGRADE



BENGHAZI (BENINA) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
15L 33R	RED C/L & BAR	X X

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

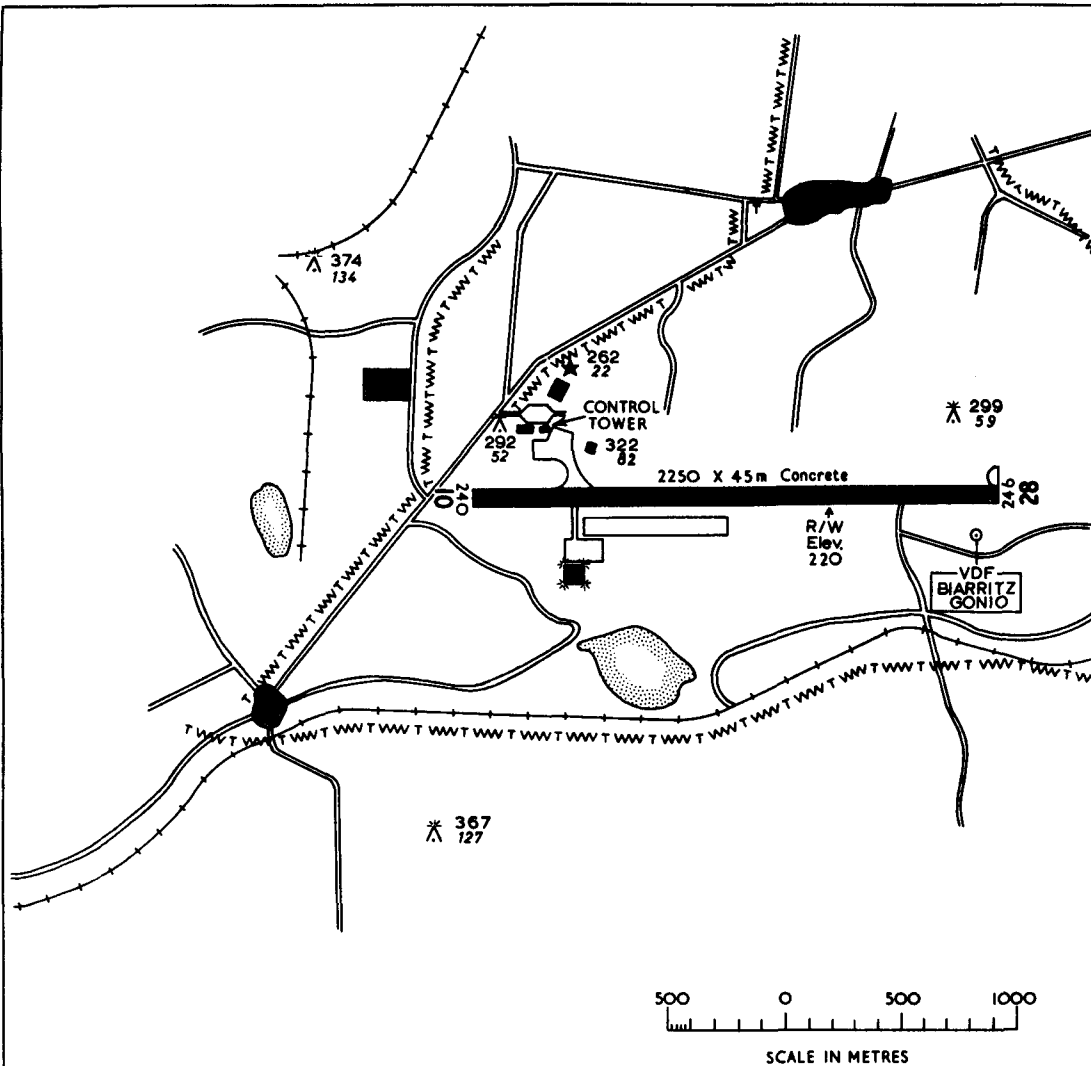
R/W	THRESHOLD	RUNWAY
15L 33R	GREEN WING BARS	L.I. OMNI-DIRECTIONAL

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR. FLOODLIGHTS
OTHER LIGHTING: RUNWAY AND TAXIWAY LIGHTS ARE
2 FEET FROM THE EDGES AND VARY BETWEEN 11
INCHES AND 13½ INCHES IN HEIGHT.

ELEVATIONS (FT)	a.m.s.l. 518 above lowest T/H 122
OFFICIAL AERODROME	425
LOWEST THRESHOLD (15L)	396
LAT 32° 06' N. LONG 20° 16' E	
VARIATION 0° W	Bearings all magnetic
SERIAL No. 48015	DATE: 5 8 64
Reason for Re-issue:	

AERODROME CHART BENGHAZI (BENINA)

STOPWAYS: R/W 33R 305M) NATURAL SURFACE (BEARING STRENGTH VARIES R/W 15R/33L 201 M. PAVED
R/W 15L 204M) SEASONALLY; MAY BE U/S IN RAINY SEASON).
DISTANCE TO GO BOARDS: RIGHT SIDES OF R/W'S 15L & 33R INDICATES 4, 3, 2 (THOUSANDS OF FEET)
WARNINGS: CAUTION ON N.S. TAXIWAY DUE TO DIPS. TAXIING CAUTION DUE UNCONTROLLED VEHICLES.



BIARRITZ AERODROME CHART

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH
10 28	NIL

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
10 28	GREEN	WHITE WITH WING BARS 305 M FROM EACH END

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, LANDING INDICATOR.
PRE. THRESHOLD BAR(RED).

ELEVATIONS (FT)

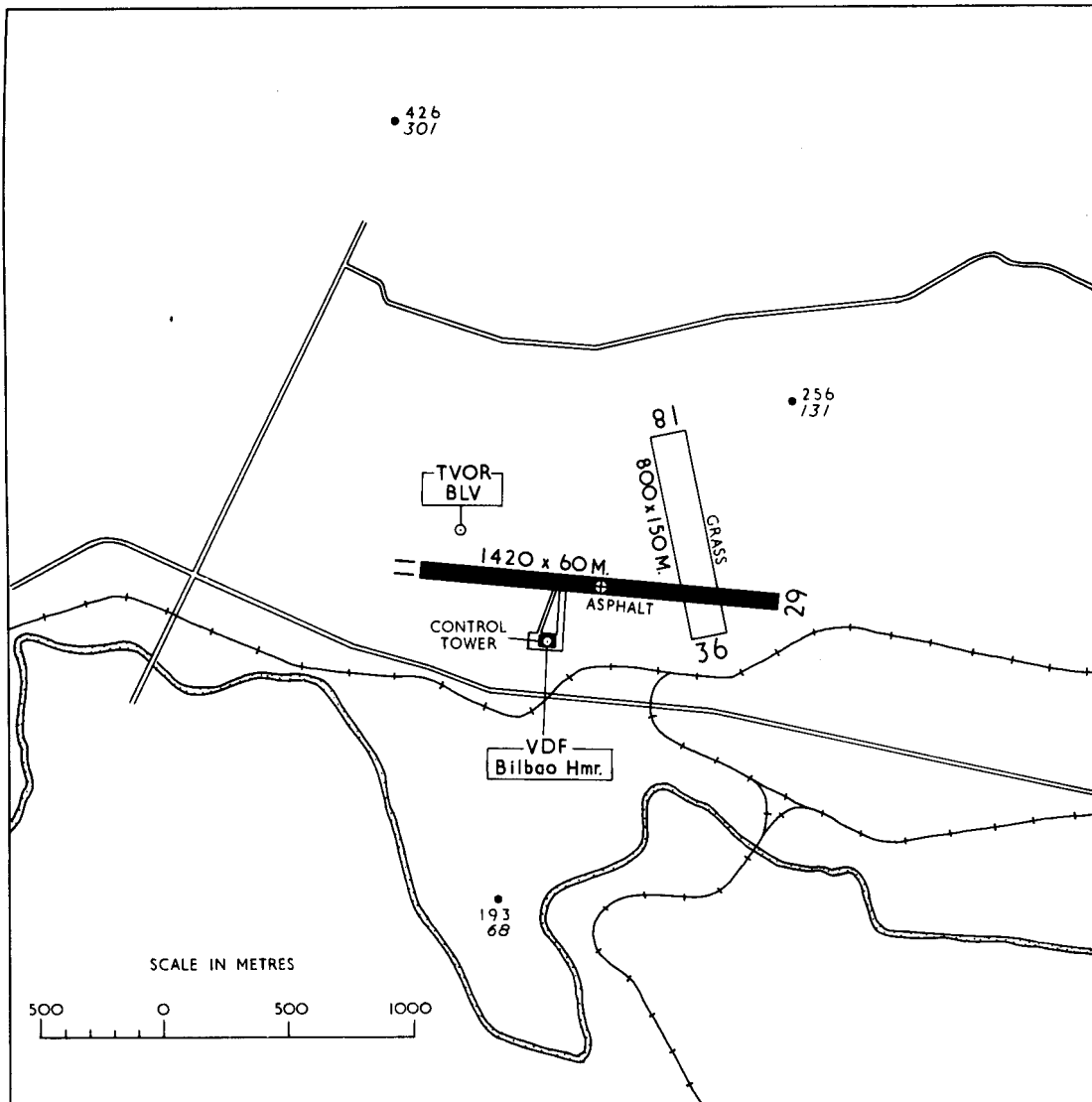
OFFICIAL AERODROME	246
LOWEST THRESHOLD (28)	240
LAT 43° 28' N, LONG 01° 32' W	
VARIATION 7° W Bearings all magnetic	
SERIAL No. 40020/1	DATE: 2-11-64
Reason for Re-issue:	
ELEVATIONS & NOTES.	

AERODROME CHART BIARRITZ

STOPWAYS: R/W 10 100M EARTH. THE GROUND DIPS SHARPLY BEYOND THIS POINT.

R/W 28 100M.

NOTE DIP IN R/W.



BILBAO

AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
11 29	NIL	H.I. WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON
R/W ALIGNMENT BEACON 2400 M. BEFORE
THRESHOLD OF R/W 11.

ELEVATIONS (FT) :—

OFFICIAL AERODROME	125
--------------------	-----

LAT 43° 18' N, LONG 02° 55' W

VARIATION 7° W Bearings all magnetic

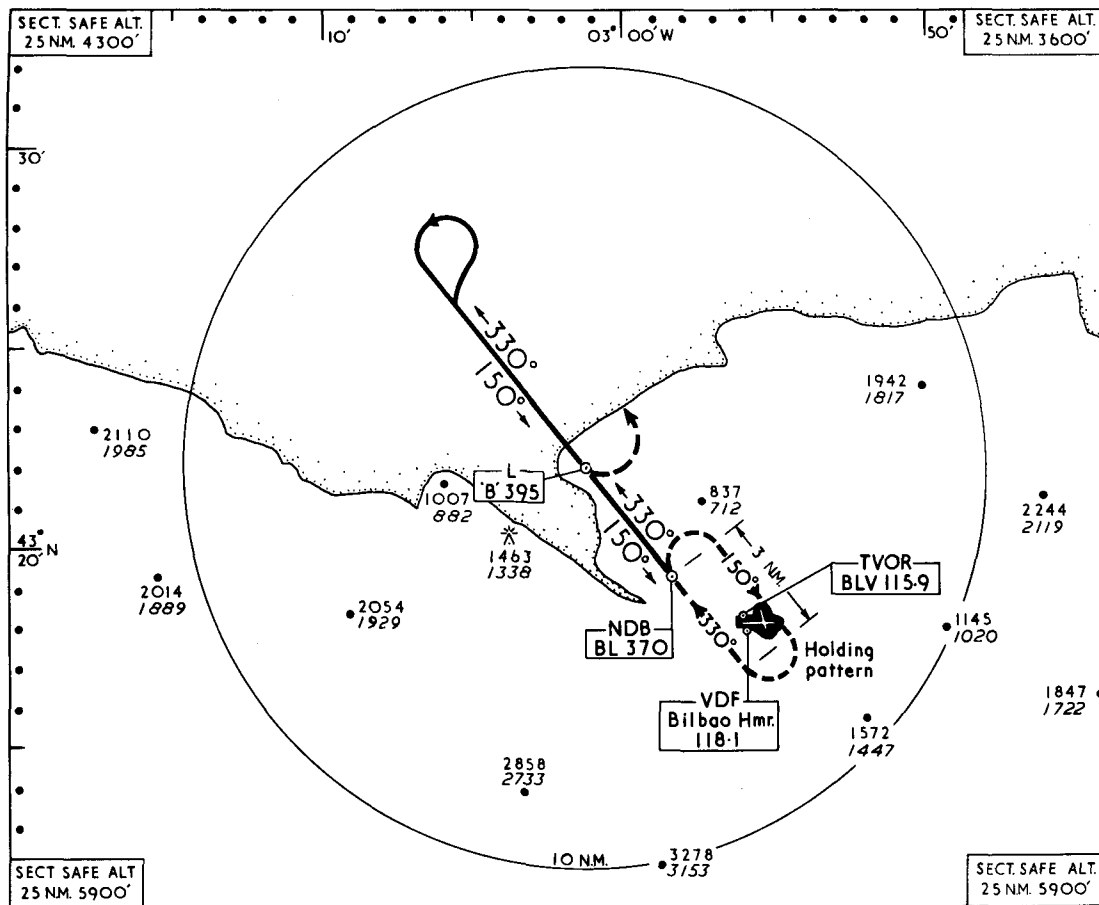
SERIAL No. 50127/1 DATE: 1.4.65

Reason for Re-issue:

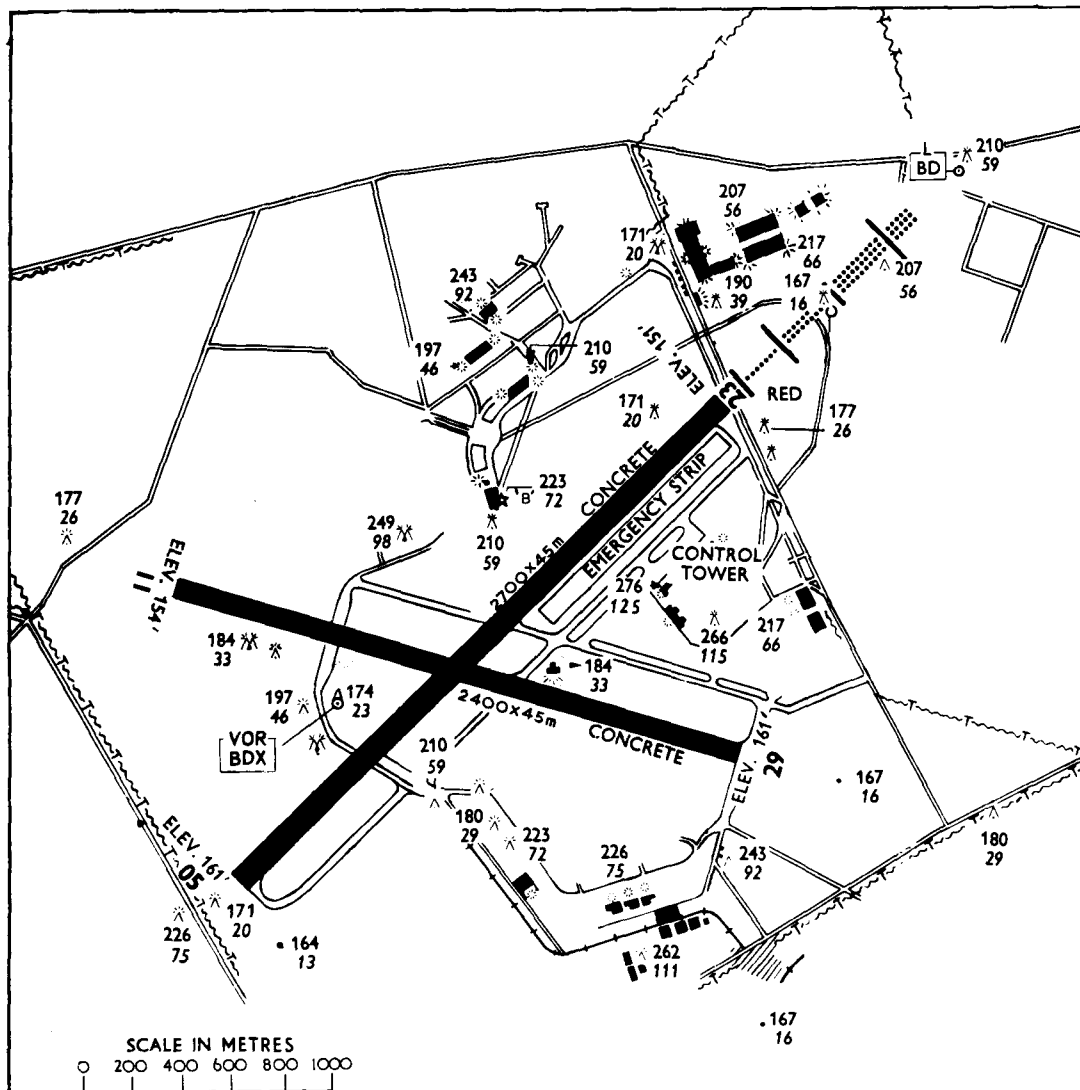
R/W LENGTH

AERODROME CHART

BILBAO



BILBAO



BORDEAUX AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
05	NIL	
23	H.I. WHITE UNI-DIRECTIONAL CENTRE LINE & BARS. RED/SODIUM LEFT SIDE 610M.	
11/29	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
05	L.I. & H.I. GREEN	WHITE & SODIUM WITH L.I. OMNI-DIRECTIONAL BARS AT 305M. UP RUNWAY
23	L.I. & H.I. GREEN	WHITE & SODIUM WITH L.I. OMNI-DIRECTIONAL WING BARS 305M. UP R/W.
11/29	L.I. GREEN	WHITE WITH WING BARS 305M. UP R/W.

OBSTRUCTION, TAXIWAY, ADME BEACON,
LANDING INDICATOR, IDENT BEACON 'B' WHITE
RED BAR 60m BEFORE THRESHOLD OF ALL
R/W's.

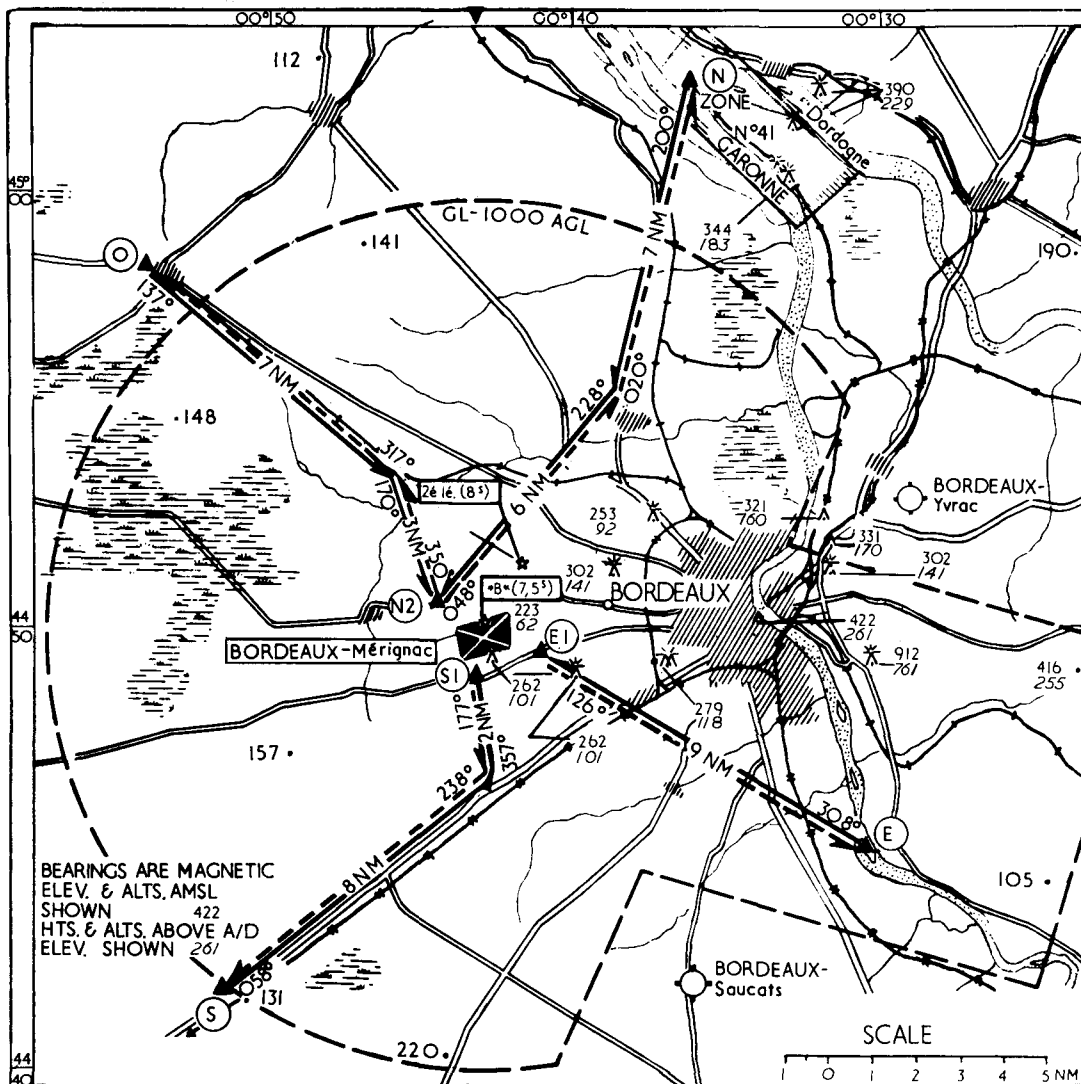
ELEVATIONS (FT.) :-

OFFICIAL AERODROME	161
LOWEST THRESHOLD (23)	151
LAT 44° 50' N, LONG 00° 43' W	
VARIATION 7° W	Bearings all magnetic
SERIAL No. 48102/1	DATE 22.8.64

Reason for Re-Issue:

V.D.F. DELETED

AERODROME CHART BORDEAUX



BORDEAUX

VISUAL PROCEDURES (ROUTES)

BRITISH EUROPEAN AIRWAYS

BEA

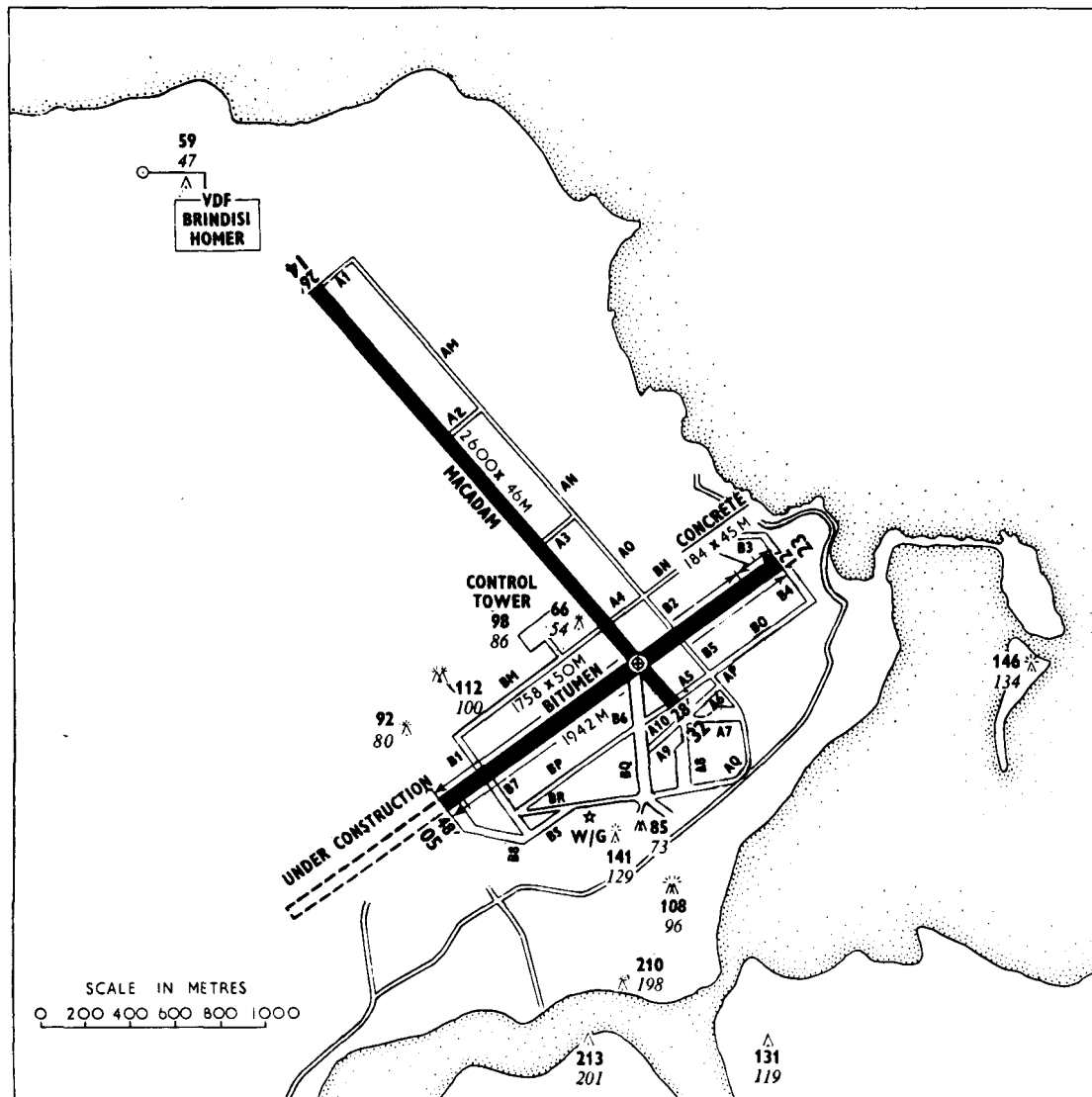
Prepared and Issued by Flight Operations
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OFFICIAL AERODROME 161

VARIATION 7° W
SERIAL No. 40016/2 DATE 22.12.64
Reason for Re-issue:
PRESENTATION

VISUAL PROCEDURES (ROUTES)

BORDEAUX



BRINDISI AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
14/32	NIL	
05		
23		

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
14/32	GREEN	WHITE
05	NIL	WHITE
23		WHITE

BUT R/W AVAIL.
DAY ONLY.

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, LANDING INDICATOR
TAXIWAY LIGHTING : confined to BQ, B6 & BM
(from R/W to apron only).

ELEVATIONS (FT) :—

OFFICIAL AERODROME	48
LOWEST THRESHOLD (23)	12
LAT 40° 39' N, LONG 17° 57' E	
VARIATION 0° W	Bearings all magnetic
SERIAL No 50149/1	DATE 11-3-65
Reason for Re-Issue:	

R/W 05, 23 AVAIL. DAY ONLY.

AERODROME CHART BRINDISI

SECT. SAFE ALT
25 N.M. 2500'

50'

18°00'E

10'

SECT. SAFE ALT.
25 N.M. 1500'

D25
Unitd

BRINDISI VOR R/W14

BRITISH EUROPEAN AIRWAYS

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APPROACH BRINDISI APP.

121.0

TOWER BRINDISI TWR.

118.1 123.5 121.5(E)

TRANS ALT 4000 FT.

G/S(KTS)			110	130	150
VOR TO TURN			4.55	4.09	3.36

G/S	170	190	210	230	250
VOR TO TURN	3.11	2.51	2.34	2.21	2.09

OVERSHOOT

CLIMB ON 137° AND JOIN HOLDING PATTERN
AT 3000 2988

ELEVATIONS (FT) :—

OFFICIAL AERODROME	48
LOWEST THRESHOLD	12
THRESHOLD R/W 14	26
VARIATION 0°	Bearings all magnetic
SERIAL No. 50128/2	DATE: 11.3.65

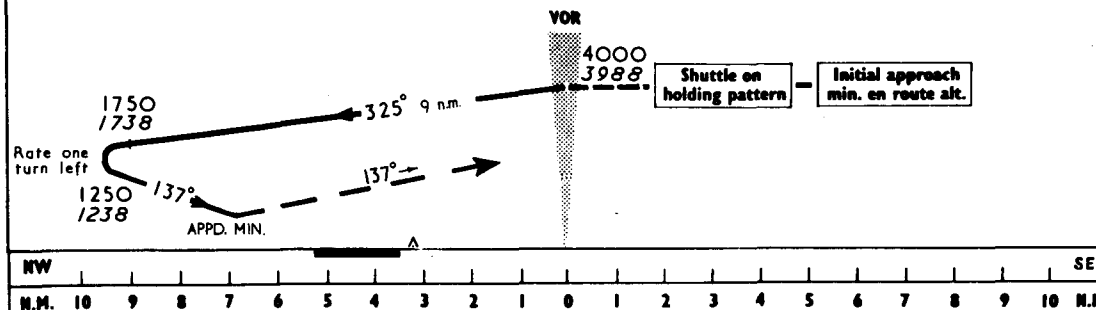
Reason for Re-Issue:

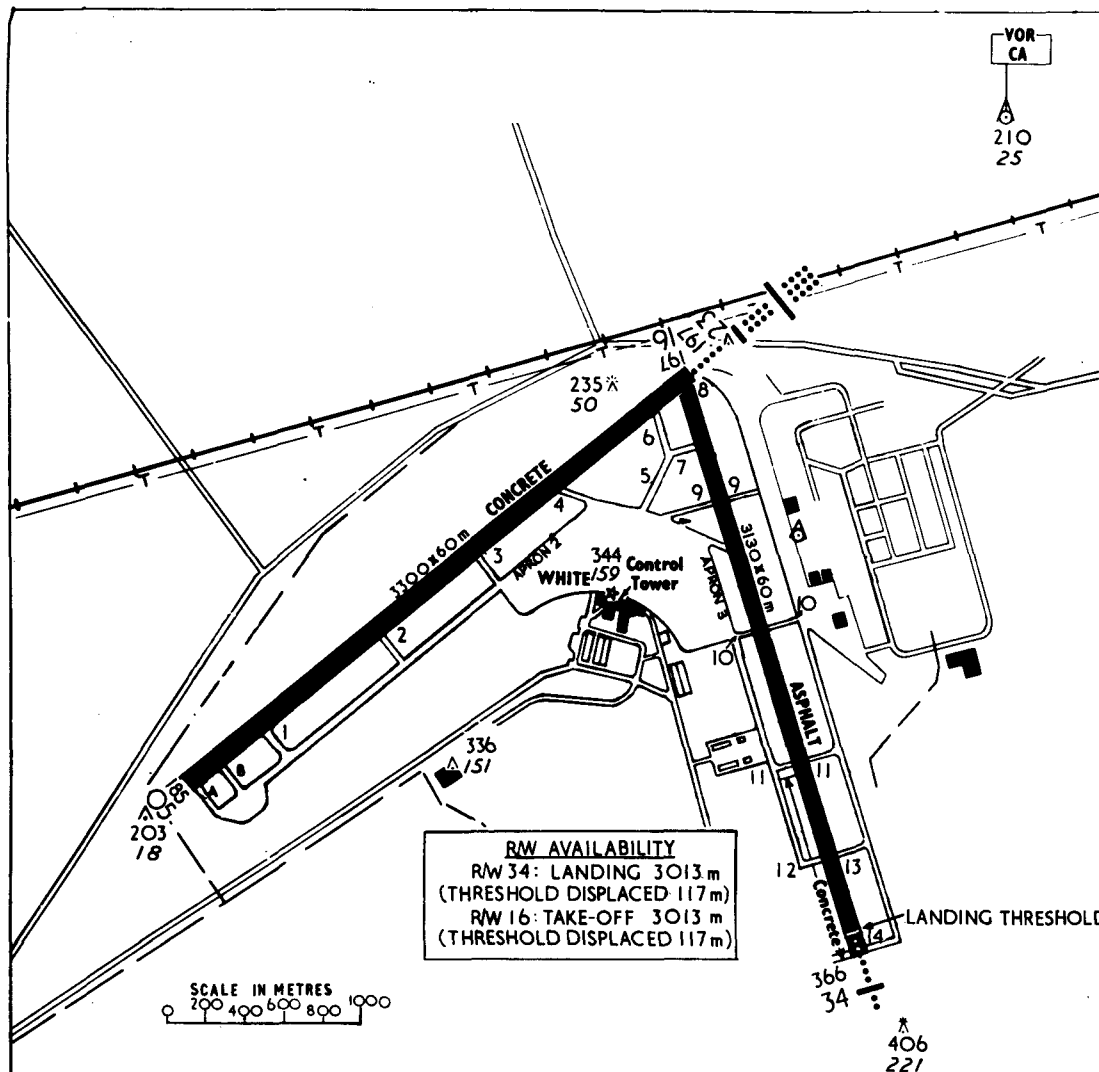
CRIT. HTS REMOVED.

VOR R/W14
BRINDISI

SECT. SAFE ALT
25 N.M. 2500

SECT. SAFE ALT
25 N.M. 1500





CAIRO AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
23	WHITE CENTRE-LINE & BARS	
05	NIL	
34	H.I. WHITE CENTRE-LINE & BAR	
16	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
23 05	GREEN	VAR. INT. WHITE (LAST 480M. AMBER)
34	GREEN WINGS	ELECTRIC L.I. & FLARES
16	GREEN	ELECTRIC L.I. & FLARES

OBSTRUCTION, TAXIWAY, ADME BEACON;
APRON FLOOD.

ELEVATIONS (FT)

OFFICIAL AERODROME	366
LOWEST THRESHOLD (05)	185
LAT 30° 08' N, LONG 31° 24' E	
VARIATION 1° E	Bearings all magnetic
SERIAL No. 50464/3	DATE: 2.8.65

Reason for Re-Issue:

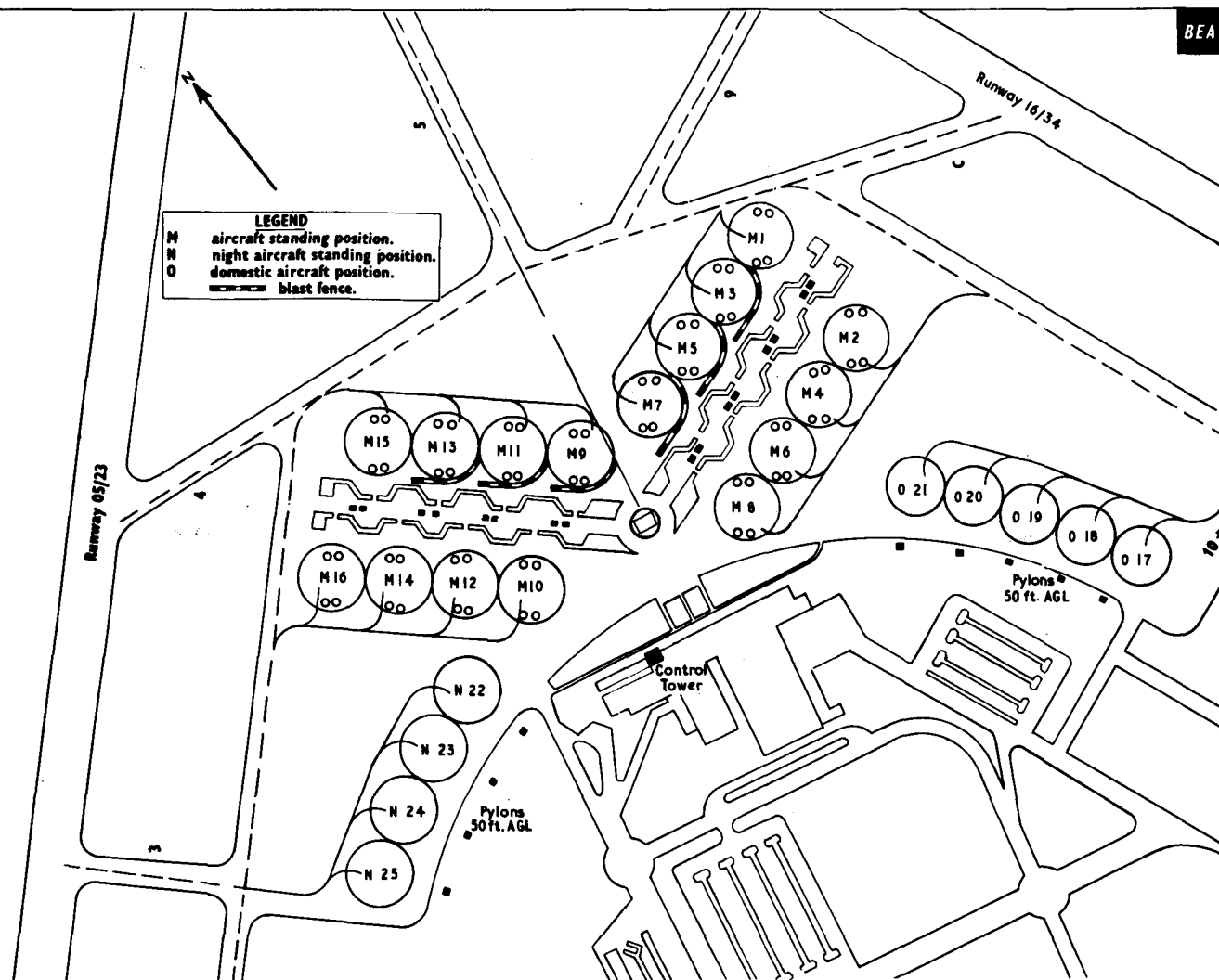
RW 05/23 RE-OPENED

AERODROME CHART CAIRO

CAUTION
LARGE HOLES IN THE PAVED SURFACE
IMMEDIATELY PRIOR TO R/W 23
THRESHOLD CONSTITUTE HAZARD IN
EVENT OF UNDERSHOOTING

WARNING
1. THRESHOLD LIGHTS R/W 16 SITUATED 50M BEFORE THRESHOLD
2. ROUGH AREAS OF TAXIWAY & CAUTION WHILE TAXYING
3. NUMEROUS PATCHES OF LOOSE STONES ON R/Ws
T/WAYS AND APRON.

A/D CIRCUITS.
R/W 23 - RIGHT HAND
4. ENGINE RUN-UP NOT
PERMITTED ON R/Ws

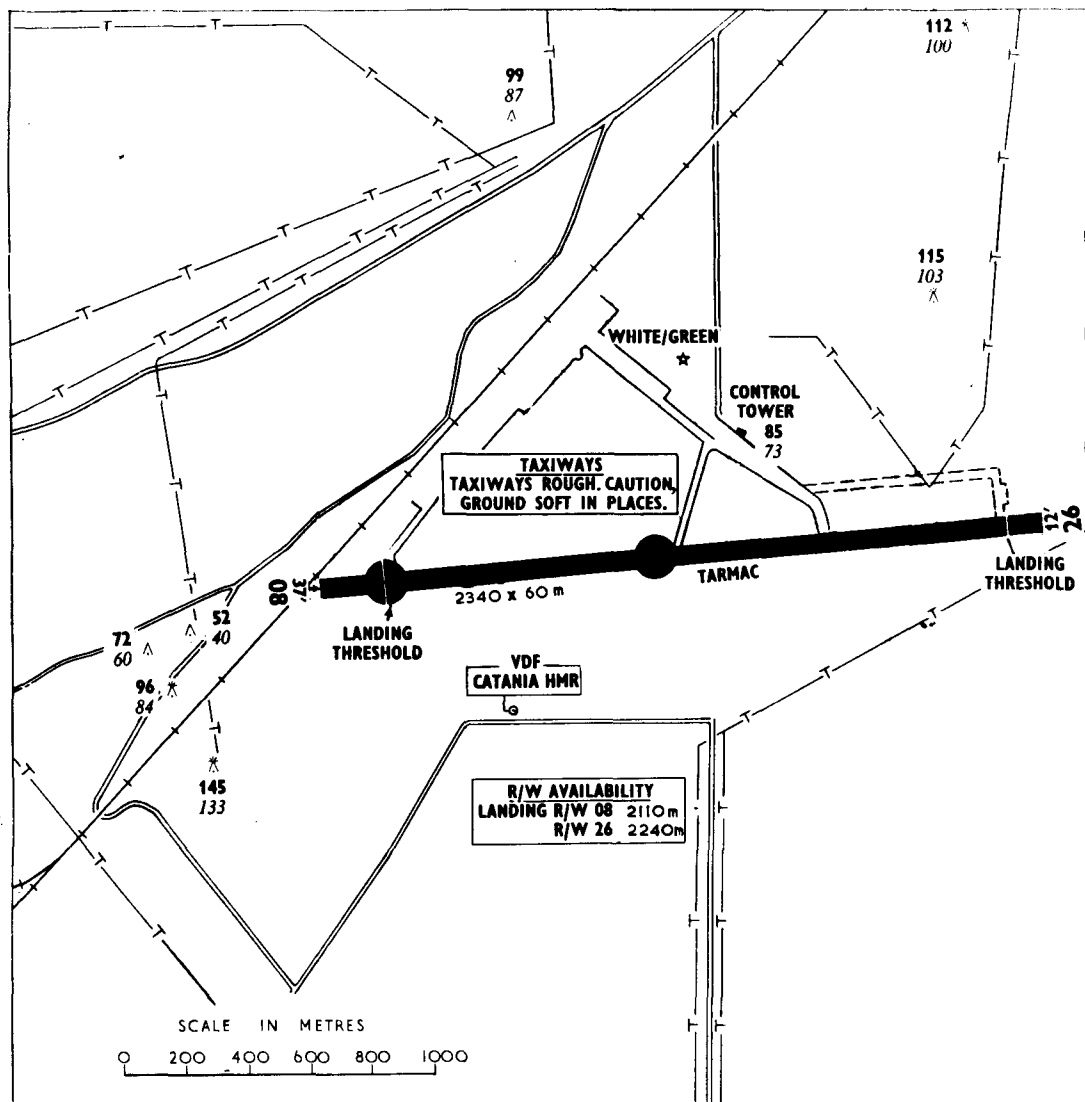


SERIAL No 37271

DATE: 5-8-64

Reason for Re-issue:

PARKING RAMPS
 CAIRO APT.



CIRCUITS: R/W 08:-RIGHT HAND

CATANIA (FONTANA ROSSA) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
08/26	NIL	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08/26	GREEN	WHITE/YELLOW (16 INS. HIGH 1 METRE OUT FROM EDGES)

OBSTRUCTION, TAXIWAY, ADME BEACON
ANGLE OF APPROACH LIGHTS

ELEVATIONS (FT) :-

OFFICIAL AERODROME	37
LOWEST THRESHOLD (26)	12
LAT 37° 28' N, LONG 15° 04' E	
VARIATION 1° W	Bearings all magnetic
SERIAL No 31654/1	DATE 26.4.65

Reason for Re-Issue:
PRESENTATION

AERODROME CHART CATANIA (FONTANA ROSSA)

Obstruction 169 /63 830M
before threshold 17

Λ
78
72

62

56

L1

ASPHALT - SEE NOTE

2360 M

32 M. WIDE

NDB ○

KERKYRA

Λ
481
475

45 M. WIDE

•
226
200

6
35

92
86

105
99

Λ 66
60

SCALE - METRES

0 200 400 600 800 1000

High ground 396 390 800 M.
before threshold 750 M. left
of centre line.

Runway surface has only a superficial asphalt treatment UFN. Possibly subject to standing water after heavy rain. Slippery after rain. In places runway is 40cm higher than airfield surface. Taxiways very narrow. N. Taxiway 14m. S. Taxiway 16m. Maintain taxiway centre line. Do not cut corners. Beware traffic on road 60m. N. of runway. Pedestrians may use causeway just south of runway.

CORFU (KERKYRA) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
-----	----------	-------------------------------

17/35	NIL	
-------	-----	--

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
17	3 (ELEVATED 18 IN) EACH SIDE OF RUNWAY (GREEN)	FIRST 1000M ELECTRIC, ELEVATED 18" REMAINDER GOOSENECKS
35	3 GOOSENECKS EACH SIDE OF R/W.	FIRST 1360M GOOSENECKS, REMAINDER ELECTRIC. ELEVATED 18"

OBSTRUCTION, FLOOD, ADME BEACON
WIND INDICATOR.
TAXIWAYS BY GOOSENECKS.

ELEVATIONS (FT)

OFFICIAL AERODROME	6
LOWEST THRESHOLD ()	6
LAT 39° 36' N, LONG 19° 54' E	
VARIATION 1° E Bearings all magnetic	
SERIAL No. 50216/1	DATE: 15.7.65

Reason for Re-issue:

REVISED LAYOUT

AERODROME CHART CORFU (KERKYRA)

SECT SAFE ALT.
25 N.M. 7000'

50°

20° 00' E

SECT SAFE ALT.
25 N.M. 7000'

CORFU

NDB (B.C.P.)

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APPROACH KERKYRA APP

119.7

TOWER KERKYRA TWR

119.7

TRANS ALT 5000 FT.

G/S (KTS)			110	130	150
NDB TO TURN			4.06	3.28	3.0

G/S	170	190	210	230	250
NDB TO TURN	2.39	2.22	2.08	1.57	1.48

MISSED APPROACH

Climb to 4000 3994 on 150° within 15 n.m. and request clearance to join holding at 6000 5994 or proceed as instructed by ATHENS ACC

ELEVATIONS (FT)

OFFICIAL AERODROME 6

LOWEST THRESHOLD 6

THRESHOLD R/W

VARIATION 1° E Bearings all magnetic

SERIAL No. 50216/11 DATE 31.3.65.

Reason for Re-issue:

REVISED PROCEDURE

NDB (B.C.P.)
CORFU

39°

40°

30°

SECT SAFE ALT.
25 N.M. 3000'

SECT SAFE ALT.
25 N.M. 6500'

INITIAL APPROACH
MIN EN ROUTE ALT

SHUTTLE ON
HOLDING

6000
5994

NDB

165°

7.5 NM

3500
3494

RATE ONE
TURN LEFT

3000
2994

APPD. MIN.

N.N.W.

S.S.E.

N.M. 12 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 11 12 N.M.

Holding
pattern
min alt
6000'

1196
1190

2988
2982

2706
2700

3416
3410

ALBANIA
(PROHIBITED)

NDB
SWK 403

292
286

396
390
996
990
1890
1884

1082
1076

1670
1664

755
273

DUBROVNIK AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
12	Calvert system white H.I. (variable) uni-directional. Length 900m 3 crossbars at 300m intervals. Red H.I. (variable) omni-directional. Length 900m 3 crossbars at 300m intervals.	3 1/4
30	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
12 & 30	Green H.I. variable bi-directional	White H.I. variable bi-directional. Last 750m each end yellow.

OBSTRUCTION, TAXIWAY, ADME BEACON G/W
IDENT BEACON DUB. WHITE

ELEVATIONS (FT) :—

OFFICIAL AERODROME	528
LOWEST THRESHOLD (30)	482
LAT 42 ° 33 ' N, LONG 18 ° 16 ' E	
VARIATION 0 °	Bearings all magnetic
SERIAL No 50210/2	DATE : 9.4.65

Reason for Re-issue:
VDF ADDED

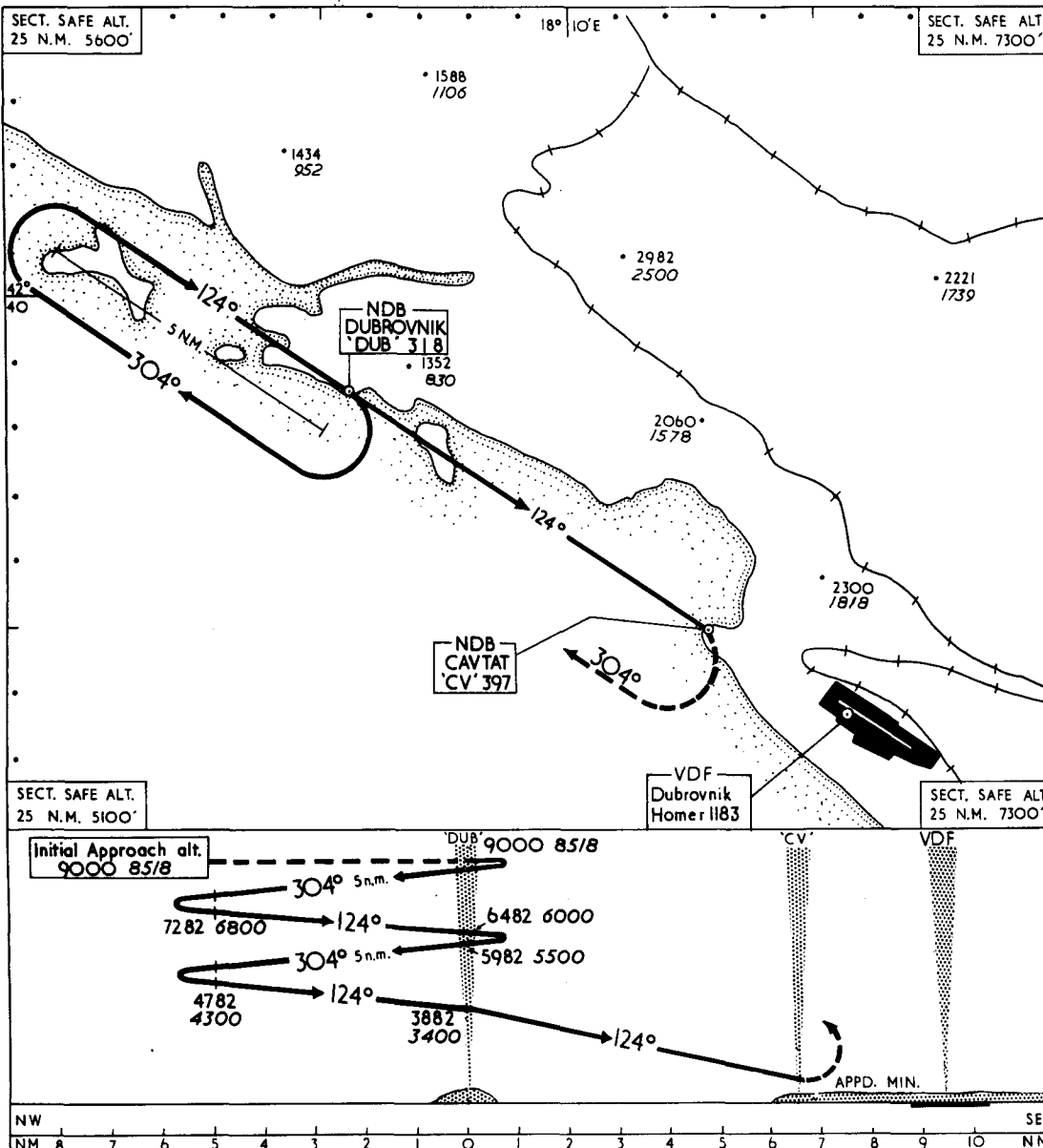
AERODROME CHART DUBROVNIK

500 0 500 1000
SCALE IN METRES

STOPWAYS: 60m each end.

SECT. SAFE ALT.
25 N.M. 5600'

SECT. SAFE ALT.
25 N.M. 7300'



DUBROVNIK

NDB/VDF R/W 12

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APPROACH DUBROVNIK APP

118.3 123.8

TOWER DUBROVNIK TWR

118.3

TRANS ALT 9000 FT.

G/S(KTS)	70	90	110	130	150
DUB TO CV	5.33	4.20	3.32	3.00	2.36
CV TO ADME	1.53	1.28	1.12	1.01	0.53

G/S	170	190	210	230	250
DUB TO CV	2.18	2.04			
CV TO ADME	0.47	0.42			

MISSED APPROACH

Turn right onto 304° climbing to NDB 'DUB'
and request further instructions.

ELEVATIONS (FT) :-

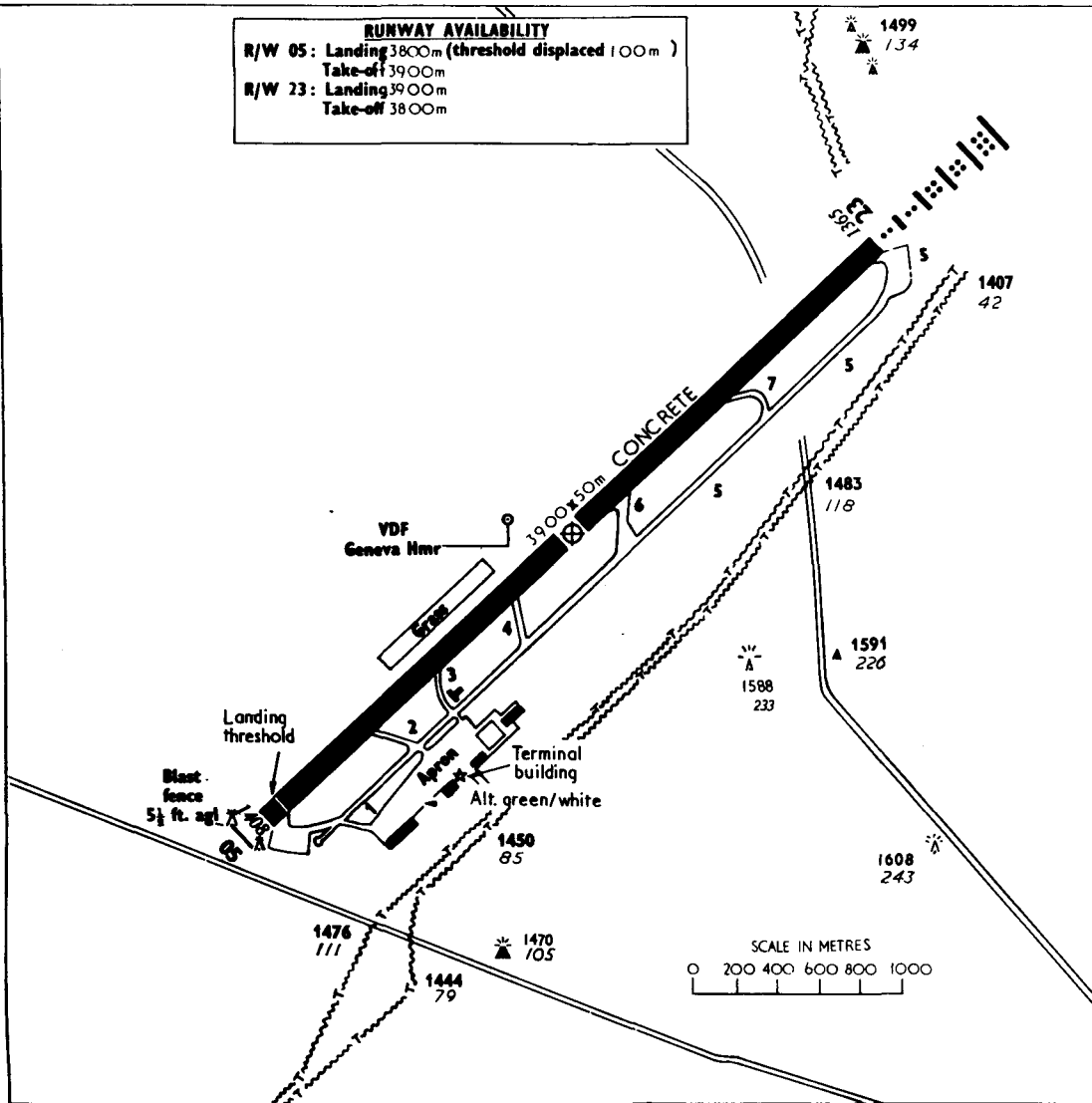
OFFICIAL AERODROME	528
LOWEST THRESHOLD	482
THRESHOLD R/W 12	528
VARIATION 0°	Bearings all magnetic
SERIAL No. 50210/2	DATE 12.4.65

Reason for Re-Issue:

VDF ADDED

NDB/VDF R/W 12
DUBROVNIK

RUNWAY AVAILABILITY
 R/W 05: Landing 3800m (threshold displaced 100m)
 Take-off 3900m
 R/W 23: Landing 3900m
 Take-off 3800m



GENEVA AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
05	NIL	3°
23	HIGH INT. CENTRE-LINE & 5 BARS LOW INT. RED CENTRE-LINE	3°

NE NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
05	HIGH & LOW INT. GREEN WINGS (100M. UP R/W)	HIGH & LOW INT. WHITE (LAST 600M. YELLOW)
23	HIGH & LOW INT. GREEN	

OBSTRUCTION, TAXIWAY, (BLUE WITH
 ADDITIONAL GREEN C/L ON TAXIWAYS 3 & 7),
 ADME BEACON, WIND INDICATOR, APRON
 FLOODS, TOUCHDOWN ZONE (915m) R/W 23.

ELEVATIONS (FT): -

OFFICIAL AERODROME	1411
LOWEST THRESHOLD (23)	1365
LAT 46° 14' N, LONG 06° 07' E	
VARIATION 4° W	Bearings all magnetic
SERIAL No. 40330/1	DATE 4 1 65
Reason for Re-issue:	

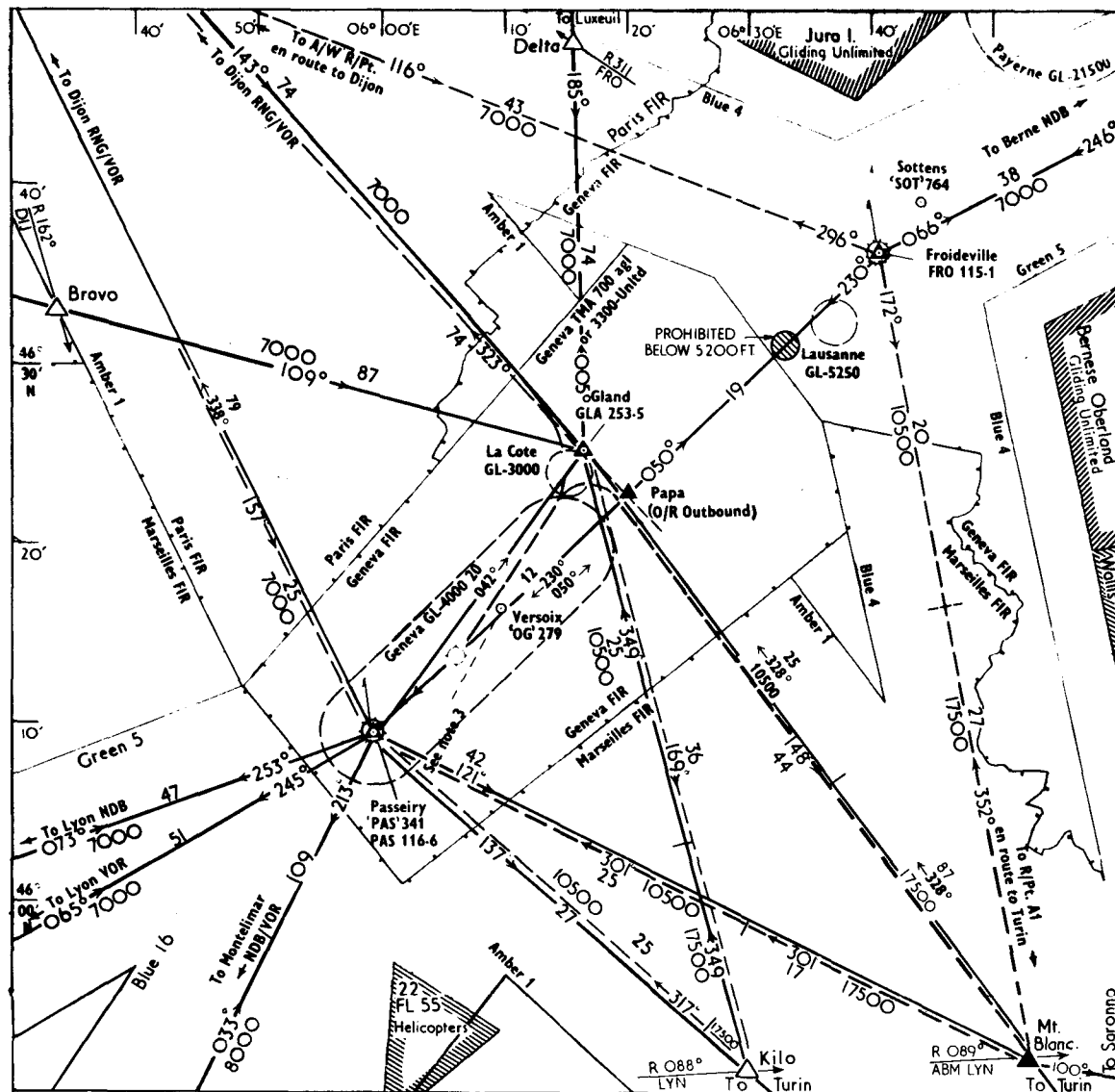
R/W AVAILABILITY

AERODROME CHART GENEVA

ELEVATED LIGHTING: 1. R/W LIGHTS (2 M OUTSIDE EDGES) ARE 25 INS. HIGH, EXCEPT ON FIRST 100M. R/W 05, WHICH ARE 14 INS. HIGH TO PERMIT TURN ROUNDS.

2. TAXIWAY LIGHTS ARE 14 INS. HIGH, 2 M OUTSIDE EDGES.

WARNING: A/C MUST NOT OVERFLY GENEVA BELOW 2300 FT. (A.G.L.) BY DAY, & 8200 FT. (A.M.S.L.) BY NIGHT.



INBOUND AND OUTBOUND ROUTES

GENEVA

INBOUND/OUTBOUND ROUTES

HOLDING PATTERNS					
HOLDING POINT	MIN ALT	TIME	INBOUND	TURN	BEA
GLAND	4000	1	050	RIGHT	
(DO NOT OVERSHOOT NDB VERSOIX)					
PASSEIRY	7000	1	033	RIGHT	

AIRCRAFT MAY BE INSTRUCTED TO HOLD AT A DESIGNATED REPORTING POINT. IF NO HOLDING PATTERN IS PUBLISHED THE PROCEDURE IS AS FOLLOWS:—UPON ARRIVAL AT REPORTING POINT TURN RIGHT 180° AND FLY FOR 1 MINUTE ON A TRACK PARALLEL TO THE AIRWAY AND OPPOSITE TO THE ORIGINAL TRACK. TURN RIGHT 180° TO REGAIN ORIGINAL TRACK AND FLY BACK TO THE REPORTING POINT.

NOTES

- Altitudes shown on chart are minimum
Approach altitudes.
- Minimum crossing altitudes—Outbound.

Facility	Outbound	Alt.
(a) Passeiry	N.W. via A1	7000
NDB/Mkr/VOR	S.W. via GS	7000
	S.W. via B16	7000
	S.E. via A1	17000
(b) Gland	N.E. via GS (Papa)	4000
NDB/Mkr	N via UA24, N.W. via A1	7000
	S.E. via A1	17000
(c) Froideville	N.E. via GS	7000
VOR	N.W. via B4	7000
	S.E. via B4	17000
- Outbound aircraft may be instructed to climb in the Passeiry holding pattern i.e. from PAS turn left towards NDB Versoix for 1 min. then return to PAS.

LEGEND

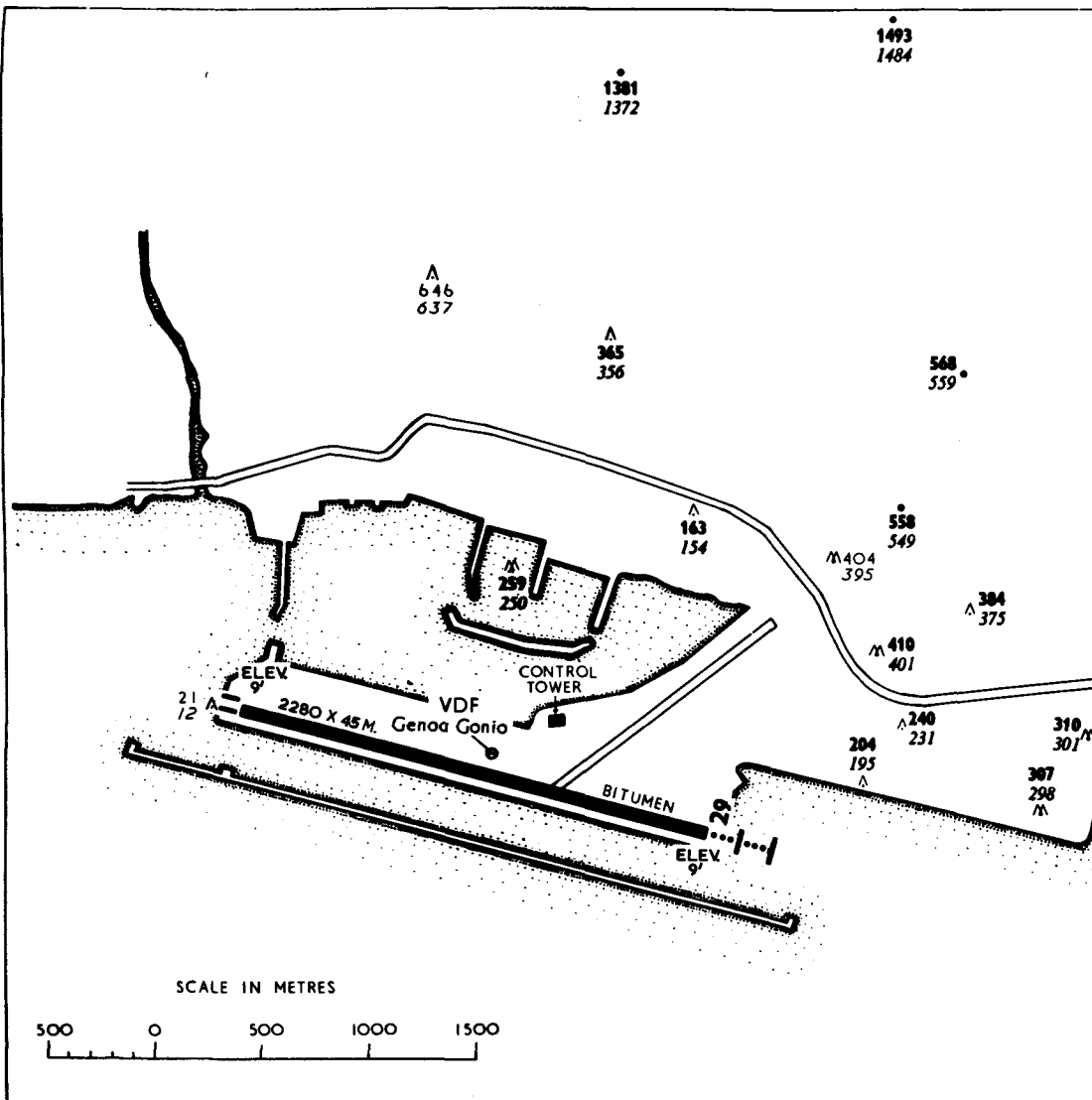
Inbound & Outbound routes shown ———
Alternate routes shown - - - - -

VARIATION 4°W	Bearings all magnetic
SERIAL No. 50373/3	DATE 18. 6. 65.

Reason for Re-issue:
ROUTES REVISED

GENEVA

EFFECTIVE DATE 1.7.65.



GENOA AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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Dept. as part of the Operations Manual

LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH
29	H.I. & L.I. WHITE CENTRE-LINE AND BARS
11	NIL

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
29 11	H.I. & L.I. GREEN WINGBARS	H.I. & L.I. AMBER

OBSTRUCTION, TAXIWAY, FLASHING WHITE LIGHT
ON R.H. SIDE OF THRESHOLD RUNWAY II.
ANGLE OF APPROACH

ELEVATIONS (FT) : —

OFFICIAL AERODROME	9
LOWEST THRESHOLD ()	9
LAT 44 °25' N, LONG 08 °50' E	
VARIATION 3 °W	Bearings all magnetic
SERIAL No 40017/1	DATE : 23.11.64

Reason for Re-Issue:

OBSTRUCTION ADDED

AERODROME CHART GENOA

CIRCUITS: R/W 11: - RIGHT HAND

NOTE: AFTER TAKE-OFF FROM R/W 29 TURN LEFT, FROM R/W 11 TURN RIGHT AND THEN AS INSTRUCTED.

LOCAL WIND EFFECTS

1. BEA CROSSWIND LIMITS FOR TAKE-OFF AND LANDING AT GENOA:-

Take-offs and landings are not permitted at Genoa if winds reported are above the limits given below:-

WIND DIRECTION FROM (MAGNETIC)

WIND SPEED (KNOTS)

100° to 130° (inc)

Normal Crosswind Limits Apply

140°

50

160°

32

180° to 220° (inc)

25

230°

28

240°

19

260°

29

270°

42

280° to 300° (inc)

Normal Crosswind Limits Apply

310°

42

330°

23

350° to 050° (inc)

15

070°

23

090°

42

2. TURBULENCE

Extreme turbulence may be expected even though the surface wind is not excessive, particularly with wind direction between North-West and East. Exercise extreme caution.

BRITISH EUROPEAN AIRWAYS

BEA

Reason for Re-issue:

SERIAL No 40054/1

Presentation

DATE: 18 11 64

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GENOA

GIBRALTAR AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI /CAO Type Angle
09	RED BAR AT R/W EXTREMITY WITH 3 WHITE STUBS AT THRESHOLD, 185M. & 365M.	X
27	CENTRE-LINE OF 3 WHITE FLASHING LIGHTS AT 1000M., 1645M. & 2745M.	X

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
09	GREEN 92M. ALONG R/W	OMNI-DIR. WHITE WITH SODIUMS AT APPROACH END.*
27		

NOTE*

SODIUMS ARE HI & ARE SWITCHED OFF
WHEN A/C LINED UP WITH R/W ON
FINAL APPROACH.

OBSTRUCTION, IDENT BEACON 'GB' (RED)

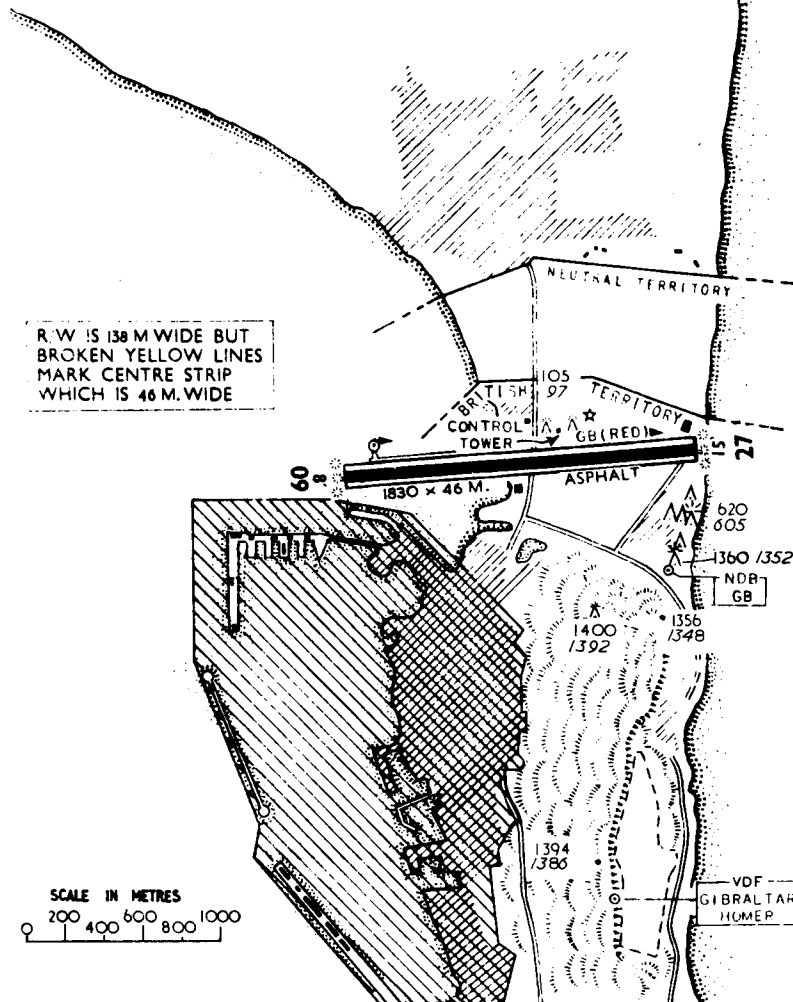
ELEVATIONS (FT): -

OFFICIAL AERODROME	15
LOWEST THRESHOLD (09)	8
LAT 36° 09' N, LONG 05° 21' W	
VARIATION 9° W	Bearings all magnetic
SERIAL No. 40331/1	DATE 2.12.64

Reason for Re-Issue:

R/W HOMER DELETED

AERODROME CHART GIBRALTAR



SPECIAL INFORMATION: A WHITE LINE 2 m WIDE IS PAINTED ACROSS FULL WIDTH OF R/W 455 m FROM EACH END. THE BAR AT DOWNWIND END IS MARKED BY 6 CONTACT LIGHTS (HN) EACH SIDE OF CENTRE STRIP. OVERSHOOT ACTION MUST BE TAKEN IN THE EVENT OF NOT TOUCHING DOWN BEFORE REACHING DOWN WIND BAR. OVERSHOOT ACTION IS ADVISED IF SURFACE WET & A/C IS NOT FIRMLY ON THE GROUND WITH BRAKES ACTING EFFECTIVELY BY THE TIME THE SPANISH ROAD (WHICH BISECTS R/W) HAS BEEN REACHED. WARNING CHAIN ARRESTER GEAR IS SOMETIMES IN USE SITUATED BETWEEN 305 m & 457 m FROM ENDS OF R/W. CIRCUITS RIGHT HAND R/W 09.

GIBRALTAR CIRCUIT CHART

BRITISH EUROPEAN AIRWAYS

BEA

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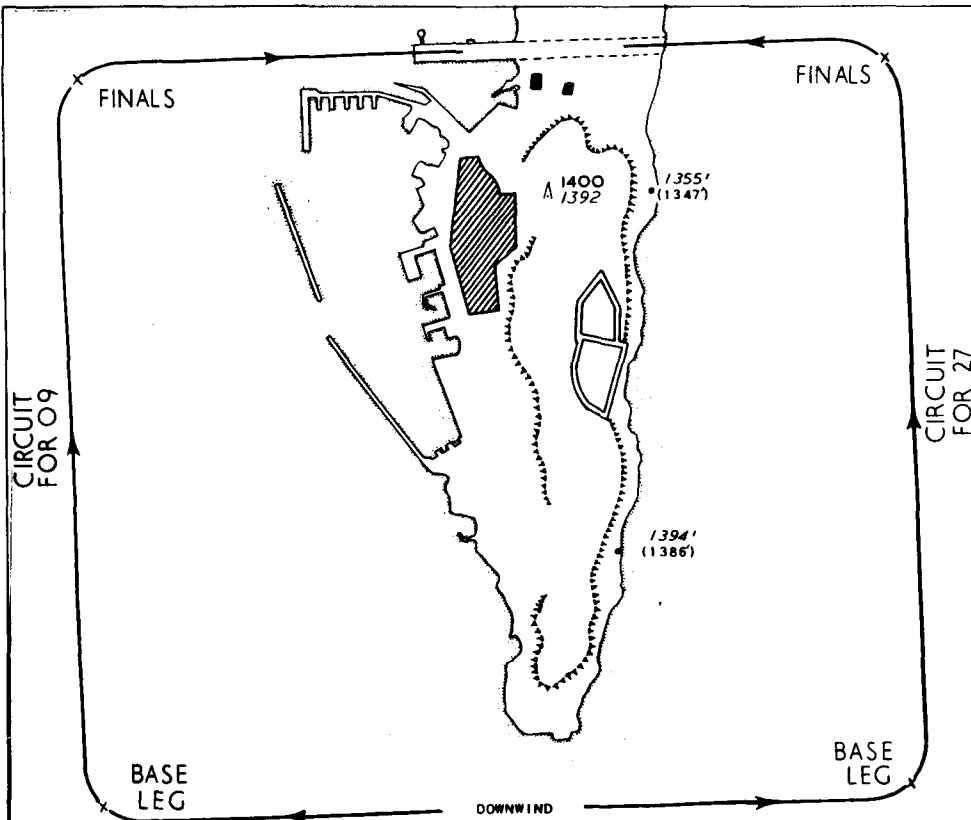


FIG. 1. STANDARD CIRCUITS

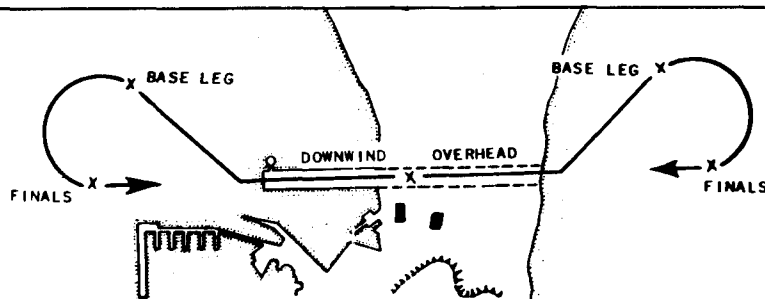


FIG. 2. RECIPROCAL TRACK CIRCUITS

SERIAL No 29659

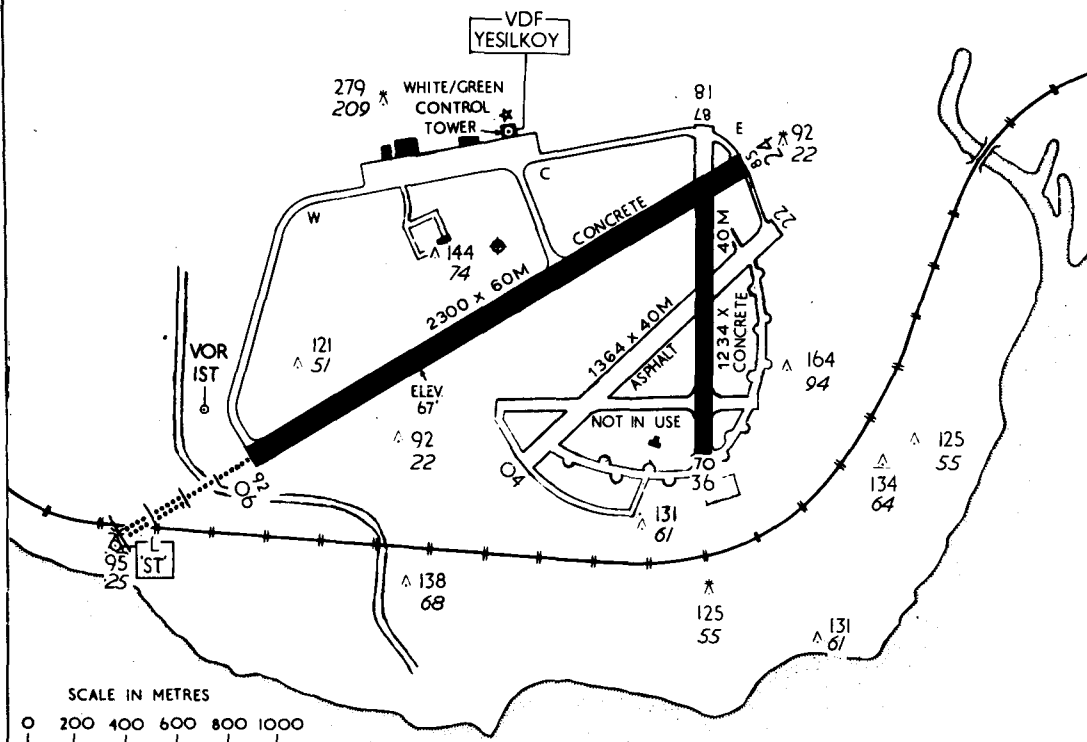
DATE 5.8.64

Reason for Re-issue:

CIRCUIT CHART
GIBRALTAR

177
107

118
48



ISTANBUL AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06	WHITE VAR. INT. (5 STAGE) CENTRE LINE & BARS	
24	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06	GREEN	VAR. INT.
24	GREEN	VAR. INT.

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, FLOODLIGHTS

ELEVATIONS (FT)

OFFICIAL AERODROME	92
LOWEST THRESHOLD (36)	70
LAT 40° 58' N, LONG 28° 49' E	
VARIATION 3° E	Bearings all magnetic
SERIAL No. 50184/2	DATE 24. 6. 65

Reason for Re-issue:

R/WAY 04/22 CLOSED

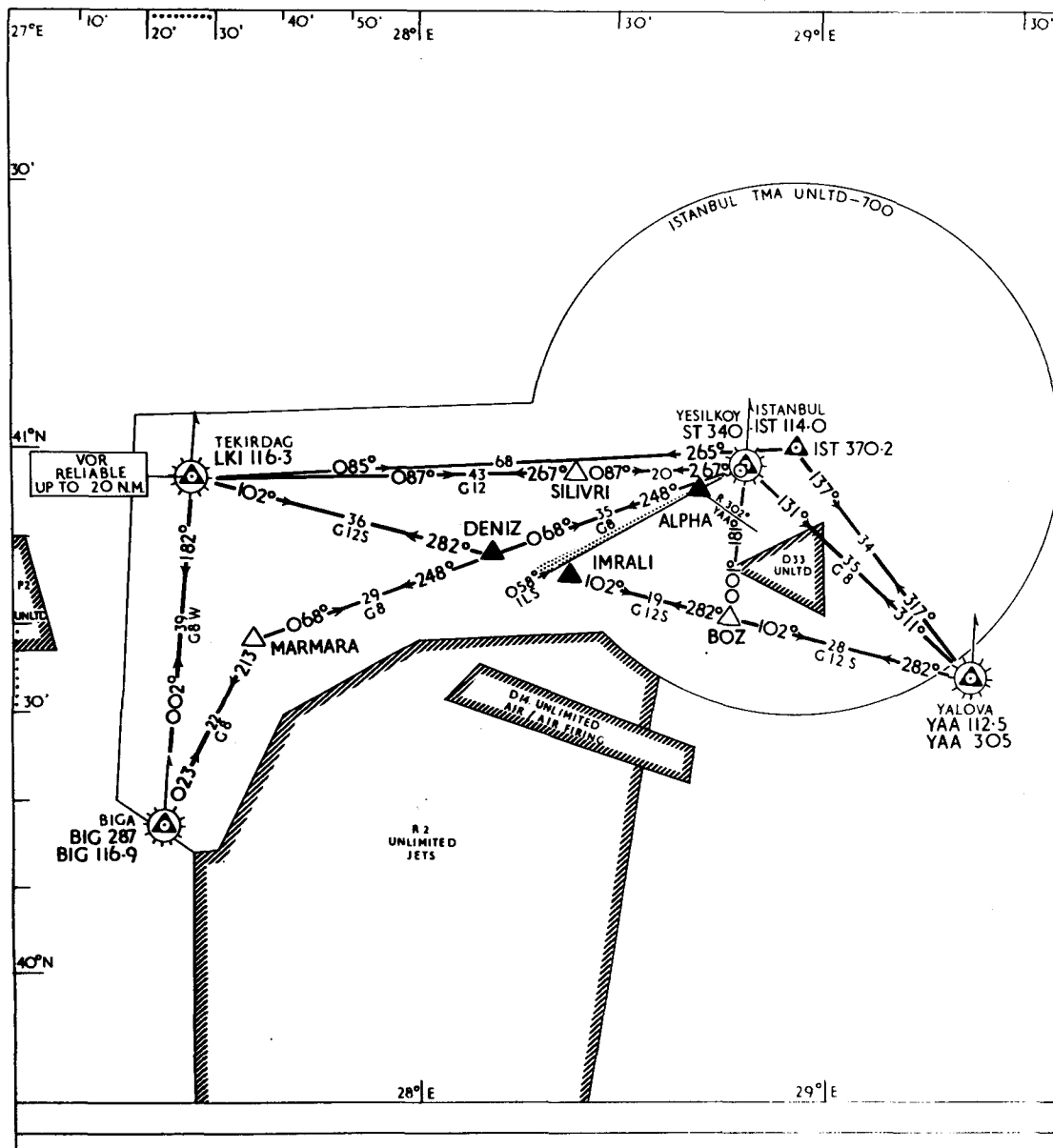
AERODROME CHART ISTANBUL

NOTE: RUNWAYS 18/36 AND 04/22 RESTRICTED TO MAXIMUM OF 13600 KGS A.U.W.

WARNING: (R/W 06/24)

1. RUNWAY & THRESHOLD LIGHTS (FRANGIBLE) ARE 28 1/2 INCHES ABOVE GROUND LEVEL AND APPROX. 1 M OUTSIDE EDGE OF PAVED SURFACE.
 2. R/W ON EMBANKMENT WITH SLOPING STRIPS.
 3. GROUND IMMEDIATELY BEYOND THRESHOLDS EXTREMELY SOFT & AFTER DISTANCE OF 92 M SLOPES AWAY SHARPLY.
 4. WARNING: TAXIWAYS EXTENSIONS CONSOLIDATED ONLY BY TARMAC CHIPPINGS. TAXI ON CONCRETE SURFACES ONLY.
- AERODROME CIRCUITS: ALL CIRCUITS TO BE MADE OVER SEA.

WARNING: DROP EXISTS OFF TAXIWAY WHEN TURNING LEFT OFF CENTRE TAXIWAY ONTO WESTERN TAXIWAY. EXTENDS FOR LAST 15 M OF CENTRE TAXIWAY AND FIRST 106 M OF WESTERN TAXIWAY. SOFT SHOULDERS ON ALL TAXIWAYS EXCEPT AS ABOVE AND NOT TO BE USED FOR TAXIING.



ISTANBUL TMA & HOLDINGS

BRITISH EUROPEAN AIRWAYS

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HOLDING PATTERNS

HOLDING POINT	MIN ALT FEET	TIME MINS	INBOUND HEAD ° M	TURN
IMRALI INT	3000	2	102	RIGHT
ISTANBUL TVOR	3000	2	253	RIGHT
" "	3000	2	062	LEFT
" "	3000	2	247	RIGHT
" R/R	4000	2	250	RIGHT
" ILS	3000	1	058	RIGHT
INT. R 302° Yalova	3000	2	087	LEFT
SILIVRI INT.	3000	2	085	LEFT
TERKIRDAG VOR	4500	2	085	LEFT
YALOVA VOR/NDB	6000	2	360	RIGHT
YESILKÖY NDB ST	3000	2	240	RIGHT
" " "	2000	2	210	RIGHT

% NOT TO EXCEED TAS 250 KTS AND TURN
RATE 1 1/2°

VARIATION 3° Bearings all magnetic

SERIAL No 50419/4 DATE 26.6.65

Reason for Re-issue

TMA CHANGES, DAs ADDED

TMA & HOLDINGS ISTANBUL

LISBON AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
03	L.I. RED CENTRE LINE & BAR	3°
05	NIL	
21	H.I. WHITE CALVERT 900M & 5 BARS L.I. RED CENTRE LINE 900M & 2 BARS	3°
23	H.I. WHITE CENTRE-LINE & BARS	
18	NIL	3°
36	L.I. RED CENTRE-LINE & BAR	3°

NB. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
03/21	H.I. GREEN WHITE WING BARS	H.I. WHITE ELEVATED OMNI-DIR (LAST 610M. YELLOW) H.I. BI-DIR. CENTRE LINE
05	L.I. GREEN	L.I. WHITE (LAST 610M YELLOW)
23	H.I. & L.I. GREEN	H.I. & L.I. WHITE (LAST 610M. YELLOW).
18 36	L.I. GREEN	VAR. INT. WHITE (LAST 610M. YELLOW).

Special note on lighting:-

H.I. lights are uni-directional except for R/W 03/21.
L.I. lights are omni-directional.

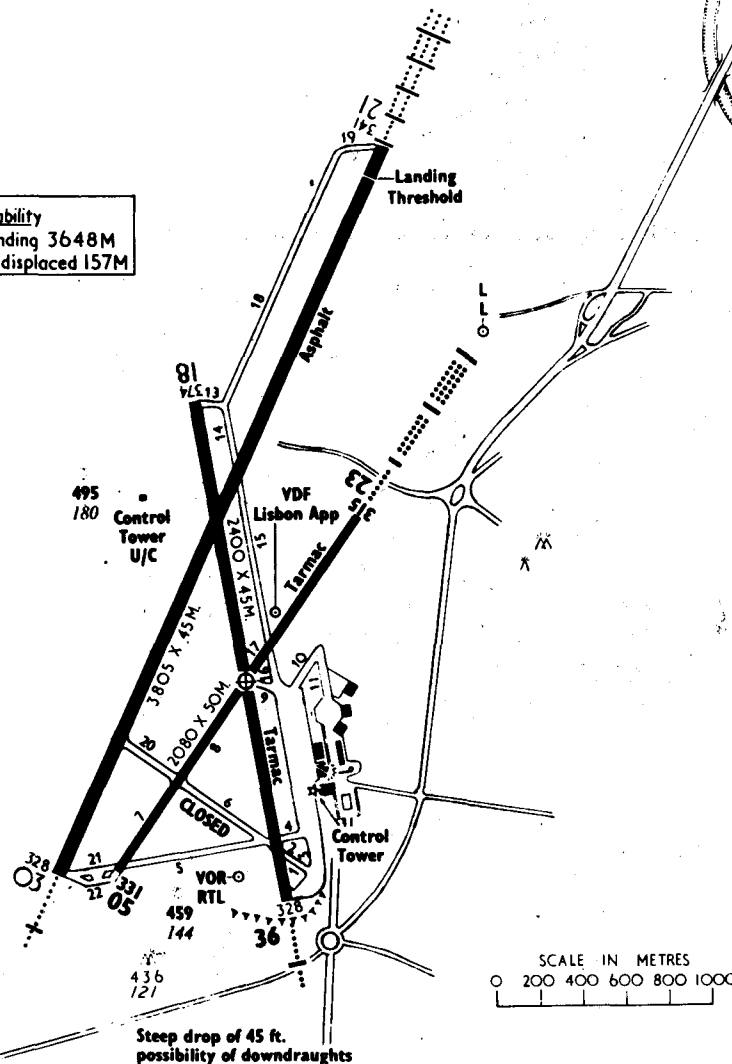
OBSTRUCTION, TAXIWAY, ADME BEACON
WIND & LANDING INDICATOR
RED STOP BARS ON TAXIWAYS BEFORE
THRESHOLDS 03 & 21

ELEVATIONS (FT) :-

OFFICIAL AERODROME	374
LOWEST THRESHOLD (23)	315
LAT 38° 46' N. LONG 09° 08' W.	
VARIATION 10° W	Bearings all magnetic
SERIAL No. 50272/1	DATE 20.5.65
Reason for Re-issue: LIGHTING	

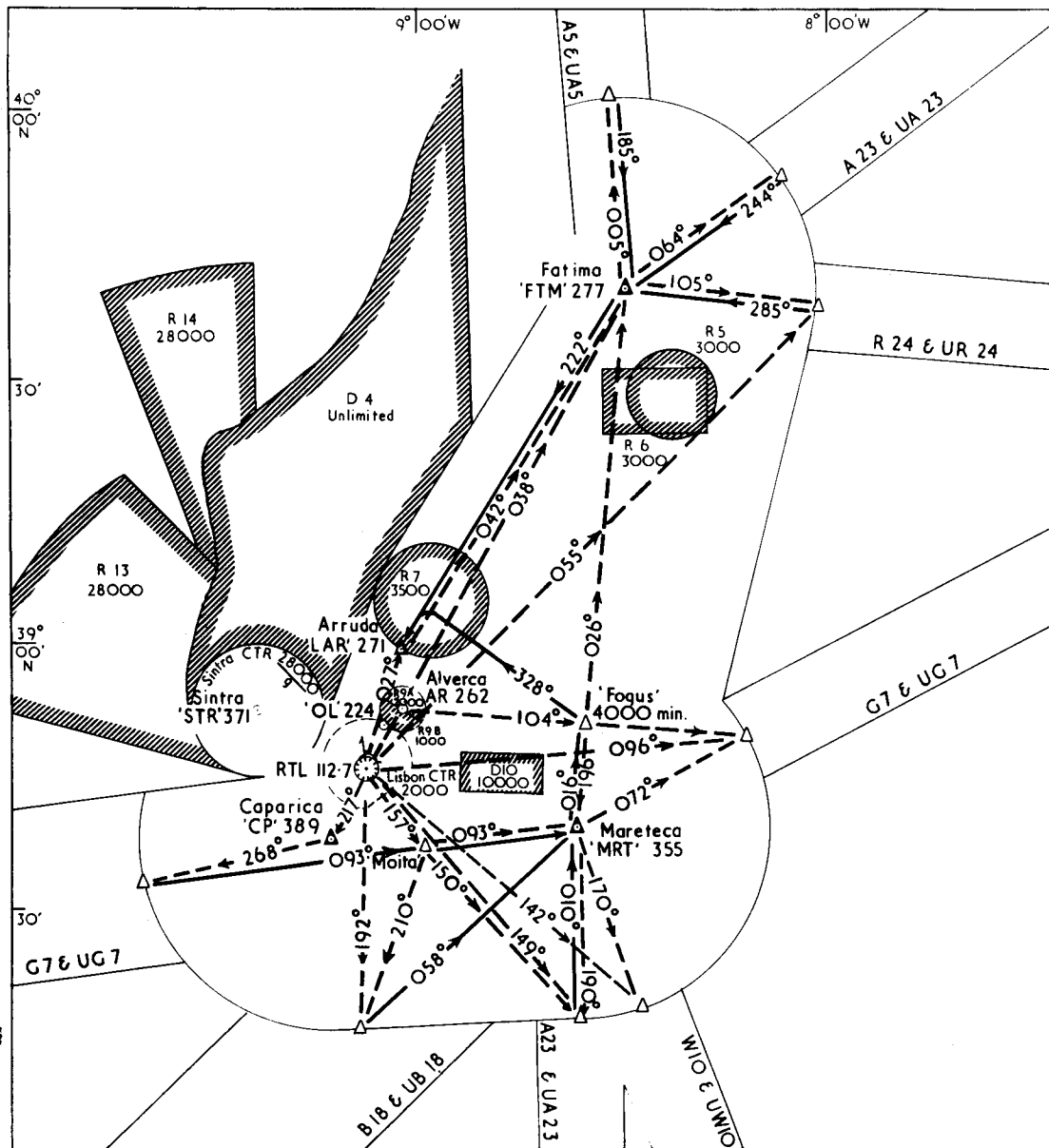
AERODROME CHART LISBON

R/W Availability
R/W 21 Landing 3648M
Threshold displaced 157M



ELEVATED LIGHTING: RUNWAY, THRESHOLD & TAXIWAY LIGHTS ARE APPROX. 2 FT. AGL & POSITIONED NOT LESS THAN
34M. OUTSIDE PAVED EDGES.

CIRCUITS: R/W 36 - RIGHT HAND.



LISBON

INBOUND/OUTBOUND ROUTES

BRITISH EUROPEAN AIRWAYS

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HOLDING PATTERNS

HOLDING POINT	MIN ALT FEET	TIME MINS	INB*NO HEAD*°M	TURN
CAPARICA CP LOC	2500	1	032	LEFT
FATIMA NDB	5000	1½	222	LEFT
ARRUDA NDB	4000	1	213	RIGHT
MARETECA NDB	5000	1½	337	RIGHT
LISBON OL LOC	2500	1	047	LEFT

Legend

INBOUND ROUTES
OUTBOUND ROUTES

SERIAL NO. 50376/2

DATE 23.6.65.

VARIATION 10° W

Bearings all magnetic

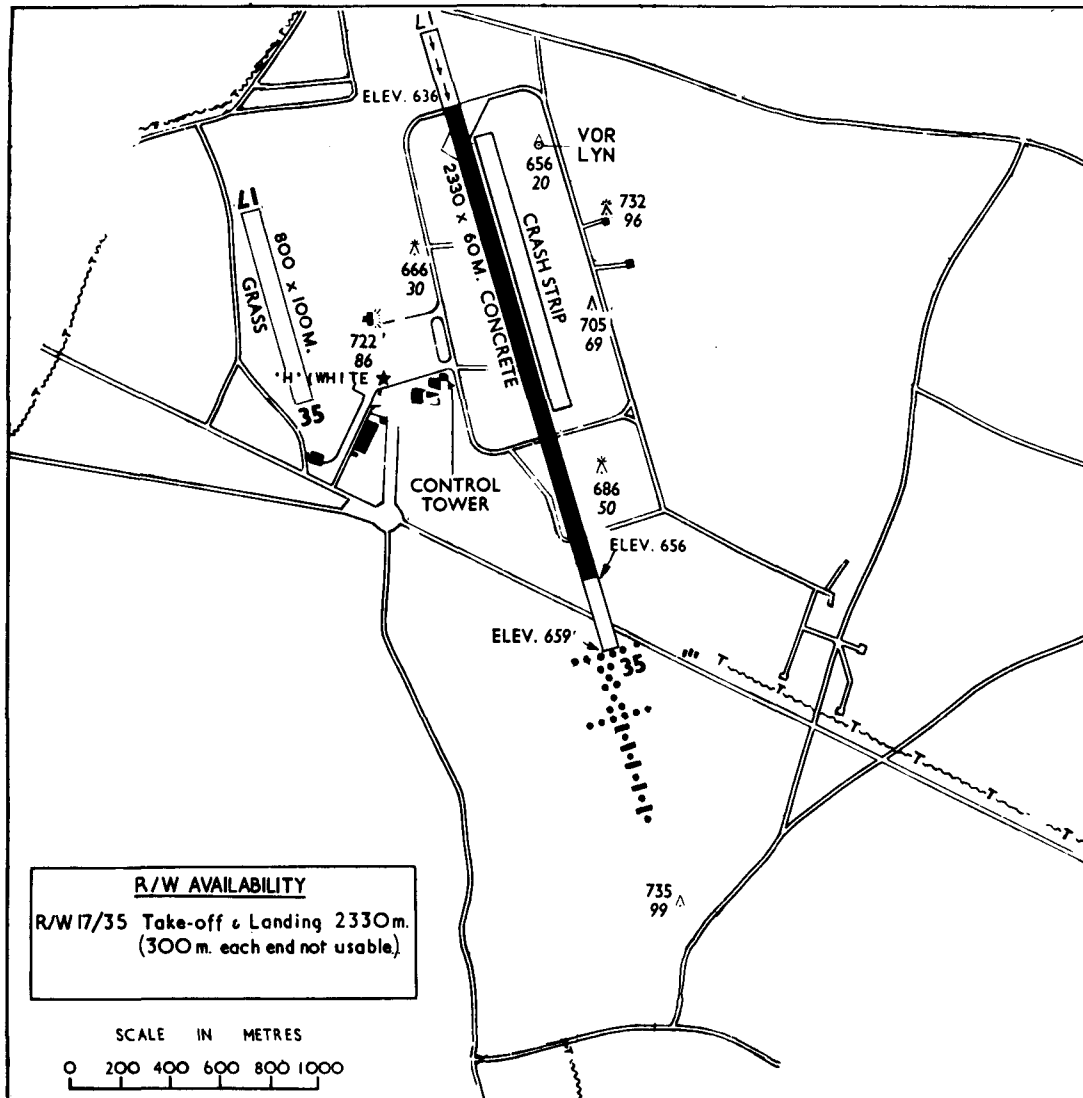
Reason for Re-Issue:

REVISED FACILITIES & ROUTES

INBOUND/OUTBOUND ROUTES

LISBON

EFFECTIVE DATE 30.6.65



STOPWAYS: R/W 35 - 135 M.

CIRCUITS: RIGHT HAND CIRCUITS FOR R/W 35 & STRIP R/W 17.

LYON AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
 ICAO
 Type
 Angle

R/W	APPROACH	
35	HIGH INT. RED & SODIUM WITH SODIUM BAR 310M. & RED BAR 585M. FROM THRESHOLD	
17	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

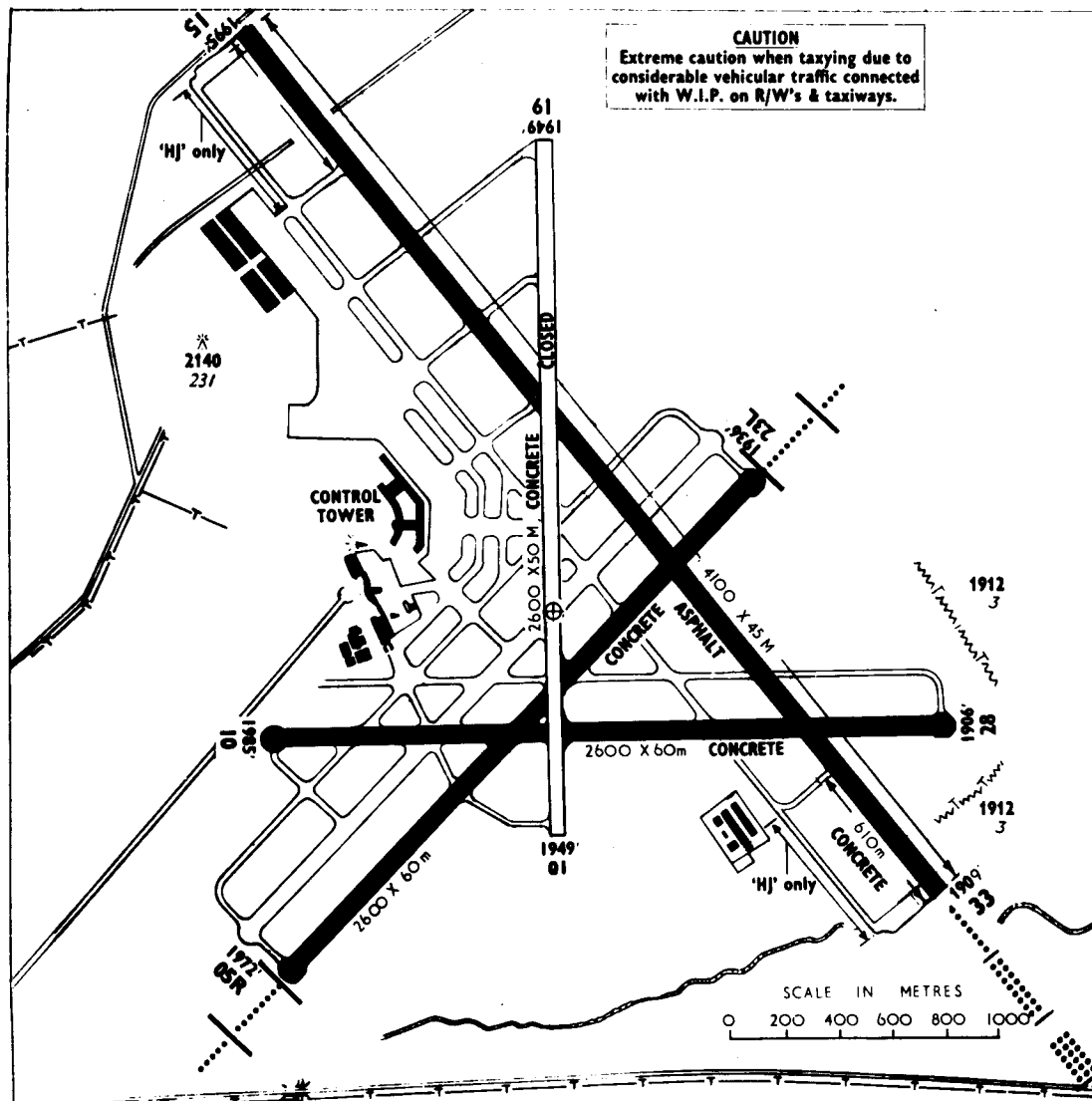
R/W	THRESHOLD	RUNWAY
35	HIGH INT. & LOW INT. GREEN WING-BARS.	HIGH INT. SODIUMS & LOW INT. WHITE WITH LOW INT. WINGBARS 305M. A LONG R/W FROM LANDING THRESHOLDS
17	LOW INT. GREEN WINGBARS	

OBSTRUCTION, TAXIWAY, IDENT. BEACON.
 H' (WHITE)

ELEVATIONS (FT) :-

OFFICIAL AERODROME	659
LOWEST THRESHOLD (17)	636
LAT 45° 44' N, LONG 04° 56' E	
VARIATION 5° W	Bearings all magnetic
SERIAL No. 50067/2	DATE: 10.3.65
Reason for Re-Issue:	
R/W AVAILABILITY.	

AERODROME CHART LYON



CAUTION
Extreme caution when taxiing due to considerable vehicular traffic connected with W.I.P. on R/W's & taxiways.

MADRID BARAJAS AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
01/19 U/S	NIL	
05R/23L	MED. INT. 458M. C/L WITH BAR AT 305M. FROM THRESHOLD	
10/28	NIL	
15	MED. INT. 535m CENTRE LINE & 2 BARS	
33	H.I. CENTRE-LINE AND BARS	X

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
01/28	MED. INT. GREEN WING-BARS	WHITE MED. INT. LAST 570m. YELLOW
05R/23L	MED. INT. GREEN WING-BARS	L.I. WHITE (LAST 570m. YELLOW)
15	H.I. GREEN WINGBARS	H.I. WHITE (LAST 610m. YELLOW)
33	H.I. GREEN WINGBARS	WHITE C/L ON FIRST 3050M. R/W 33 WITH WINGBARS AT 153, 305, 458, 610M. FROM T/H.

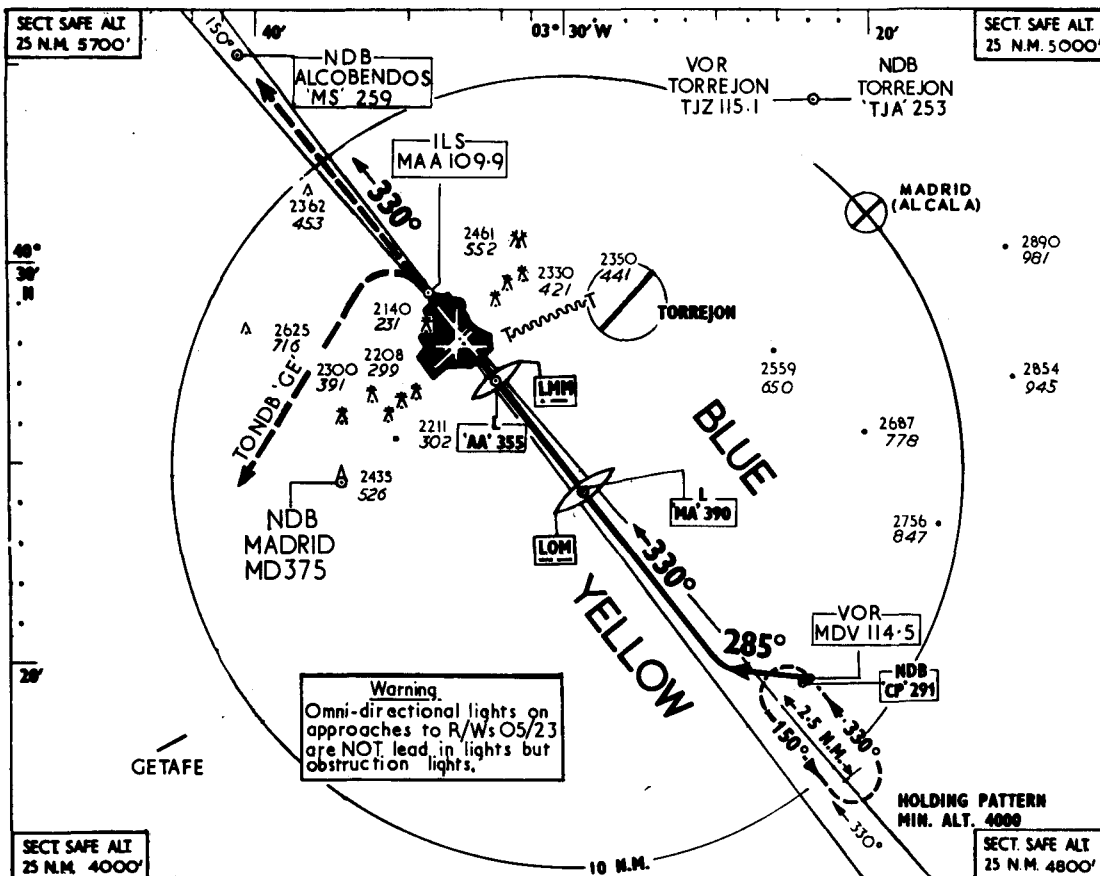
OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, APRON YELLOW RED CROSS /
GREEN ARROW AT THRESHOLD R/W 05R,
23L.

ELEVATIONS (FT):-

OFFICIAL AERODROME	1998
LOWEST THRESHOLD (33)	1909
LAT 40° 28' N, LONG 03° 34' W	
VARIATION 7° W	Bearings all magnetic
SERIAL No. 50261/2	DATE 21.5.65
Reason for Re-Issue:	
ELEVATION	

AERODROME CHART MADRID BARAJAS

STOPWAYS: R/W 15:- 64M.
CIRCUITS: RIGHT HAND FOR R/W'S 15, 19, 23L, 28. LEFT HAND FOR R/W'S 01, 05R, 10, 33.
WARNINGS: 1. R/W 15/33 PAVEMENT 62 M. WIDE FRANGIBLE R/W LIGHTS 20 INS. AGL. 6M IN FROM PAVED EDGES.
2. NO DAY MARKINGS R/W 15/33. 3. TAXIWAY LIGHTS ARE 24 INS. AGL. 4. HYDRANTS ON APRON 24 INS. AGL UNLIT.



MADRID (BARAJAS) ILS R/W 33

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APPROACH	MADRID APP.		
	120.9	119.9	121.5
TOWER	BARAJAS TWR.		
	118.1	121.5	3023.5
TRANS ALT	5000 FT.		

G/S (KTS)	70	90	110	130	150
LOM TO T/H	3.33	2.46	2.16	1.55	1.40
DESCENT ON G/P (FT/MIN)	322	414	506	598	690
G/S	170	190	210	230	250
LOM TO T/H	1.28	1.19			
DESCENT ON G/P (FT/MIN)	781	873			

OVERSHOOT

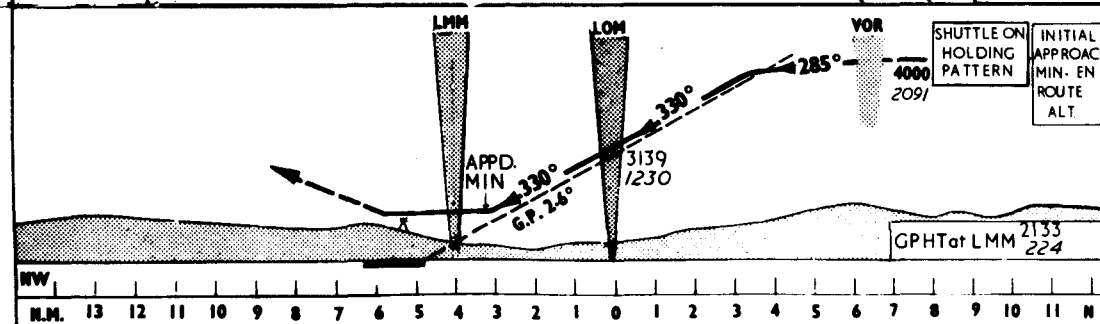
- CLIMB ON 330° TO 2500' 59/ THEN TURN LEFT CLIMBING TO 4000' 209/ AT NDB 'GE'.
- CLIMB ON 330° TO 4000' 209/ AT NDB 'MS'.

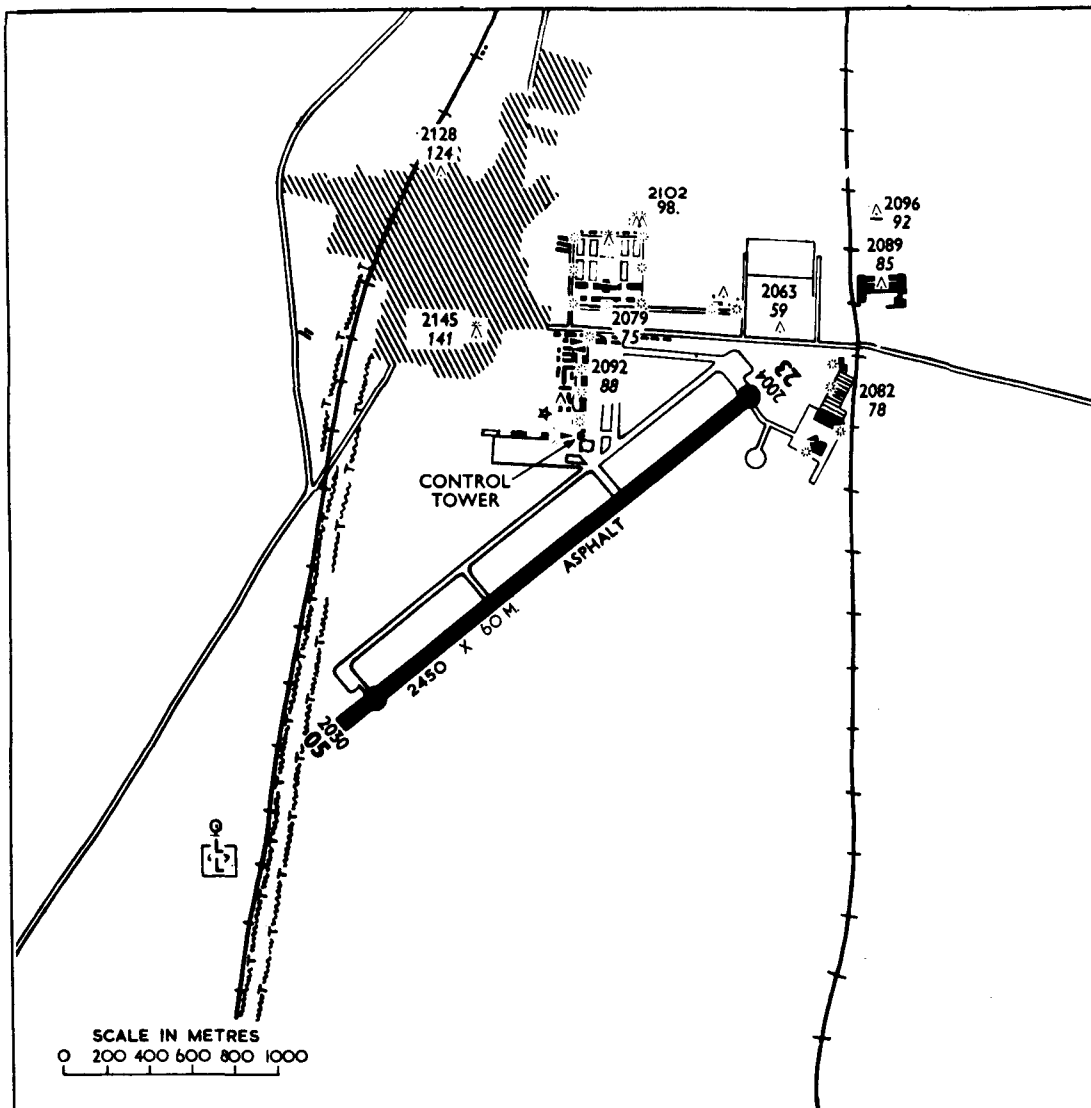
ELEVATIONS (FT) :-

OFFICIAL AERODROME	1998
LOWEST THRESHOLD	1909
THRESHOLD R/W 33	1909
VARIATION 7° W	Bearings all magnetic
SERIAL No. 50261/3	DATE: 19.5.65
Reason for Re-issue:	

ELEVATIONS & PROCEDURE HTS

ILS R/W 33
 MADRID (BARAJAS)





MADRID (GETAFE) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
05/ 23	NIL.	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
05/ 23	GREEN	MEDIUM INTENSITY WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR.

ELEVATIONS (FT) : -

OFFICIAL AERODROME	2030
LOWEST THRESHOLD (23)	2004
LAT 40° 18' N, LONG 03° 43' W	
VARIATION 8° W	Bearings all magnetic
SERIAL No. 40194/1	DATE : 26.11.64
Reason for Re-Issue: LIGHTING, NOTES & R/W WIDTH.	

N.B. ARRESTOR BARRIERS LOCATED IMMEDIATELY BEFORE THRESHOLDS

AERODROME CHART MADRID (GETAFE)

SECT. SAFE ALT.
25 N.M. 7000'

50'

03° 40'W

SECT. SAFE ALT.
25 N.M. 7000'

MADRID (GETAFE)

NDB-L/BCN R/W 05

BRITISH EUROPEAN AIRWAYS

EIA

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APPROACH

TOWER

GETAFE

TWR

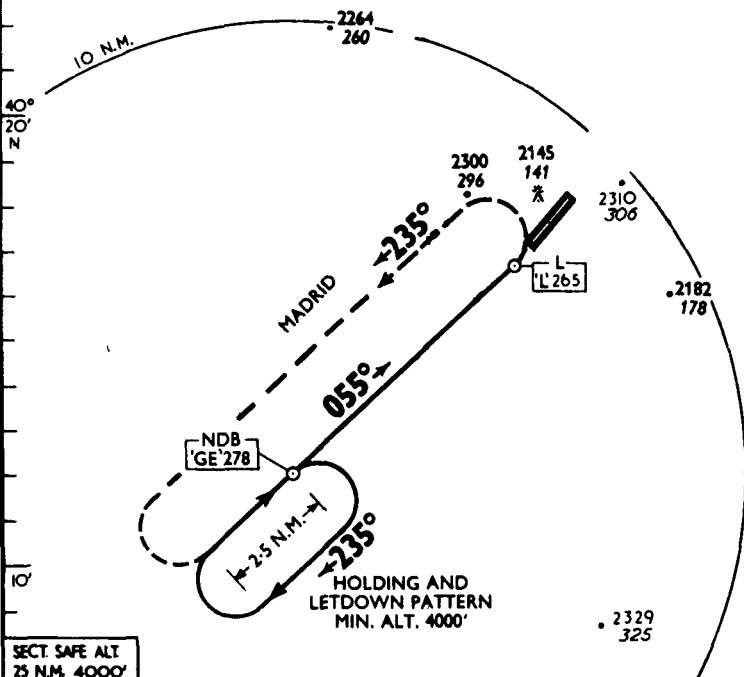
133.3 132.3 1215 5703

TRANS ALT

5000

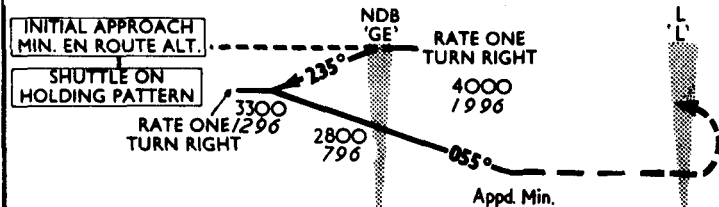
FT.

G/S (KTS)	70	90	110	130	150
GE TO T/H	6.12	4.51	3.58	3.24	2.54
G/S	170	190			
GE TO T/H	2.33	2.18			



SECT. SAFE ALT.
25 N.M. 4000'

SECT. SAFE ALT.
25 N.M. 4000'



SW N.M. 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 11 12 13 14 N.M. NE

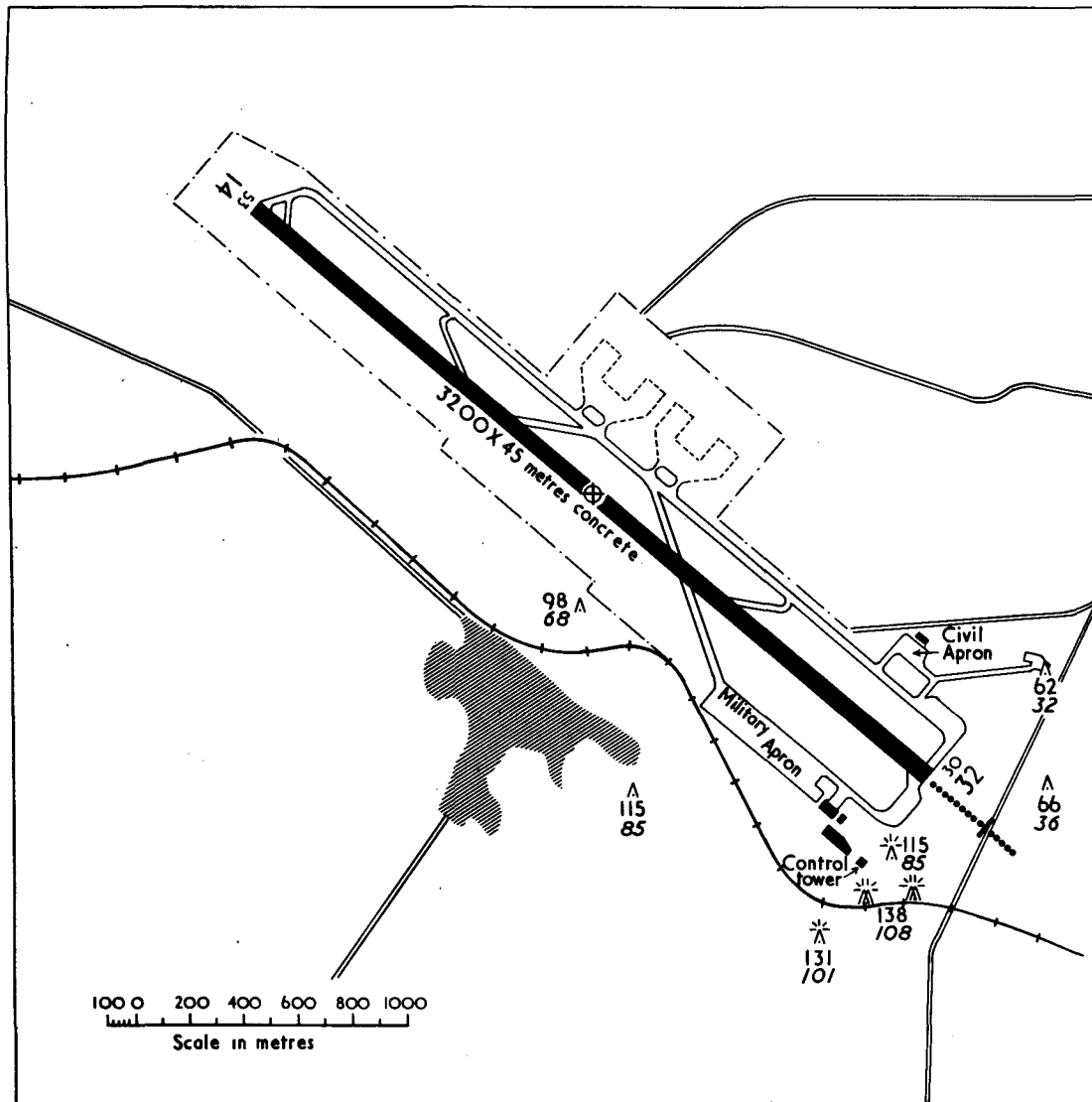
OVERSHOOT

TURN LEFT ONTO 235° CLIMBING TO 4000 / 1996
AND RETURN TO NDB

ELEVATIONS (FT.): -

OFFICIAL AERODROME	2030
LOWEST THRESHOLD	2004
THRESHOLD R/W 05	2030
VARIATION 8°W	Bearings all magnetic
SERIAL No. 50198/2	DATE: 19.3.65
Reason for Re-issue:	
D24 DELETED	

NDB-L/BCN R/W 05
MADRID (GETAFE)



MALAGA AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
14	NIL	
32	MED INTENSITY CENTRE-LINE & BAR	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
14	GREEN BARS	MED. INTENSITY WHITE (LAST 600M YELLOW)
32		

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, APRON.

ELEVATIONS (FT)

OFFICIAL AERODROME	53
LOWEST THRESHOLD (32)	30
LAT $36^{\circ}40'N$, LONG $04^{\circ}30'W$	
VARIATION $7^{\circ}W$	Bearings all magnetic
SERIAL No. 50257/2	DATE: 9.6.65.

Reason for Re-Issue:
COMPLETE REVISION

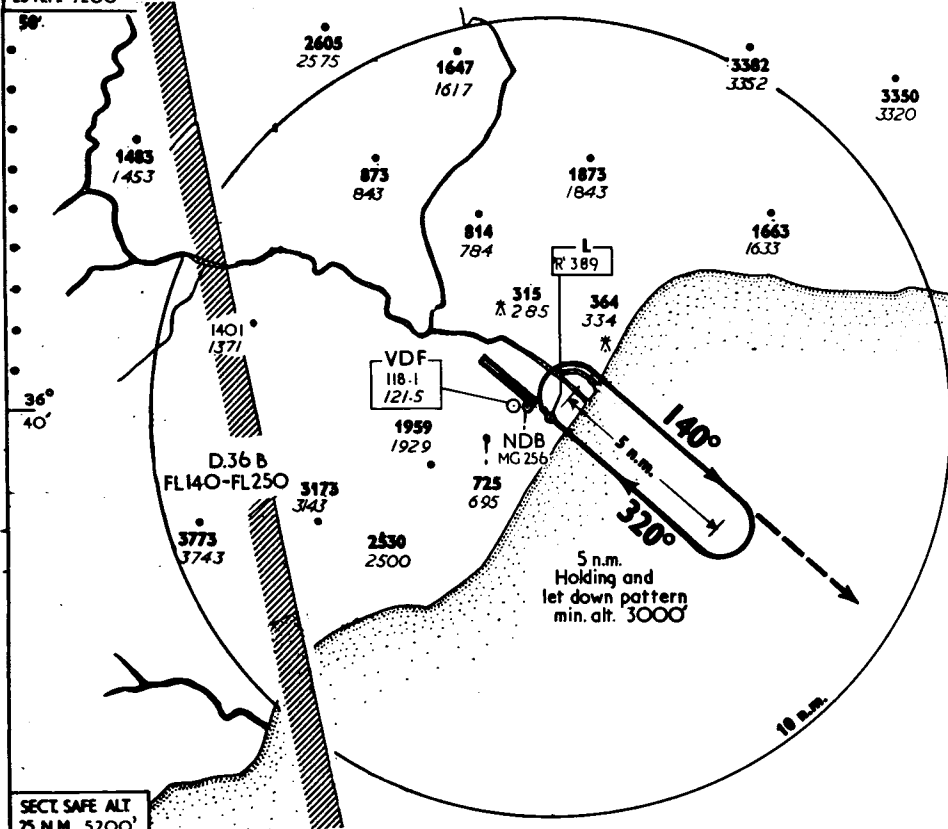
AERODROME CHART MALAGA

NOTE: R/W LIGHTS ARE 15M. FROM EDGE, $23\frac{1}{2}$ INS. AGL
T/W LIGHTS ARE 3M. FROM EDGE, $15\frac{1}{2}$ INS. AGL

CAUTION: STANDING WATER ON RUNWAY AFTER
HEAVY RAINFALL.

CIRCUITS: RIGHT HAND FOR R/W 32.

SECT. SAFE ALT.
25 N.M. 7200'



SECT. SAFE ALT.
25 N.M. 5200'

SECT. SAFE ALT.
25 N.M. 6200'

SECT. SAFE ALT.
25 N.M. 3600'

MALAGA

L/BCN (B.C.P.)

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH

TOWER

MALAGA

TWR

118.1

132.3

121.5 E

TRANS ALT

3000

FT.

G/S (KTS)	70	90	110	130	150
ABM'R TO TURN	4.17	3.20	2.44	2.19	2.00

G/S	170	190			
ABM'R TO TURN	1.46	1.35			

OVERSHOOT

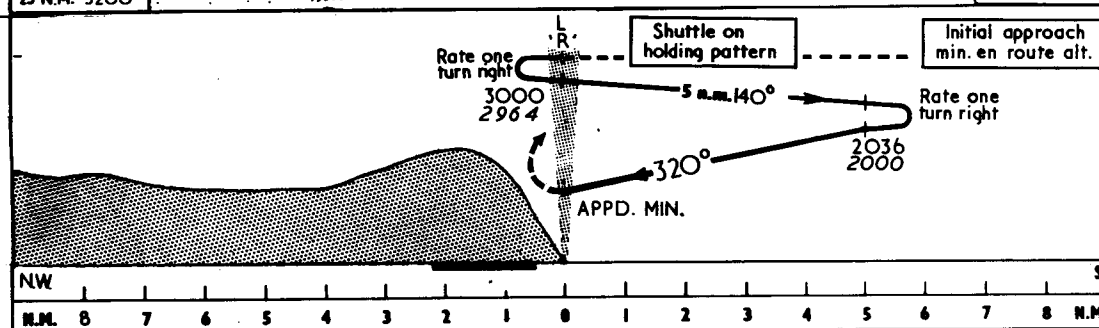
CLIMBING TURN RIGHT ONTO 140° TO 3000
2964

ELEVATIONS (FT):—

OFFICIAL AERODROME	53
LOWEST THRESHOLD	30

VARIATION 7°W | Bearings all magnetic
SERIAL No. 50257/4 | DATE: 9.6.65.
Reason for Re-Issue: ELEVATION & VARIATION.

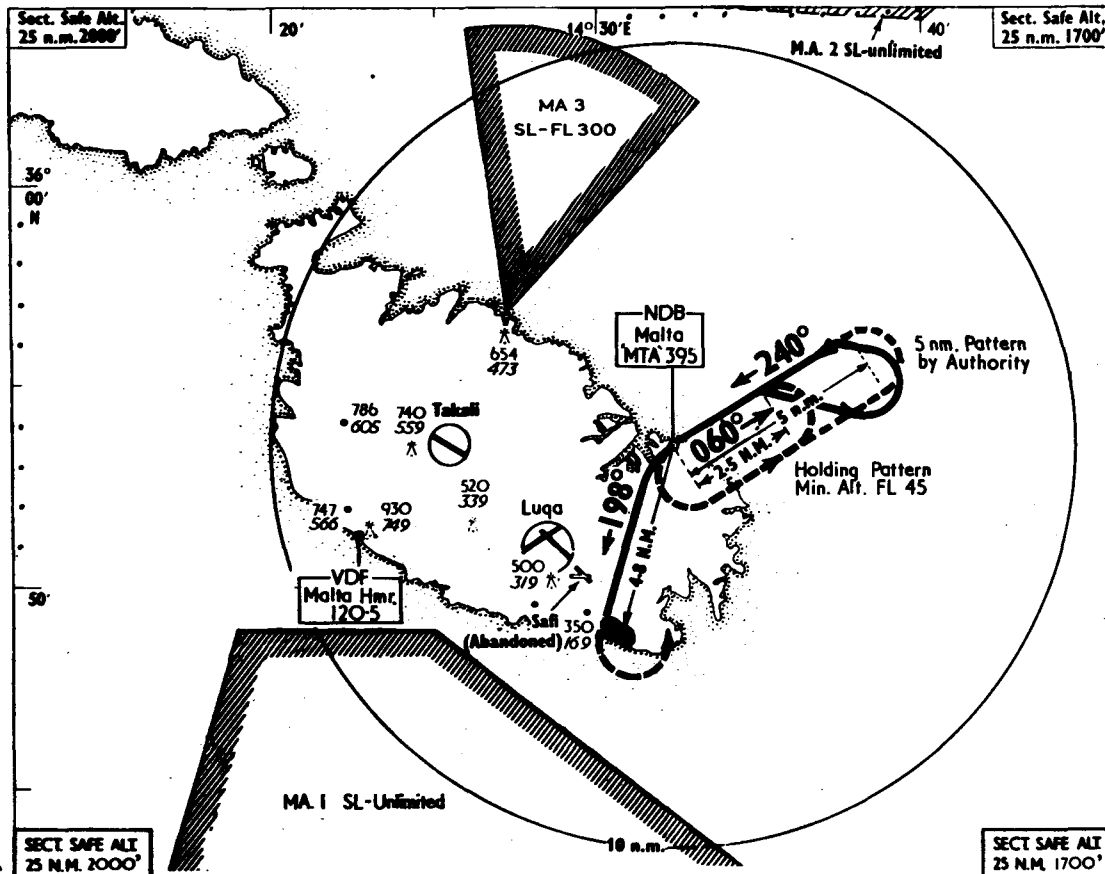
L/BCN (B.C.P.)
MALAGA



NW

SE

N.M. 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 N.M.



MALTA (HALFAR) NDB 'MT'(BCP)

BRITISH EUROPEAN AIRWAYS

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APPROACH MALTA APP.

119.1 120.5 121.5 E

TOWER HALFAR TWR

118.1 121.5 E

TRANS ALT 2500 FT.

G/S(KTS)	70	90	110	130	150
NDB TO TURN	2.09	1.40	1.22	1.09	1.0
NDB TO ADME	4.07	3.12	2.37	2.13	1.55

G/S	170	190	210	230	250
NDB TO TURN	0.53	0.47			
NDB TO ADME	1.42	1.31			

OVERSHOOT

TURN LEFT & RETURN TO NDB
AT 1800 1619

ELEVATIONS (FT):—

OFFICIAL AERODROME	230
LOWEST THRESHOLD	181

VARIATION 2°W | Bearings all magnetic

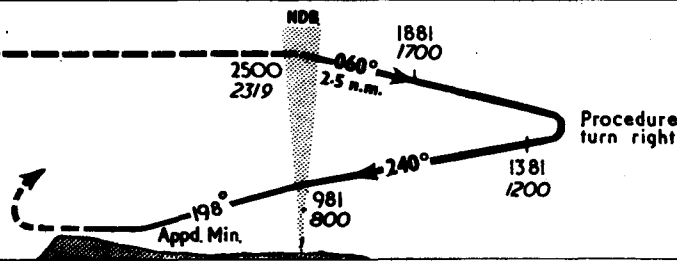
SERIAL No. 50370/1 | DATE: 7.7.65

Reason for Re-issue:

NDB IDENT.

NDB 'MT'(BCP)
MALTA (HALFAR)

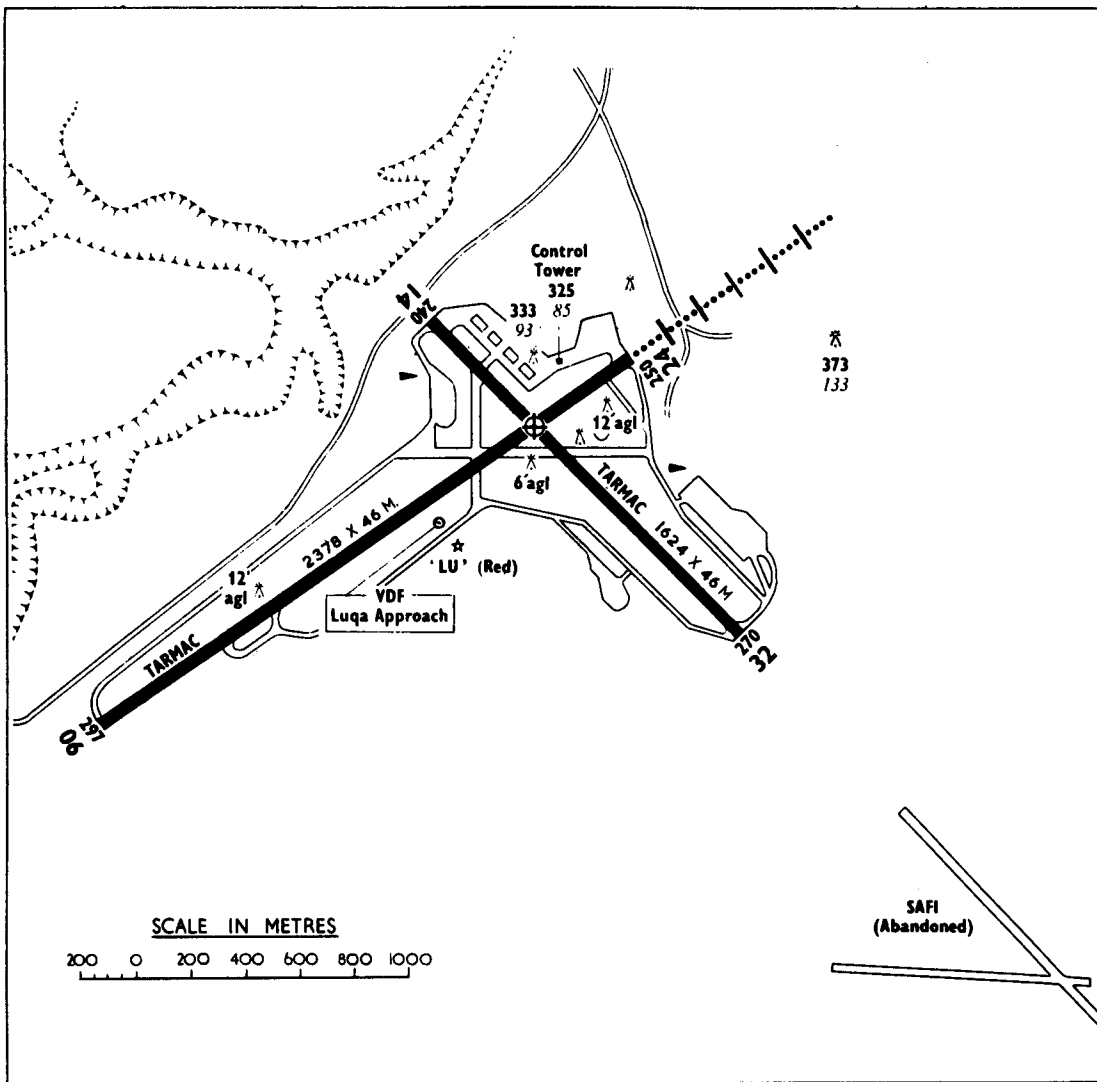
INITIAL APPROACH
25 nm. 2500 2319



SSW

N.M. 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 N.M.

NNE



MALTA (LUQA) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06	H.I. WHITE CENTRE-LINE (460M.) WITH 2 CROSS-BARS, L.I. RED 'T'.	3°
24	H.I. WHITE CENTRE-LINE (920M.) WITH 5 BARS L.I. RED 'T'.	3°
14/32	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06	GREEN PLUS SODIUMS	OMNIDIRECTION VAR. INT. WHITE PLUS SODIUMS
24	GREEN PLUS SODIUMS	OMNIDIRECTION VAR. INT. WHITE PLUS SODIUMS
14/32	GREEN PLUS SODIUMS	OMNI-DIRECTIONAL WHITE PLUS SODIUMS

OBSTRUCTION, TAXIWAY, FLOODLIGHTS,
LANDING INDICATOR IDENT BEACON 'LU' (RED),
ANGLE OF APPROACH R/W 14/32.

ELEVATIONS (FT) :-

OFFICIAL AERODROME	300
LOWEST THRESHOLD (14)	240
LAT 35° 52' N, LONG 14° 29' E	
VARIATION 2° W Bearings all magnetic	
SERIAL No. 50010/1	DATE : 5.1.65

Reason for Re-issue:

SCALE

AERODROME CHART MALTA (LUQA)

STOPWAYS: R/W 06-192m. R/W 14-107m. R/W 24-183m.

NOTE: JET BARRIER 20M. FROM END OF R/Ws 06 & 24. HEIGHT WHEN LOWERED: 2 FT AT 6M. AND 8 INS. AT 1M. OUTSIDE
EXTENDED R/W EDGES. AERODROME CIRCUITS: R/W 24 AND 32:- RIGHT HAND. R/W 06 AND 14:- LEFT HAND.

NOTE: PILOTS ARE WARNED THAT A/C WILL BE PARKED AT A DISTANCE OF 69m. FROM BOTH RUNWAYS. EXTREME CARE ON LANDING
AT ALL TIMES. LANDING ON R/W 14 - TURBULENCE CAN BE EXPECTED WHEN WIND ABOVE 20 KTS.

WARNING: RADIO SONDE ASCENTS 0530, 1130, 1730 & 2330

R/W AVAILABILITY

R/W 14L/32R T/O & Landing 2100m during VMC; 2928m for Jet A/c & on 30mins. notice for other A/c (* measured from 32R threshold.)
 R/W 32L Landing 1900m. first 100m. not available.
 R/W 14R/32L Available only on TWR instructions.

MARSEILLES (MARIGNANE) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
 French
 Type
 Angle

R/W	APPROACH	
14L	Nil	X
32R	Nil	
14R/ 32L	Nil	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
14L	L.I. & H.I. Green wing bars	H.I. Uni-dir. yellow, white wings at 305m L.I. Omni-dir. white
32R	Green H.I. uni-dir. & L.I. omni-dir.	L.I. omni-dir. White with last 315m Yellow. L.I. uni-dir. wing bars at 305m up R/W.
14R	H.I. & L.I. omni-dir. Green wing bars.	L.I. omni-dir. with H.I. uni-dir. sodium on North side
32L	L.I. uni-dir. green.	

OBSTRUCTION, TAXIWAY, IDENT. BEACON 'Q' WHITE. WIND & LANDING INDICATOR. UNI-DIRECTIONAL, RED BAR BEFORE THRESHOLD 14L.

ELEVATIONS (FT)	a.m.s.l.	174
	above lowest T/H	167

OFFICIAL AERODROME 66

LOWEST T/H (14L & 14R) 7

LAT 43° 26' N, LONG 05° 13' E

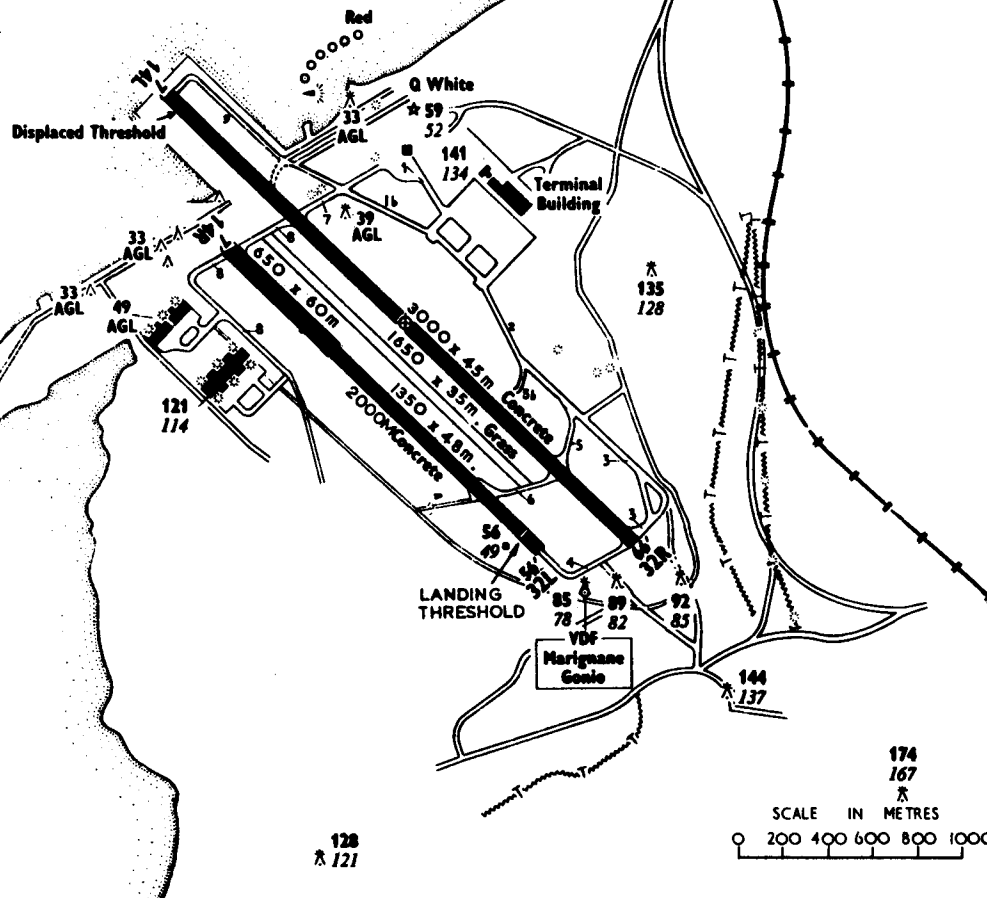
VARIATION 4° W Bearings all magnetic

SERIAL No. 40228/2 DATE: 27.10.64

Reason for Re-issue:

R/W Availability.

AERODROME CHART MARSEILLES (MARIGNANE)



STOPWAYS: R/W 14L 100m, R/W 32 200m.

SECT. SAFE ALT.
25 N.M. 3400

SALON DE
PROVENCE

050°10'E

SECT. SAFE ALT.
25 N.M. 4800

MARSEILLES (MARIGNANE)

ILS R/W 14L

BRITISH EUROPEAN AIRWAYS

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APPROACH MARIGNANE APP

120-2 119-7

TOWER MARIGNANE AIRPORT

118-1 119-7

TRANS ALT 2500 FT.

G/S (KTS)	70	90	110	130	150
OM TO TURN	1 m 43 s	1 m 20 s	1 m 06 s	0 m 55 s	0 m 48 s
OM TO T/H	3 m 34 s	2 m 46 s	2 m 16 s	1 m 55 s	1 m 40 s
DESCENT ON G/P (FT/MIN)	309	398	486	575	663
G/S	170	190	210	230	250
OM TO TURN	0 m 42 s	0 m 38 s	0 m 34 s	0 m 31 s	0 m 29 s
OM TO T/H	1 m 28 s	1 m 19 s	1 m 12 s	m s	m s
DESCENT ON G/P (FT/MIN)	751	840	930		

CRITICAL HEIGHTS (FT)

TRIDENT AUTO 400 MANUAL 400

OTHER TYPES WITH G/P 400 NO G/P 400

OVERSHOOT

RIGHT CLIMBING TURN TO 1483 / 476
TO RNG

ELEVATIONS (FT) a.m.s.l. 2500
above lowest T/H 2493

OFFICIAL AERODROME	66
LOWEST THRESHOLD	7
THRESHOLD R/W 14L	7
VARIATION 4°W	Bearings all magnetic
SERIAL No. 40229/2	DATE: 27. 10. 64
Reason for Re-issue:	
CRITICAL HEIGHTS. SAFE ALT.	

ILS R/W 14L

MARSEILLES (MARIGNANE)

WARNING
BACK BEAM NOT TO BE USED
NO MIDDLE MARKER

YELLOW

BLUE

RNG
MAR
383

OM

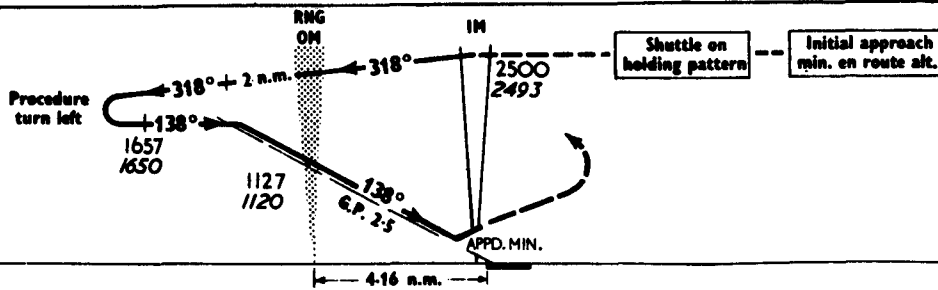
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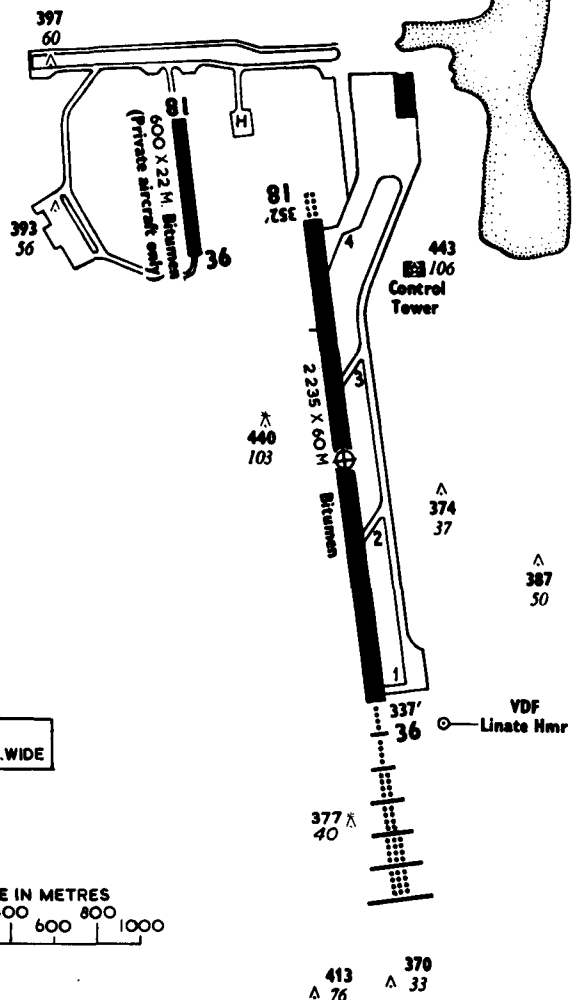
ILS
ML 110-3

Holding pattern
min. holding alt.
2500

SECT. SAFE ALT.
25 N.M. 3200

SECT. SAFE ALT.
25 N.M. 4800





MILAN (LINATE) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH
36	H.I. & L.I. WHITE CENTRE-LINE & BARS H.I. WHITE FLASHING LIGHTS ON OUTER - MOST 610M.
18	LEAD-IN LIGHTS

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
36 18	GREEN	WHITE & YELLOW

OBSTRUCTION, TAXIWAY, ADME BEACON,
WIND INDICATOR, APRON LIGHTS, ANGLE OF
APPROACH LIGHTS.

ELEVATIONS (FT) a.m.s.l. 443
above lowest T/H 106

OFFICIAL AERODROME 352

LOWEST THRESHOLD (36) 337

LAT 45° 27' N, LONG 09° 17' E

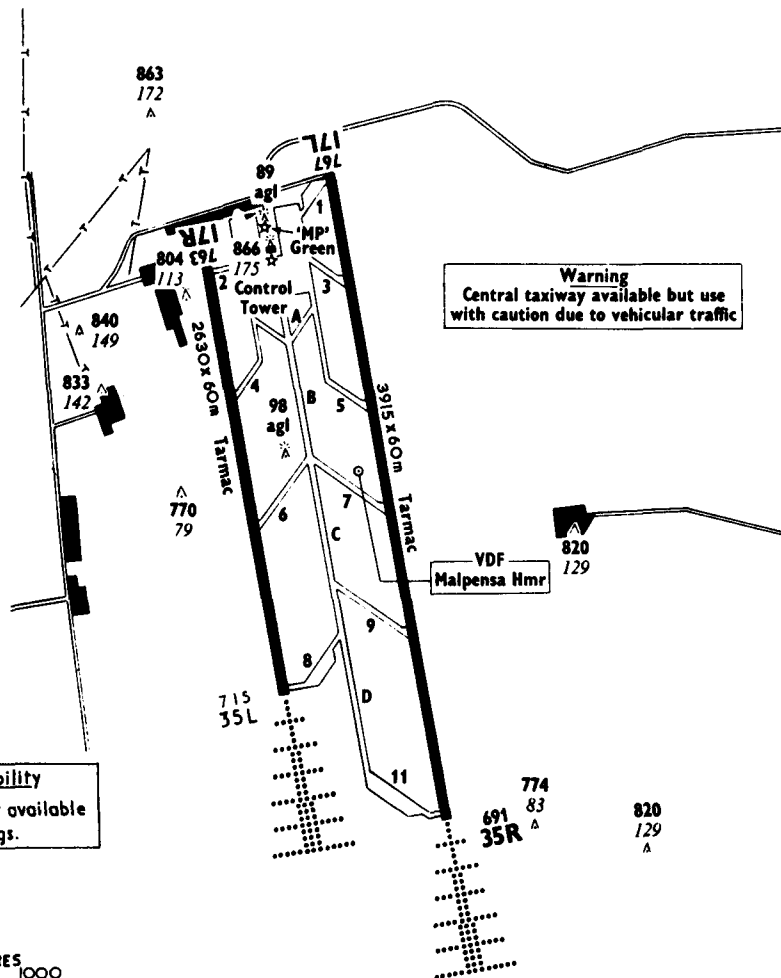
VARIATION 3° W Bearings all magnetic

SERIAL No. 32372 DATE: 5-8-64

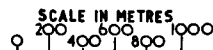
Reason for Re-Issue:

AERODROME CHART
MILAN (LINATE)

CIRCUITS: MAIN R/W 36 RIGHT HAND. LIGHT A/C R/W 18 RIGHT HAND.
STOPWAYS: R/W'S 18 & 36 - 60M.



R/W Availability
R/W 17R/35L not available
for night landings.



STOPWAYS: 60m ALL RUNWAYS

MILAN (MALPENSA) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
35R 35L	C/L & BARS WITH H.I. FLASHING WHITE C/L	
17L 17R	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
35R	GREEN	WHITE, LAST 610M. WHITE & YELLOW
35L 17L 17R	GREEN	WHITE, LAST 610M. WHITE & YELLOW

OBSTRUCTION, TAXIWAY, ADME BEACON,
WIND & LANDING INDICATOR, FLOODLIGHT, IDENT
BEACON 'MAL' (GREEN)

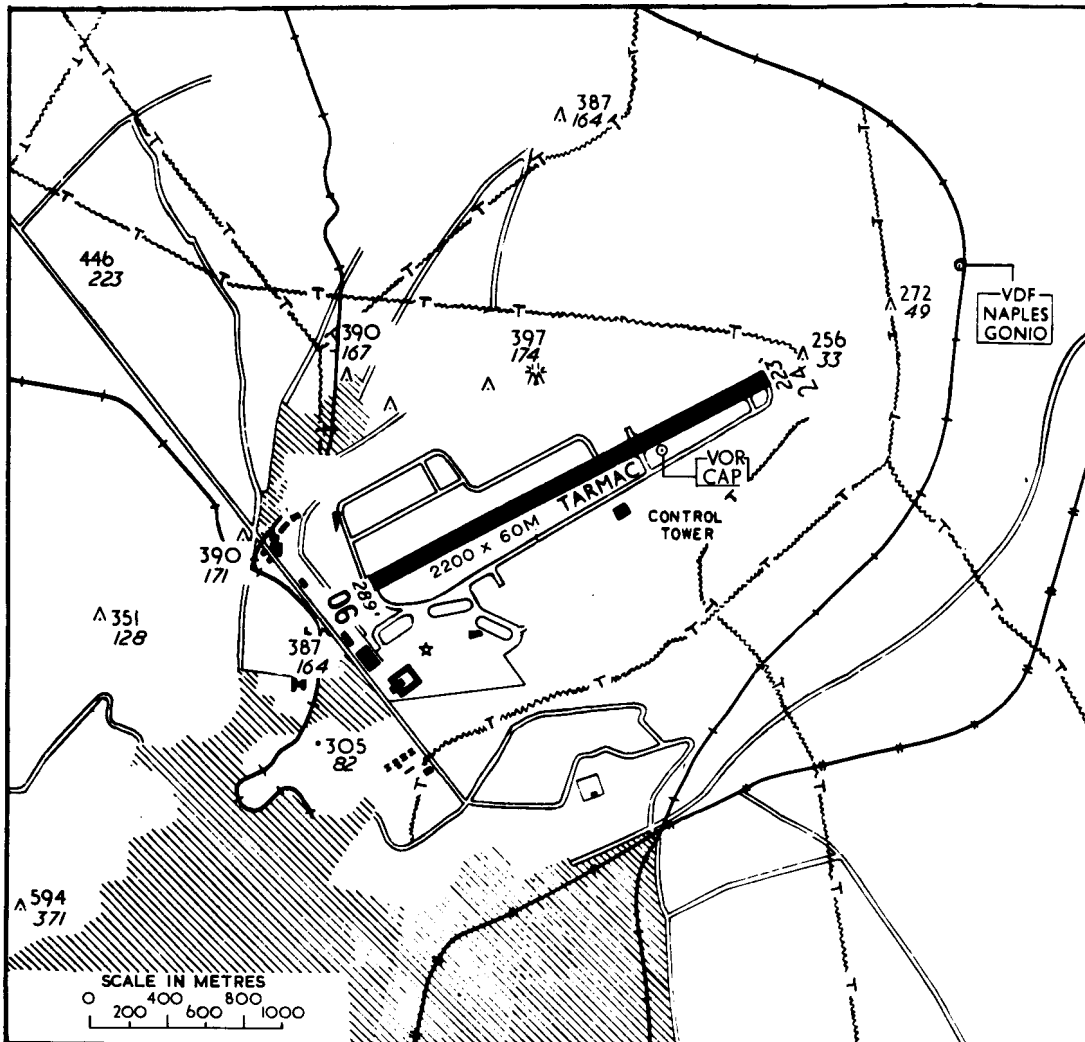
ELEVATIONS (FT) :—

OFFICIAL AERODROME	767
LOWEST THRESHOLD (35R)	691
LAT 45° 38' N, LONG 08° 44' E	
VARIATION 3° W	Bearings all magnetic
SERIAL No. 50150	DATE: 11.3.65

Reason for Re-issue:

IDENT. BEACON

AERODROME CHART MILAN (MALPENSA)



NAPLES (CAPODICHINO) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
06.24	NIL	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
06.24	GREEN	WHITE & YELLOW

OBSTRUCTION, TAXIWAY, ADME BEACON
FLOODLIGHTS

ELEVATIONS (FT).—

OFFICIAL AERODROME 289

LOWEST THRESHOLD (24) 223

LAT 40° 53' N, LONG 14° 17' E

VARIATION 2° W Bearings all magnetic

SERIAL No 50480/3 DATE 12.8.65

Reason for Re-issue:

OBSTRUCTIONS

STOPWAYS: R/W 06 250M., R/W 24 210M.

AERODROME CHART
NAPLES (CAPODICHINO)

NAPLES CONTROL ZONE RADAR SERVICE

NAPLES

A radar service is provided by the local GCA in order to:-

- a Assist aircraft during the ILS or VOR approach procedures to R/W 24.
- b Control aircraft during complete GCA procedure for R/W 24.

The service, in English is provided as follows:-

1 Hours of service

- 1.1. Continuous, when ceiling is lower than 2000 ft. and/or the visibility is lower than 3000 M.
- 1.2. On pilots request 15 minutes prior notice in other cases.

- 2 Aircraft will be cleared to contact GCA frequency at the minimum holding altitude over Naples NDB, or, if no holding is expected, before reaching the NDB. Contact must be effected before the aircraft leaves the NDB.
- 3 GCA will confirm to the pilot the radar identification during the intermediate approach of the ILS or VOR procedure R/W 24.
- 4 GCA unit, after the aircraft report over the VOR, will follow the aircraft on the outbound leg and provide the pilot with range information from the point where the aircraft commences the base turn, at intervals of 1 N.M. Normally, due to the radar cone of silence which could extend to 2 N.M. from the VOR, the GCA unit will provide pilots with range information only when the aircraft is at 3 N.M. from the point where it is due to commence the base turn.
- 5 GCA unit will notify the pilot if the aircraft during the base turn is proceeding beyond 7 N.M. from the VOR.
- 6 GCA unit will notify the pilot when, during the final phase of the ILS procedure, the aircraft is too far above or below the glide path and/or too far left or right of the localiser centre-line; or, if during the final phase of the VOR procedure, the aircraft is too far left or right of the approach radial.

BRITISH EUROPEAN AIRWAYS BEA

Reason for Re-Issue:

REVISED PROCEDURE

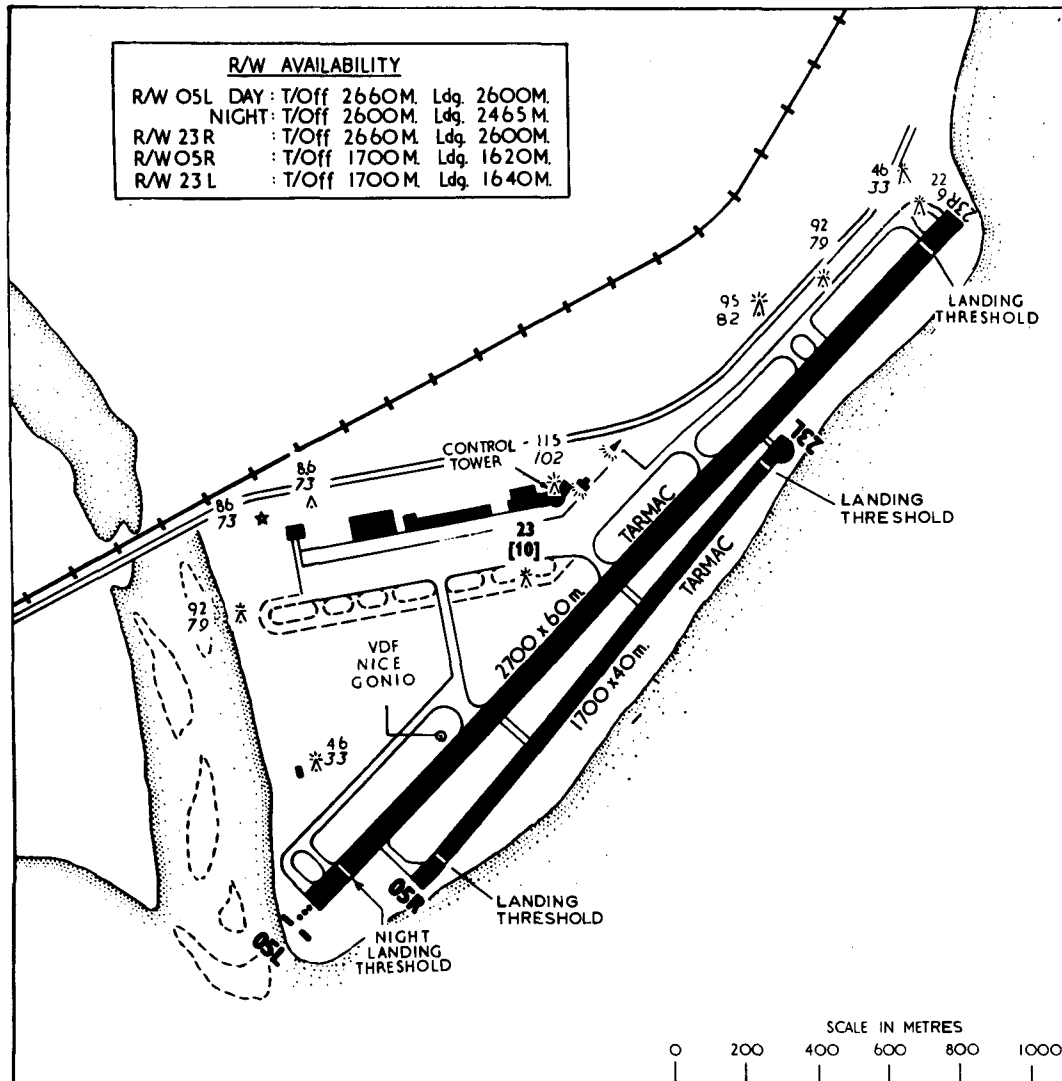
SERIAL No. 50480/1

DATE: 10 8 65

NAPLES

R/W AVAILABILITY

R/W 05L DAY: T/Off 2660M. Ldg. 2600M.
 NIGHT: T/Off 2600M. Ldg. 2465M.
 R/W 23R : T/Off 2660M. Ldg. 2600M.
 R/W 05R : T/Off 1700M. Ldg. 1620M.
 R/W 23L : T/Off 1700M. Ldg. 1640M.



NOTES: 1. OVERFLYING TOWN OF NICE PROHIBITED.
 2. RIGHT HAND CIRCUITS R/W 05.
 3. JET A/C REQUESTED TO TAKE OFF R/W 23R. AFTER TAKE-OFF, TURN ONTO 155°R ON REACHING 1000FT, THEN CONTINUE CLIMB.

RUNWAY QDM'S
 05L/23R = 047°/227°
 05R/23L = 044°/224°

NICE AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
05L	RED CENTER-LINE & BAR	
23R	RED BAR	
05R	NIL	
23L		

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
05L	H.I. & L.I. GREEN WINGBARS 135M UP R/W.	ALTERNATE L.I. WHITE/H.I. SODIUM FOR 1450M. REMAINDER L.I. WHITE. WHITE WINGS 305 M. FROM LANDING THRESHOLD
23R	L.I. GREEN WINGBARS 100M. UP R/W	L.I. WHITE
05R	L.I. WINGS	L.I. WHITE
23L		

OBSTRUCTION, TAXIWAY, ADME BEACON
 WIND INDICATOR, LANDING INDICATOR
 WHITE WING BARS 305M FROM THRESHOLD
 R/W 23R, & FROM DISPLACED THRESHOLD
 R/W 05L

ELEVATIONS (FT):

OFFICIAL AERODROME 13

LAT 43° 40' N. LONG 07° 13'E

VARIATION 3°W / Bearings all magnetic

SERIAL No. 50310/2 DATE: 14.7.65

Reason for Re-Issue:
 R/W AVAILABILITY & OBSTRUCTIONS

AERODROME CHART NICE

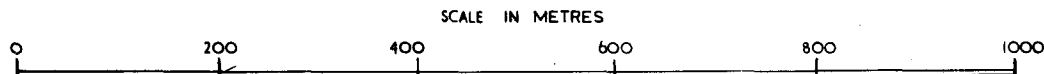
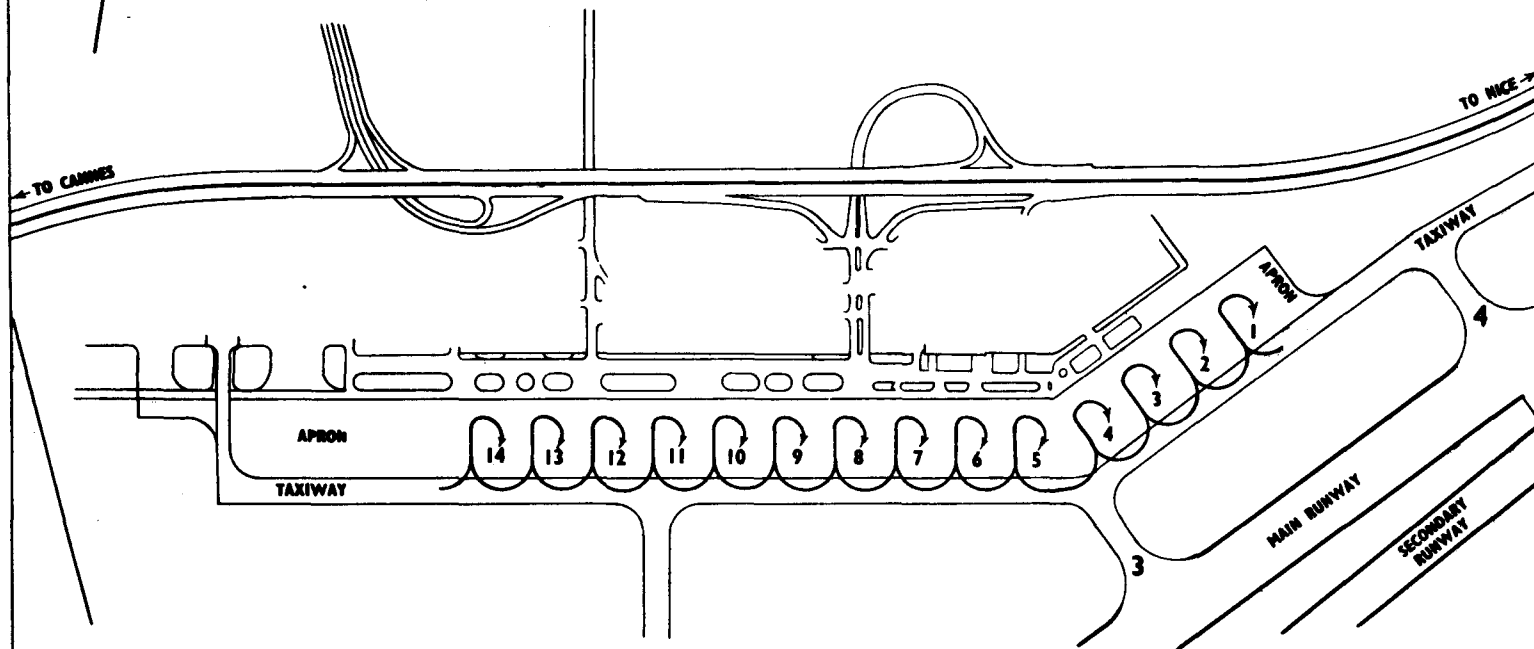


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NICE PARKING POSITIONS

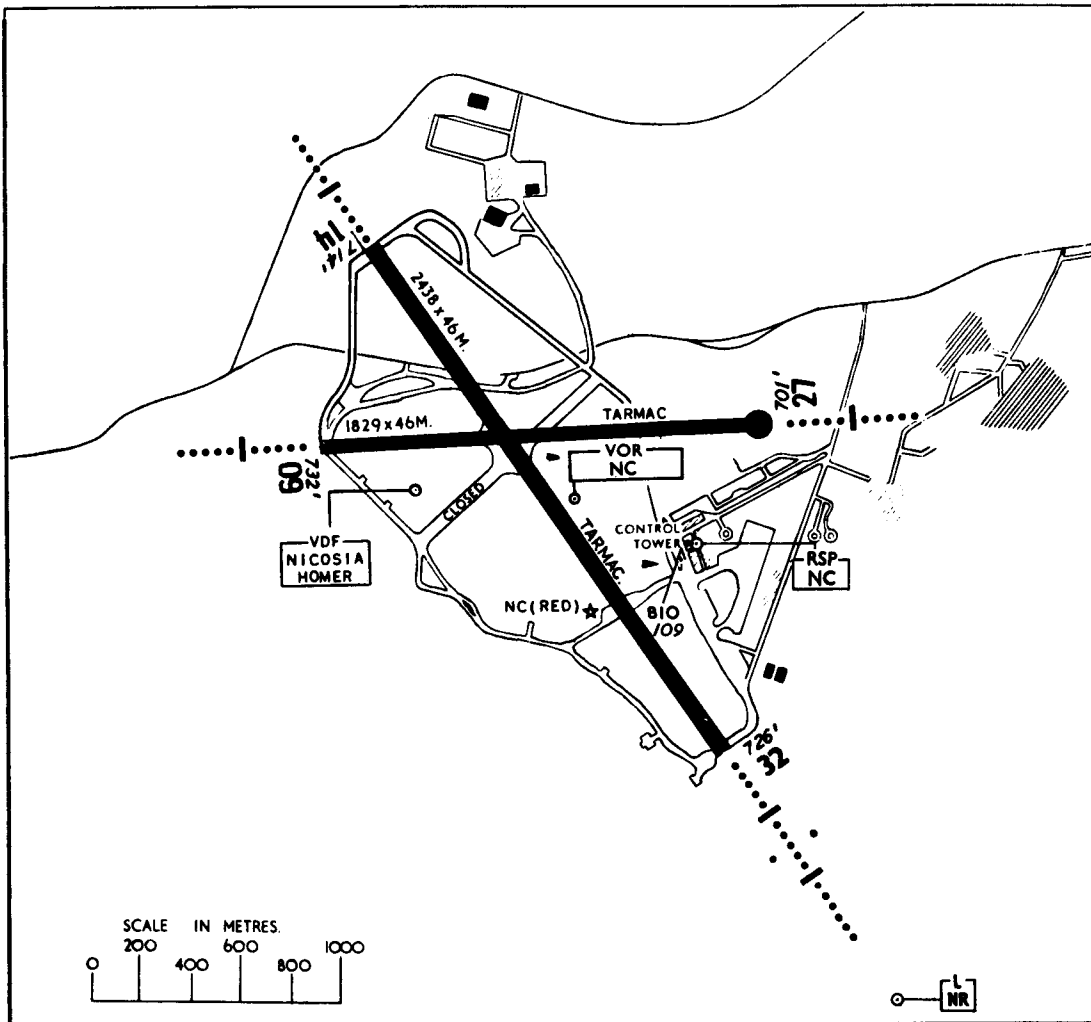


SERIAL No 40019

DATE 5.8.64

Reason for Re-Issue:

PARKING POSITIONS NICE



NICOSIA AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
09 / 27	RED CENTRE LINE WITH CROSS BAR	
14	RED CENTRE LINE WITH CROSS BAR	
32	RED CENTRE LINE WITH CROSS BAR	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
09 / 27	GREEN	ELEVATED WHITE
14	GREEN	ELEVATED WHITE
32	GREEN	ELEVATED WHITE (WITH SODIUM ON FIRST 3000 FT.)

OBSTRUCTION, TAXIWAY, IDENT BEACON,
(NC) RED, GLIDE PATH INDICATORS.

ELEVATIONS (FT) a.m.s.l. 810
above lowest T/H 709

OFFICIAL AERODROME 734

LOWEST THRESHOLD (27) 701

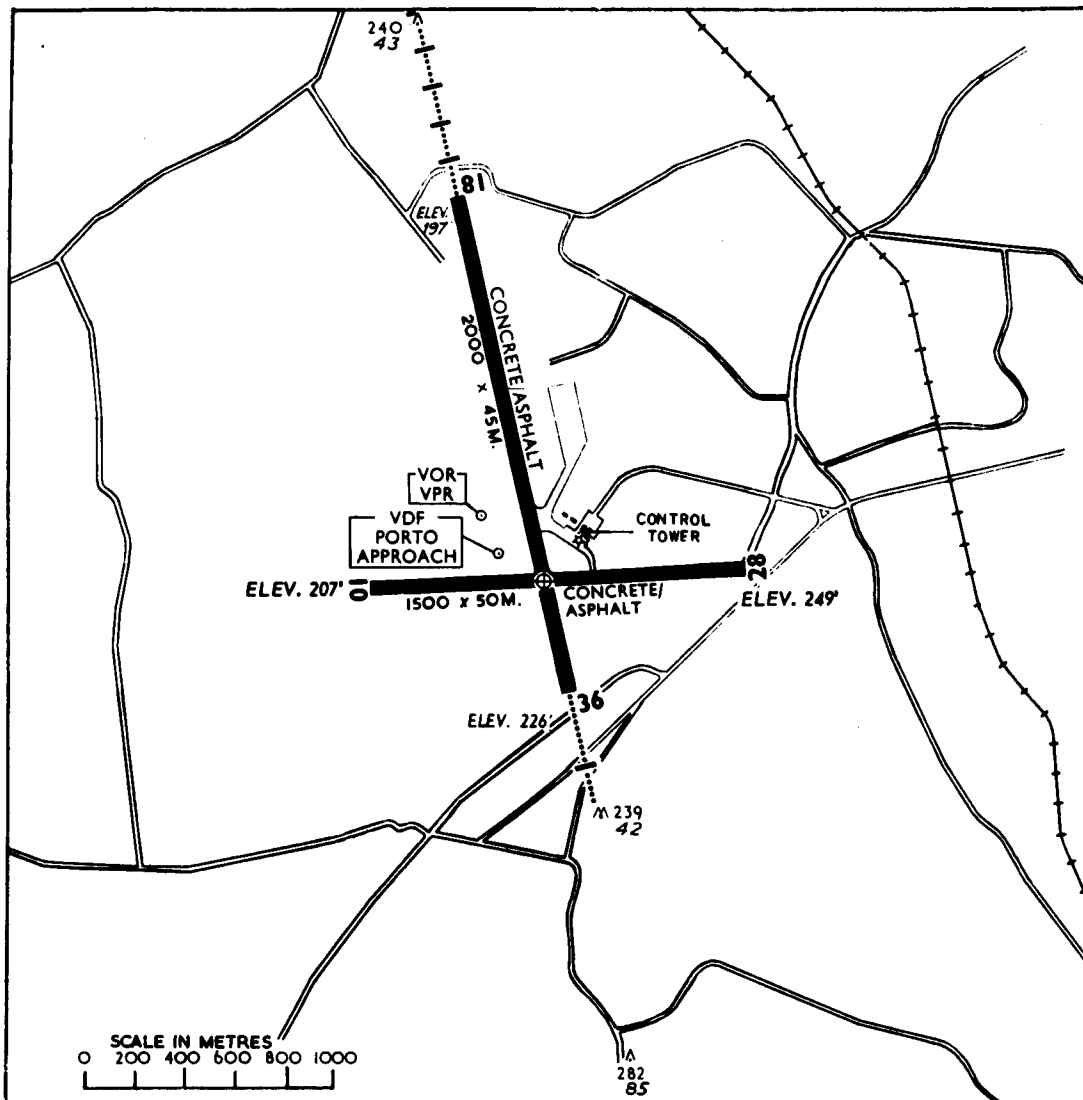
LAT 35° 09' N, LONG 33° 16'

VARIATION 2° E Bearings all magnetic

SERIAL No. 32/97. DATE: 28.7.64

Reason for Re-issue:

AERODROME CHART
NICOSIA



Oporto AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
18	H.I. UNI-DIRECTIONAL WHITE CENTRE-LINE & 5 BARS. L.I. OMNI-DIRECTIONAL RED CENTRE-LINE & BARS.	3°
36	L.I. OMNI-DIRECTIONAL RED CENTRE-LINE & BAR.	3°
10, 28	NIL	

NB. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
18 & 36	GREEN H.I. UNI & L.I. OMNI-DIR- ECTIONAL	H.I. WHITE BI-DIRECT- IONAL H.I. WHITE BI-DIRECTIONAL CENTRE-LINE L.I. OMNI-DIRECTIONAL
10, 28	GREEN L.I. OMNI-DIRECTIONAL	L.I. WHITE OMNI-DIRECT- IONAL

OBSTRUCTION, BLUE TAXIWAY, ADME BEACON
WIND AND LANDING INDICATOR, APRON
FLOODLIGHTS.

ELEVATIONS (FT): -

OFFICIAL AERODROME	249
LOWEST THRESHOLD (18)	197
LAT	41° 14' N, LONG 08° 41' W
VARIATION 10° W	Bearings all magnetic
SERIAL No. 40393/1	DATE: 8.1.65

Reason for Re-issue:

LIGHTING

AERODROME CHART Oporto

SECT. SAFE ALT
25 N.M. 4300'

SECT. SAFE ALT.
25 N.M. 4300'

OPORTO VOR (B.C.P)

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APPROACH PORTO APP.

121.1 3023.5

TOWER PORTO TWR.

118.1 3023.5

TRANS ALT 2500 FT.

G/S (KTS)	90	110	130	150
VOR TO TURN	4.10	3.25	2.53	2.30

G/S	170	190	210	230	250
VOR TO TURN	2.13	1.59	1.47	1.37	1.30

CRITICAL HEIGHTS (FT)

VAN/COMET	700
VISCOUNT	750

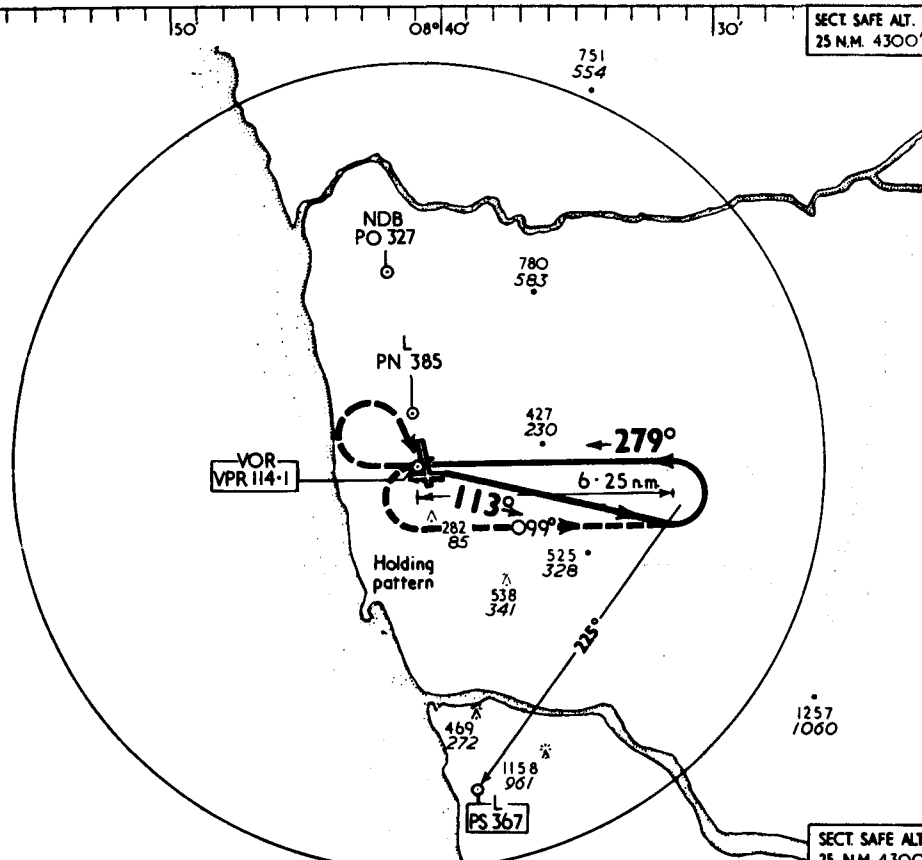
OVERSHOOT

CLIMB AHEAD, TURN RIGHT AND RETURN TO
VOR AT 2750 2553

ELEVATIONS (FT): —

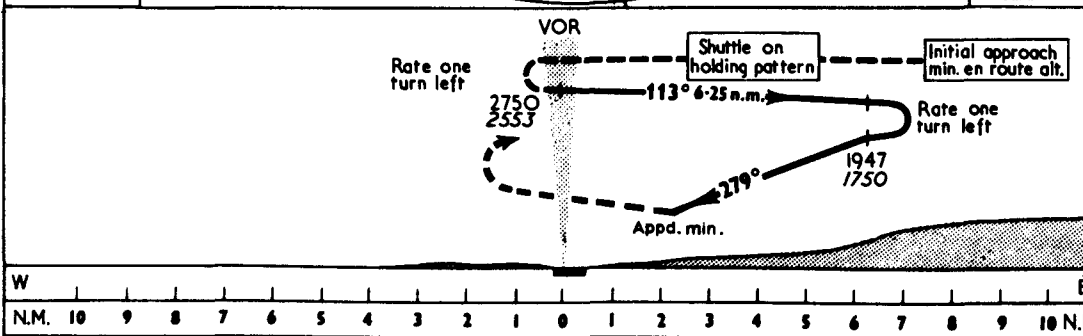
OFFICIAL AERODROME	249
LOWEST THRESHOLD	197
THRESHOLD R/W	
VARIATION 10°W	Bearings all magnetic
SERIAL No. 38079/1	DATE: 8.1.65
Reason for Re-Issue:	
PRESSENTATION	

VOR (B.C.P)
OPORTO



SECT. SAFE ALT
25 N.M. 2200'

SECT. SAFE ALT
25 N.M. 4300'



PALERMO (PUNTA RAISI) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
07	NIL	
25	RED CENTRE-LINE & BARS	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
07	GREEN AND RED	WHITE AND YELLOW
25		

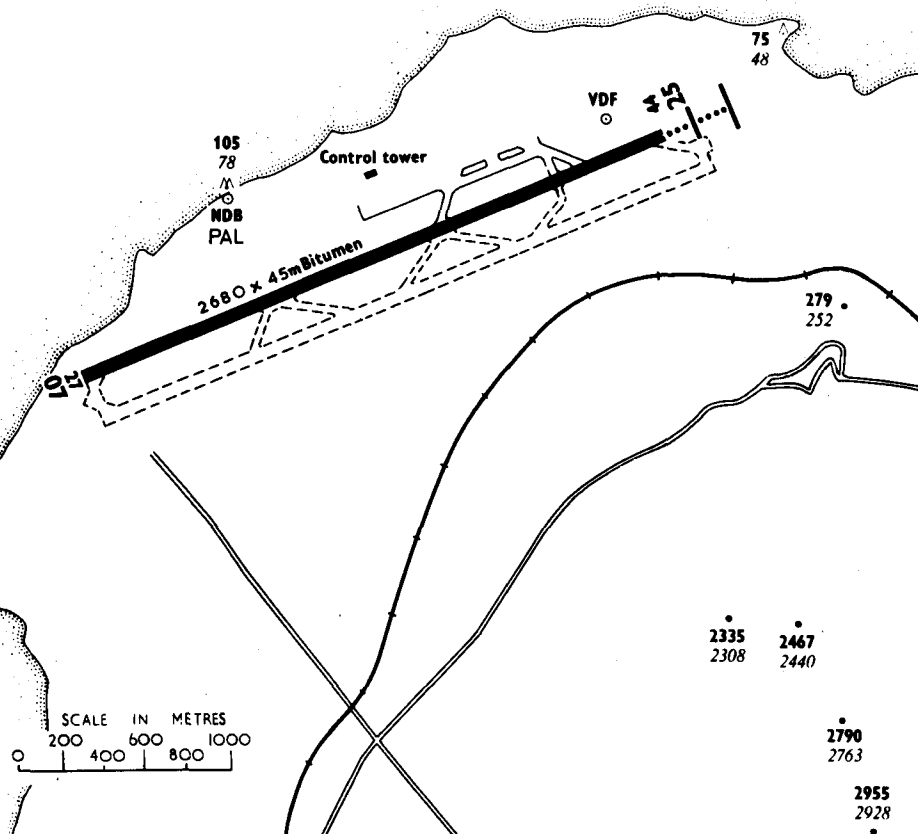
OBSTRUCTION, TAXIWAY, ADME BEACON, IDENT.
BEACON 'Q' (GREEN), WIND & LANDING
INDICATORS, FLOODLIGHTS.

ELEVATIONS (FT)	a.m.s.l.	105
	above lowest T/H	78
OFFICIAL AERODROME		55
LOWEST THRESHOLD (07)		27
LAT	38° 11' N.	LONG 13° 06' E
VARIATION	2° W	Bearings all magnetic
SERIAL No.	40170/1	DATE 13.10.64

Reason for Re-issue: NDB IDENT

EFFECTIVE 22.10.64

AERODROME CHART
PALERMO (PUNTA RAISI)



PALERMO

LOCAL WIND EFFECTS

1. B.E.A. CROSS WIND LIMITS FOR LANDING AT PALERMO

Landings are not permitted at Palermo if winds reported are above the limits given below:-

<u>WIND DIRECTION</u> <u>FROM (MAGNETIC)</u>	<u>WIND SPEED</u> <u>KNOTS</u>
090°	37
100°	26
110°	20
120°	17
130°	15
140°	14
150°	13
160°	13
170°	13
180°	14
190°	15
200°	17
210°	20
220°	26
230°	37
240° through North to 080° (inc)	Normal cross wind limits apply.

2. B.E.A. CROSS WIND LIMITS FOR TAKE-OFF

For any reported wind direction, take offs are not to be carried out if the wind speed is more than 5 knots greater than the maximum value permitted for landing.

3. TURBULENCE

Beware turbulence when landing - particularly on R/W 25.

BRITISH EUROPEAN AIRWAYS

BEA

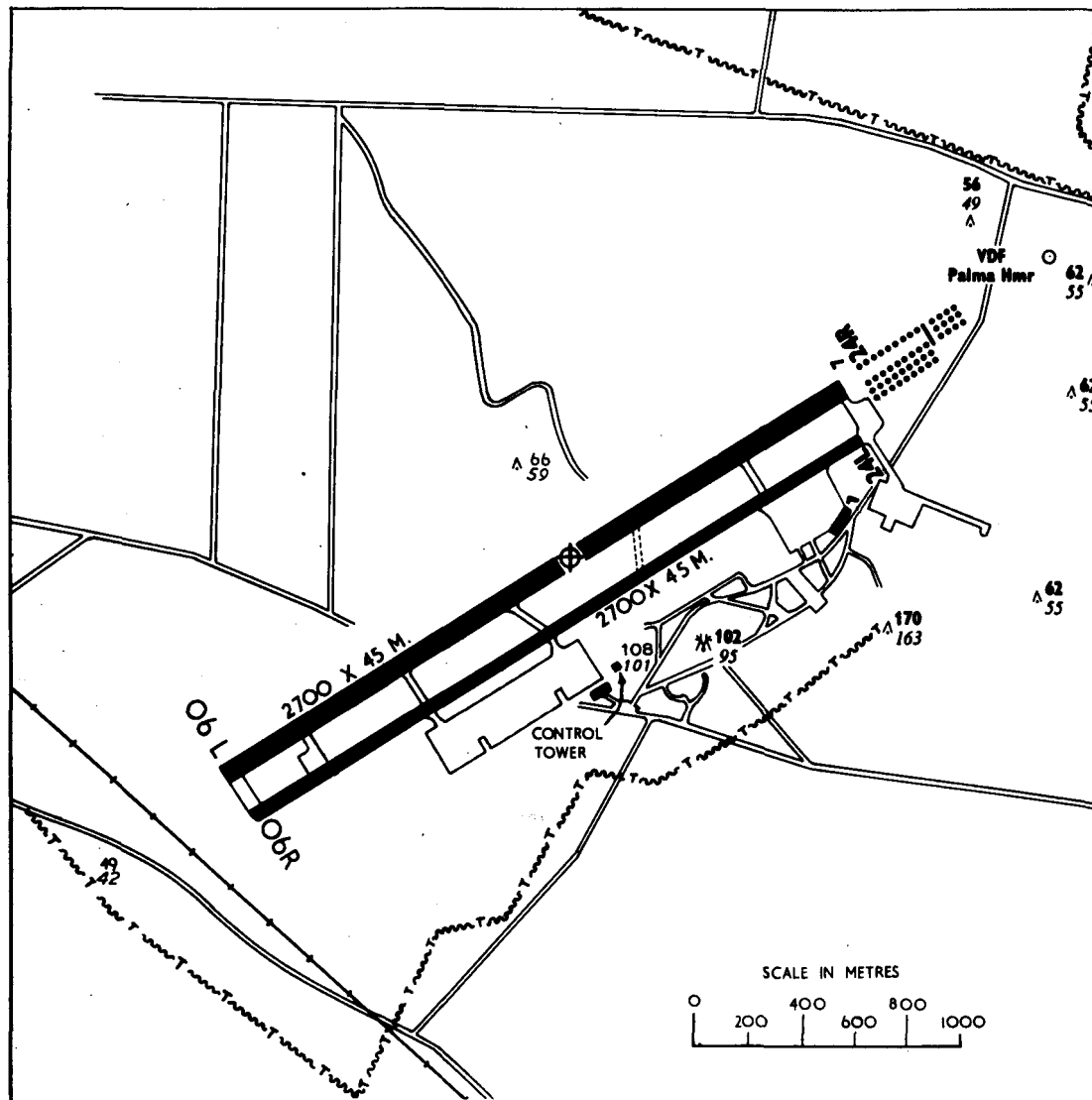
Reason for Re-issue:

SERIAL No. 40055

DATE: 25.8.64.

PALERMO

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Dept. as part of the Operations Manual.



PALMA (SON SAN JUAN) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
24R	H.I. AMBER	
06L	NIL	
06R/ 24L	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
24R 06L	H.I. GREEN WINGBARS	H.I. (VARIABLE) WHITE WITH CENTRE LINE. LAST 600 M YELLOW
06R/ 24L	MED. INT. GREEN WINGBARS	MED. INT.

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND AND LANDING INDICATORS, STOPWAY
H.I. RED.

ELEVATIONS (FT):—

OFFICIAL AERODROME 13

LOWEST THRESHOLD 24R 7

LAT 39°33' N, LONG 02°44' E

VARIATION 4°W | Bearings all magnetic

SERIAL No. 50371/4 | DATE: 15.7.65.

Reason for Re-issue:

RUNWAY LENGTH

AERODROME CHART PALMA (SON SAN JUAN)

STOPWAYS: R/W 06L - 300M.

NOTE: R/W 06R/24L USEABLE AS TAXIWAY.

Sect Safe Alt
25nm 4000'

02 40E

50'

03 00

Sect Safe Alt
25nm 7000'

PALMA

ILS R/W 24

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APPROACH PALMA APP

120.7 119.4

TOWER SON SAN JUAN TWR

118.1 121.7 132.8

TRANS ALT 4000' FT.

G/S (KTS)	70	90	110	130	150
ARM LOM TO TURN			1.22	1.09	1.0
LOM TO T/H	3.37	2.49	2.18	1.57	1.42
DESCENT ON G/P (FT/MIN)	370	480	580	690	800
G/S	170	190	210	230	250
ARM LOM TO TURN	0.53	0.47	0.43	0.39	0.36
LOM TO TURN	1.30	1.20			
DESCENT ON G/P (FT/MIN)	990	1010			

MISSED APPROACH

Turn left climbing to 3000' home to
MJV VOR & hold

ELEVATIONS (FT) :-

OFFICIAL AERODROME	13
LOWEST THRESHOLD	7
THRESHOLD R/W 24	7
VARIATION 5° W	Bearings all magnetic
SERIAL No. 5037 / 1	DATE: 15.7.65

Reason for Re-issue:

MINOR

ILS R/W 24

PALMA

BLUE

YELLOW

39°
40N

2730
2723

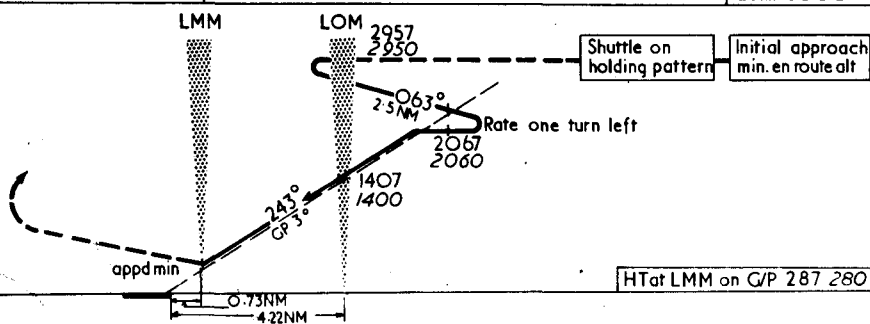
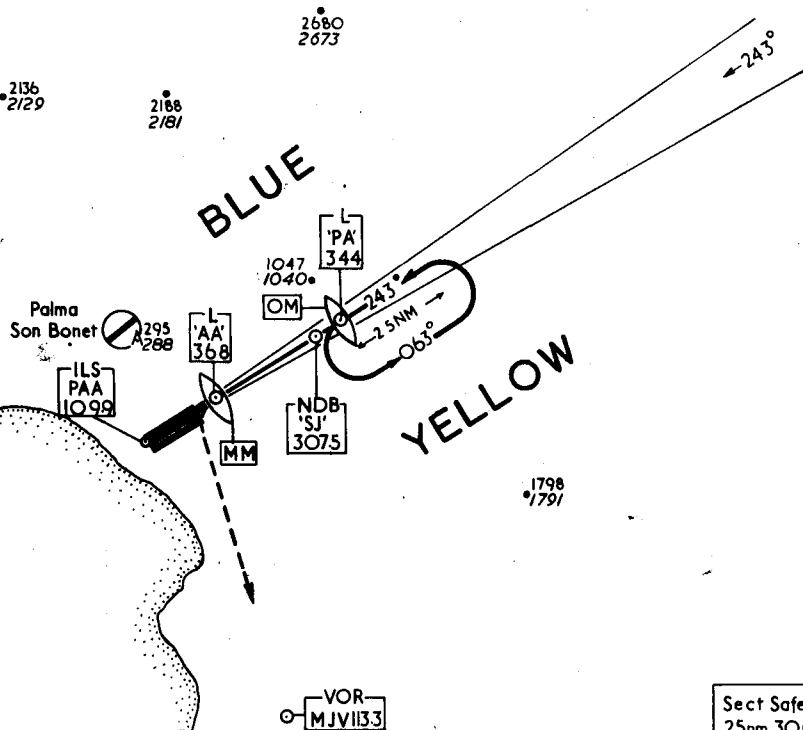
1591
1584

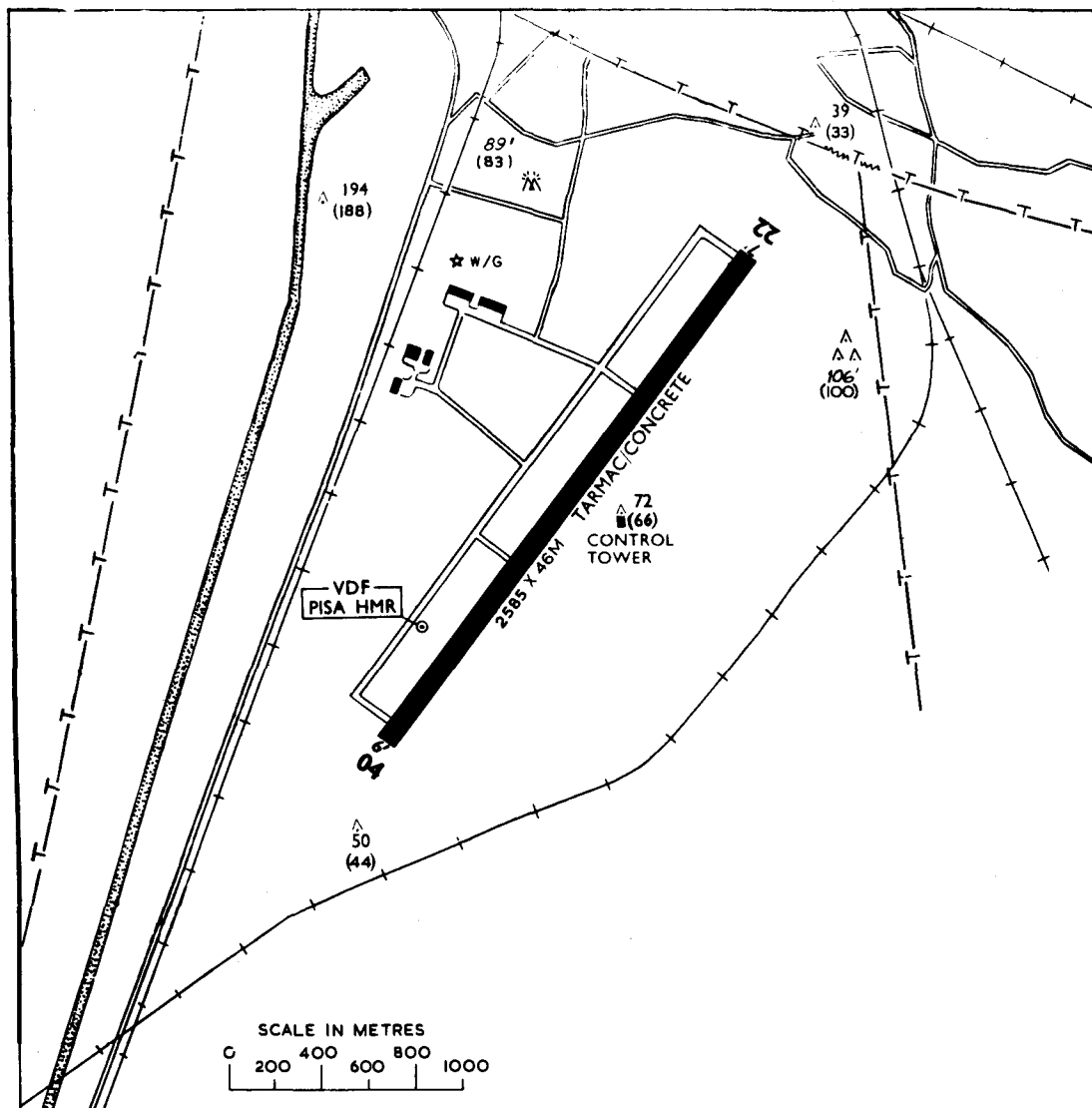
39°
50N

Sect Safe Alt
25nm 4300'

VOR
MJV 113.3

Sect Safe Alt
25nm 3000'





STOPWAY: R/W 04 149M. — R/W 22 255M.

NOTE: P.S.P. PARKING APRON SOUTH OF EXISTING PAVED APRON AVAILABLE FOR PISTON ENGINEED & TURBO-PROP A/C

PISA AERODROME CHART

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH
04	MODIFIED CALVERT
22	NIL

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
04	GREEN	YELLOW
22		

OBSTRUCTION, TAXIWAY, ADME BEACON
LANDING INDICATOR, ANGLE OF APPROACH
LIGHTS R/W 04/22.

ELEVATIONS (FT) a.m.s.l. 194
above lowest T/H 188

OFFICIAL AERODROME 8'

LOWEST THRESHOLD (04) 6'

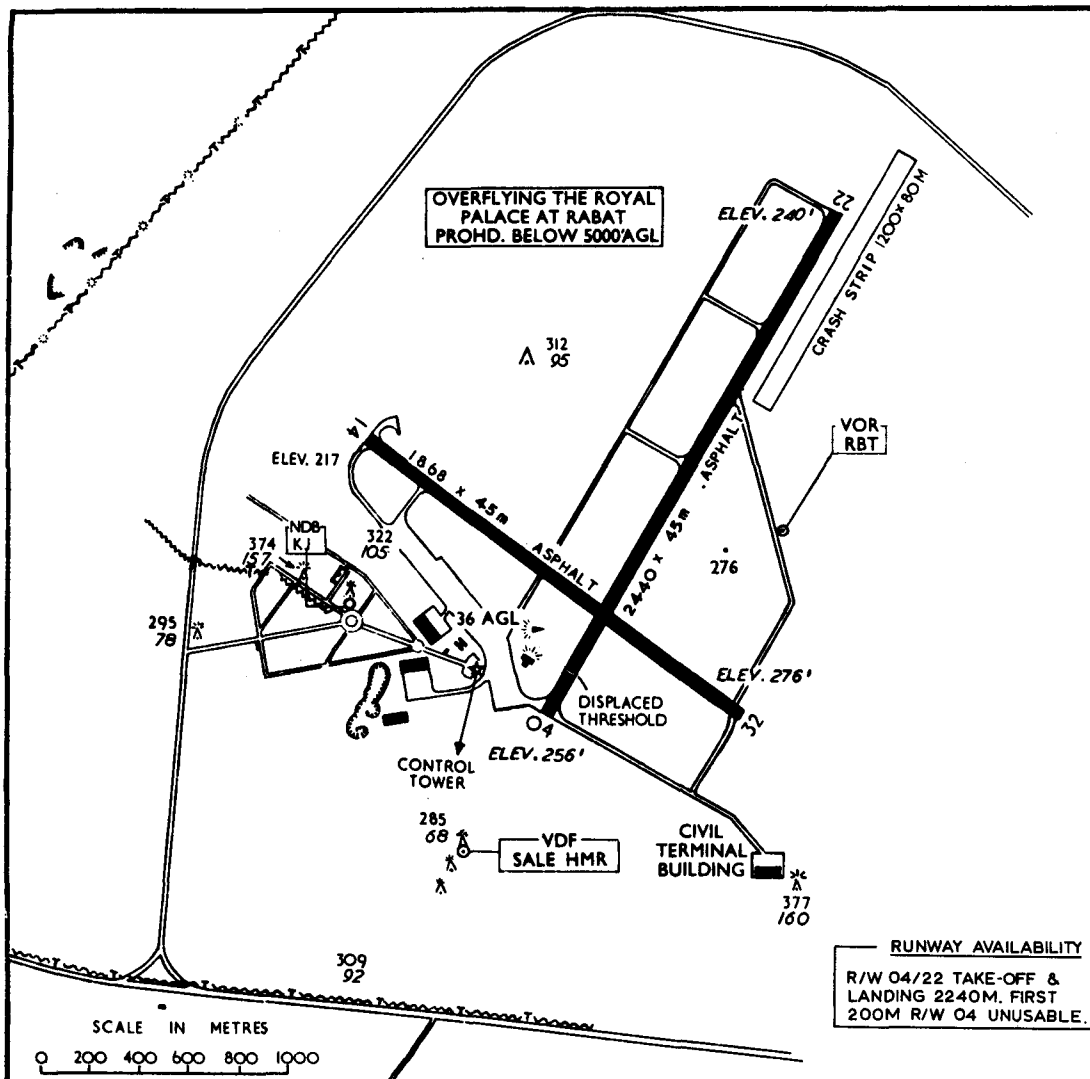
LAT 43° 41' N, LONG 10° 24' E

VARIATION 2° W Bearings all magnetic

SERIAL No. 48231 DATE: 5.8.64

Reason for Re-issue:

AERODROME CHART
PISA



RUNWAY AVAILABILITY
R/W 04/22 TAKE-OFF & LANDING 2240M. FIRST 200M R/W 04 UNUSABLE.

RABAT (SALE) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
04/22 14/32	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
04/22 14/32	YES(ELEVATED 20")	YES(ELEVATED 20")

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND AND LANDING INDICATOR

ELEVATIONS (FT)

OFFICIAL AERODROME	276
LOWEST THRESHOLD (14)	217
LAT	34° 03' N, LONG 06° 45' W
VARIATION	9° W Bearings all magnetic
SERIAL No.	40107/1
DATE	5.11.64

Reason for Re-issue:
ELEVATIONS & OBSTRUCTIONS.

AERODROME CHART RABAT (SALE)

STOPWAYS: R/W's 04/22 - 180m.

SECT. SAFE ALT.
25 N.M. 2000'

06° 50' W

40'

SECT. SAFE ALT.
25 N.M. 3000'

RABAT (SALE)

NDB. (BCP)

BRITISH EUROPEAN AIRWAYS

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APPROACH

TOWER

SALE AIRPORT

118.3 119.7

TRANS ALT

2100

FT.

G/S (KTS)	70	90	110	130	150
NDB TO TURN	m s	m s	4 m s	3 m s	3 m s
	m s	m s	m s	m s	m s

G/S	170	190	210	230	250
NDB TO TURN	2 m s	2 m s	2 m s	1 m s	1 m s
	m s	m s	m s	m s	m s

CRITICAL HEIGHT (FT)

1150

OVERSHOOT

TURN RIGHT ONTO 220° CLIMBING TO 2250
2033 ON CONTROL INSTRUCTIONS.

ELEVATIONS (FT)

a.m.s.l. 2300
above lowest T/H 2074

OFFICIAL AERODROME

276.

LOWEST THRESHOLD

217

VARIATION 8° W | Bearings all magnetic

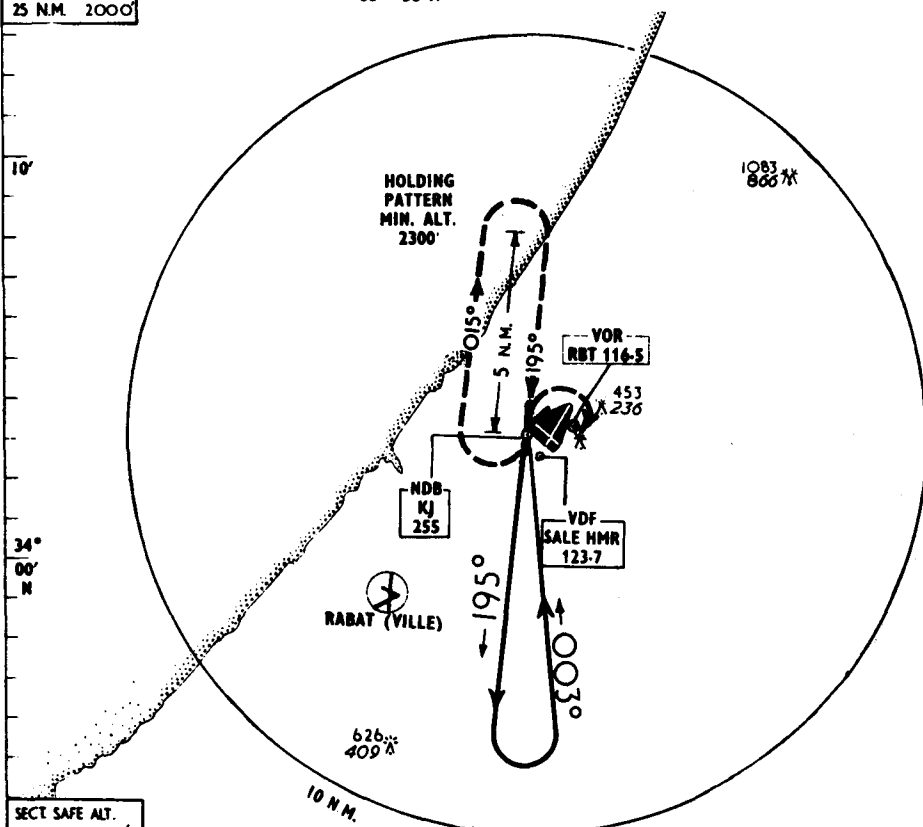
SERIAL No. 40107/1

DATE: 5-11-64

Reason for Re-issue:

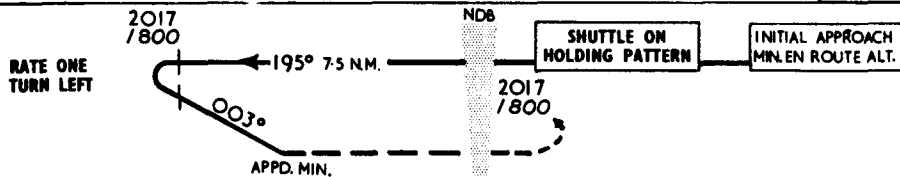
CRIT. HT. & GENERAL REVISION.

NDB. (BCP)
RABAT (SALE)

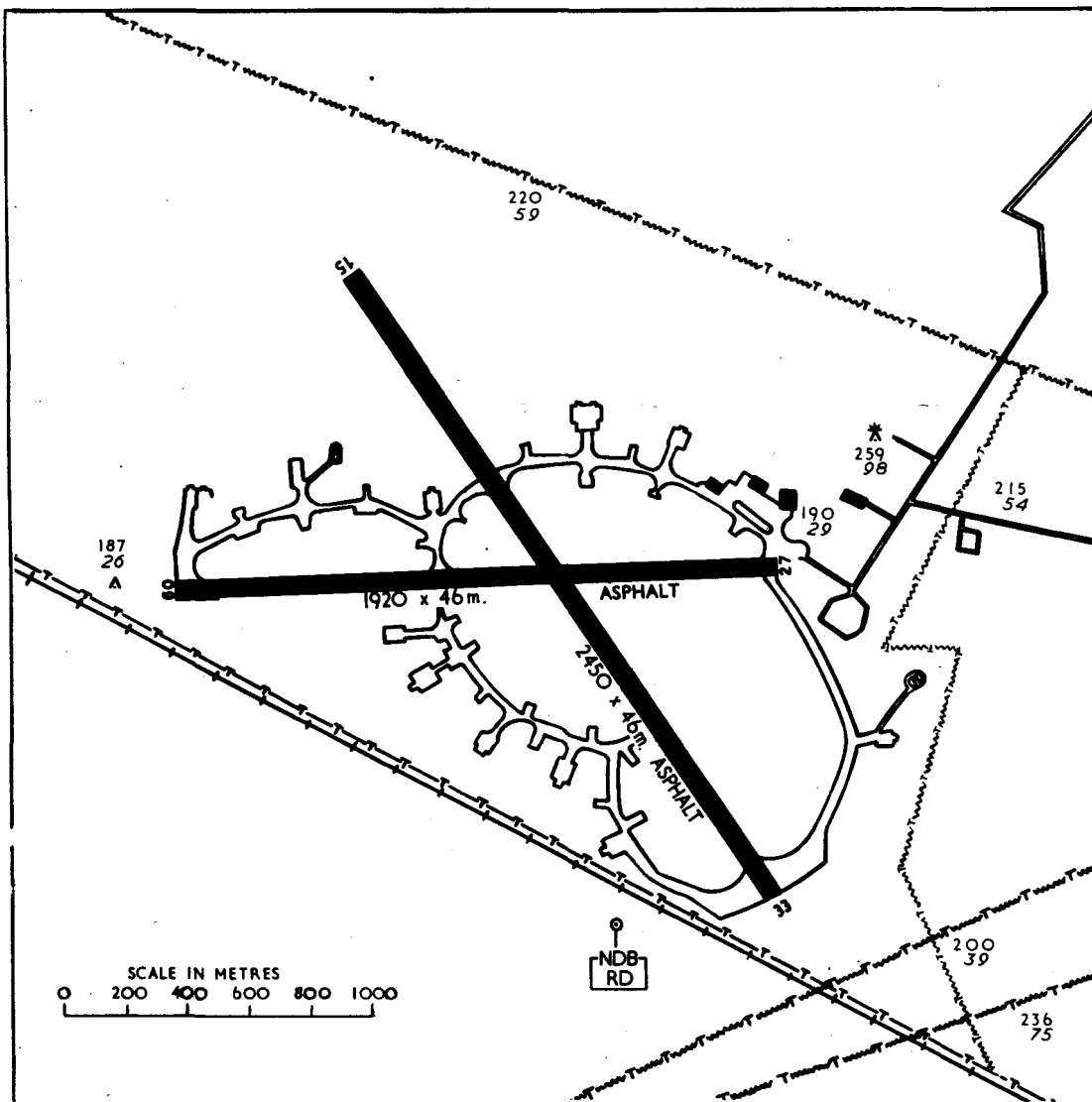


SECT. SAFE ALT.
25 N.M. 3000'

SECT. SAFE ALT.
25 N.M. 3000'



N.M. 14 13 12 11 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 N.M.



RAMAT DAVID AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
09 15 27 33	NIL	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
09 15 27 33	NIL	YES

OBSTRUCTION, ADME BEACON, ILLUMINATED 'T'

ELEVATIONS (FT): —

OFFICIAL AERODROME 161

LAT 32° 39' N, LONG 35° 11' E

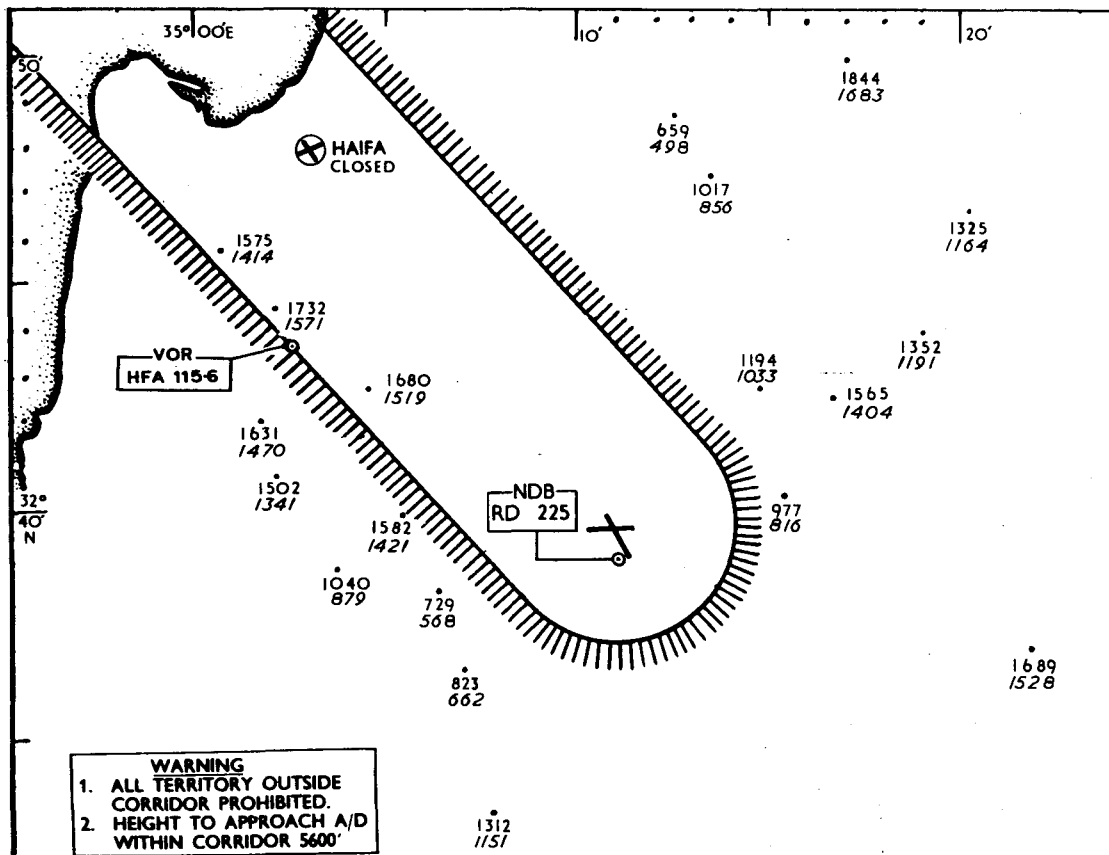
VARIATION 2° E Bearings all magnetic

SERIAL No. 50295/2 DATE: 7-7-65

Reason for Re-issue:

MINOR PRESENTATION.

AERODROME CHART
RAMAT DAVID



RAMAT DAVID VISUAL APPROACH CHART

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APPROACH

TOWER

RAMAT DAVID TWR.

118-9 124-3 120-5 121-5 E

TRANS ALT

4 000

FT.

ELEVATIONS (FT): —

OFFICIAL AERODROME

161

VARIATION 2° E Bearings all magnetic

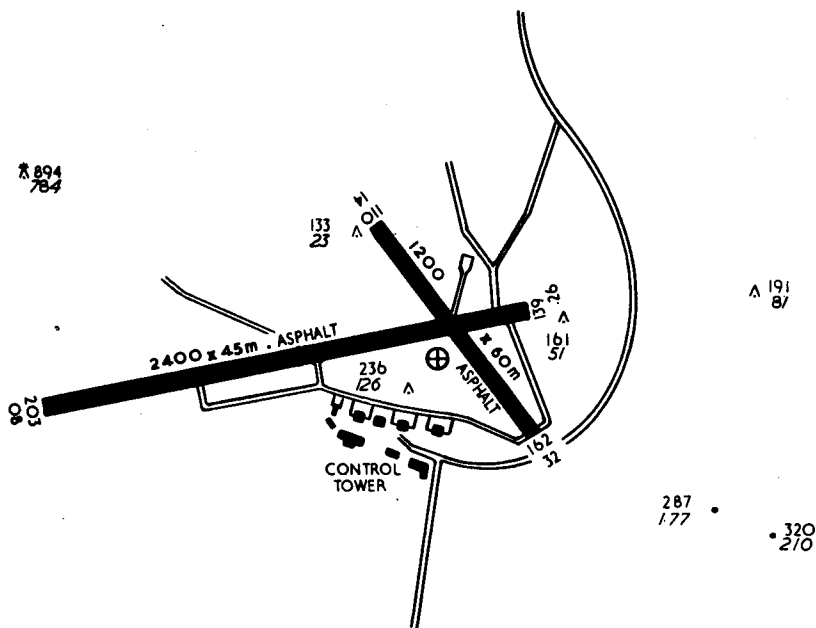
SERIAL No. 50295/2

DATE: 7-7-65

Reason for Re-issue:

NDB FREQUENCY

VISUAL APPROACH CHART
RAMAT DAVID



SCALE IN METRES
0 200 400 600 800 1000

WARNING: DO NOT CONFUSE NUMEROUS OBSTRUCTION LIGHTS ON SURROUNDING HILLS WITH AIRPORT LIGHTING.

RHODES

AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI CAO Type Angle
08, 26	NII	
14, 32	NII	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08, 26	H.I. green bi-directional	H.I. white
14, 32	NII	Goosenecks O/R

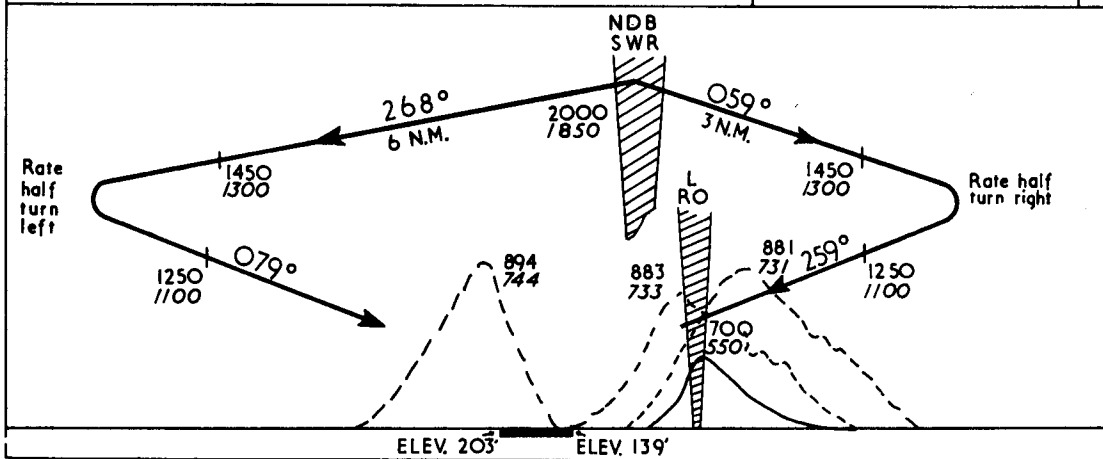
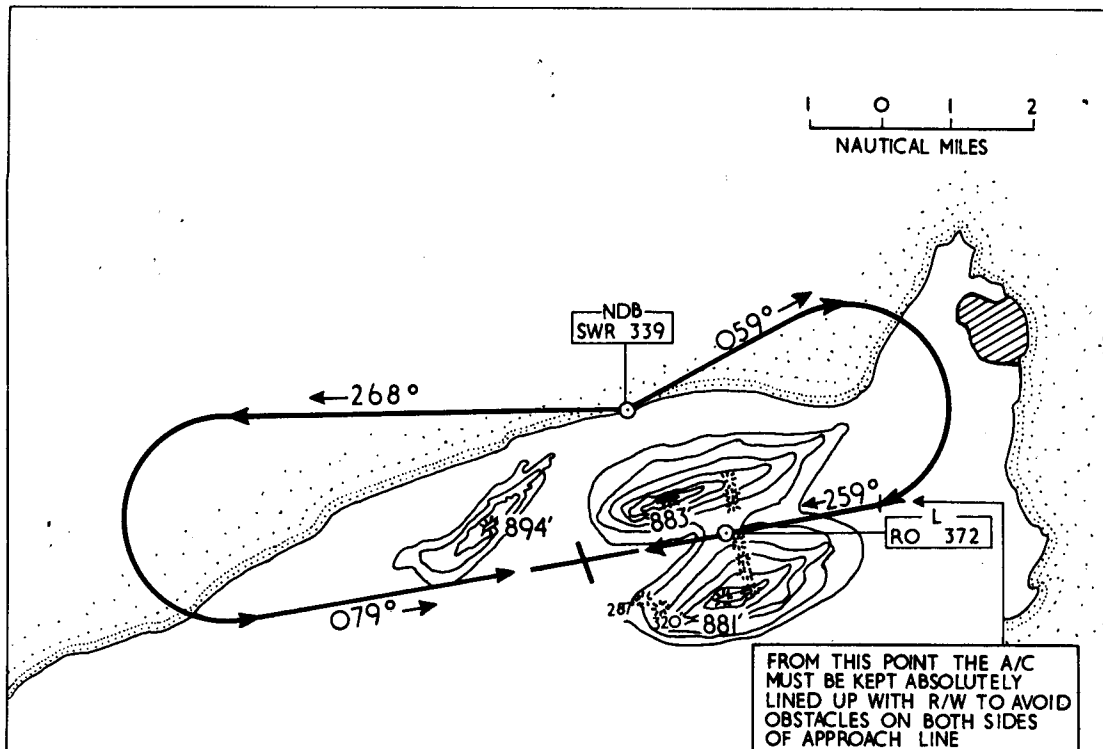
OBSTRUCTION, TAXIWAY, ADME BEACON
WIND AND LANDING INDICATOR, FLOOD, IDENT
BEACON 'R'(WHITE)

ELEVATIONS (FT): -

OFFICIAL AERODROME	150
LOWEST THRESHOLD (14)	110
LAT 36° 23' N, LONG 28° 07' E	
VARIATION 1° E	Bearings all magnetic
SERIAL No. 45913/2	DATE: 17.3.65

Reason for Re-Issue:
PRESENTATION

AERODROME CHART
RHODES



RHODES VISUAL APPROACH CHART

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TOWER A/C RHODES RADIO

119.7

TRANS ALT 5500 FT.

NOTES: 1. All circuits must be carried out north of the aerodrome.

2. Special Procedures for Runway 26.

- (a) In the event of a partial failure of obstruction lights, an approach for landing can still be carried out, provided that the obstruction lights of obstacles 881 Ft. and 894 Ft. are operating and the neck between the hills on the final approach is visible and final approach track can be accurately maintained by visual reference to the runway, or runway lights.
- (b) If the locator "RO" is not operating, an approach for landing may still be carried out, provided that all obstruction lights are in regular operation. In this case the aircraft's altitude when crossing the obstruction lights line of the neck, must be 600 Ft. (QFE) and final approach track must be accurately maintained by visual reference to the runway or runway lights.

ELEVATIONS (FT) : —

OFFICIAL AERODROME 150

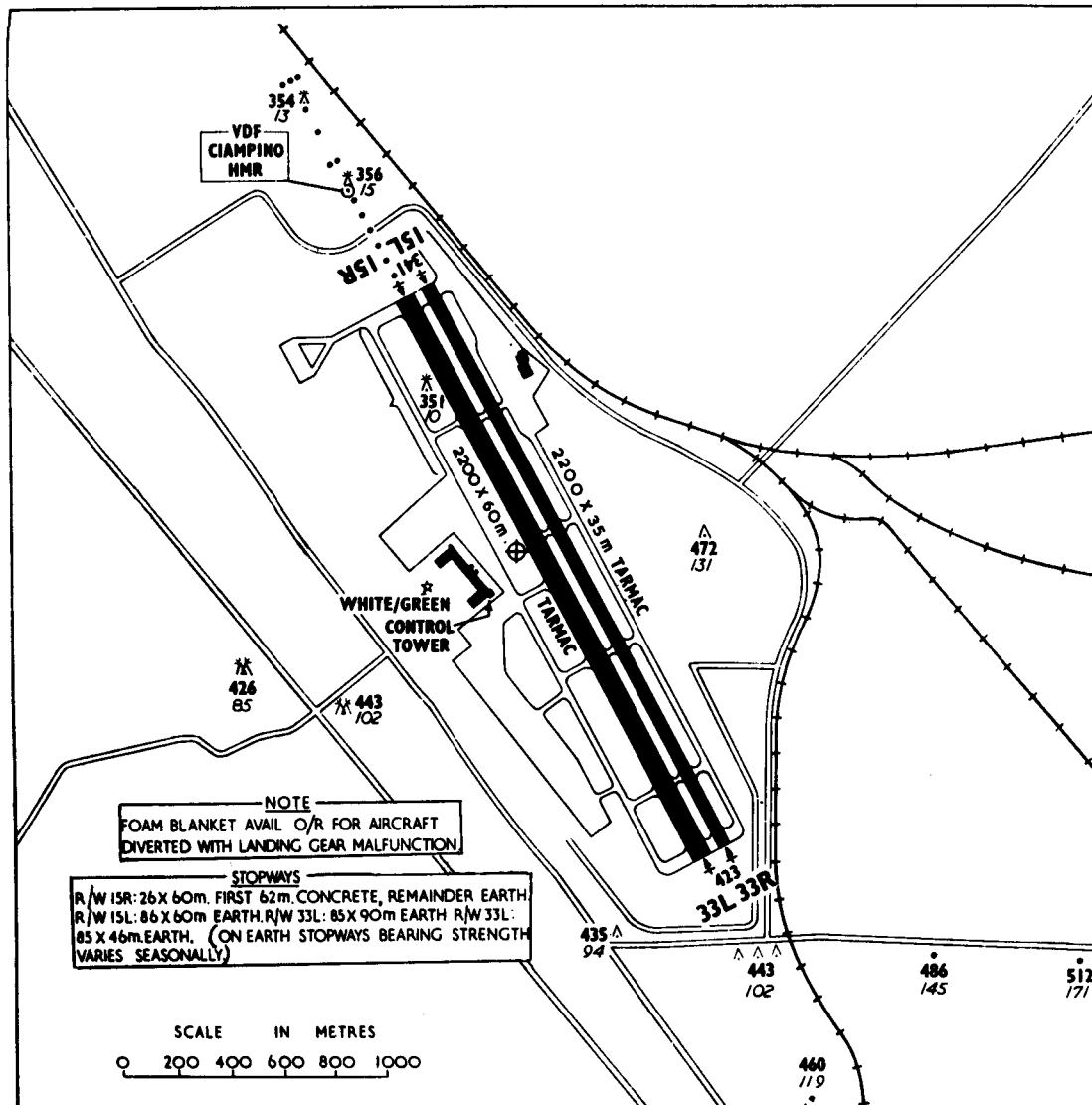
VARIATION 1° E Bearings all magnetic

SERIAL No. 50196/2 DATE: 17.3.65

Reason for Re-issue:

ELEVATIONS

VISUAL APPROACH CHART
RHODES



ROME (CIAMPINO) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI (CAO Type Angle
15R	White Centre-line with two amber cross-bars & one amber light 275m from threshold	
33L	Nil	
15L/ 33R	Nil	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
15R 33L	H.I. GREEN WITH GREEN WINGS.	White Var. Int. last 410m Amber
15L/ 33R	GREEN	

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, LANDING INDICATOR.
ANGLE OF APPROACH, GREEN ARROW/
RED CROSS ON EACH R/W

ELEVATIONS (FT) a.m.s.l. 443
above lowest T/H 102

OFFICIAL AERODROME 423

LOWEST THRESHOLD (15) 341

LAT 41° 48' N, LONG 12° 36' E

VARIATION 2° W Bearings all magnetic

SERIAL No. 40219/1 DATE: 13.10.64

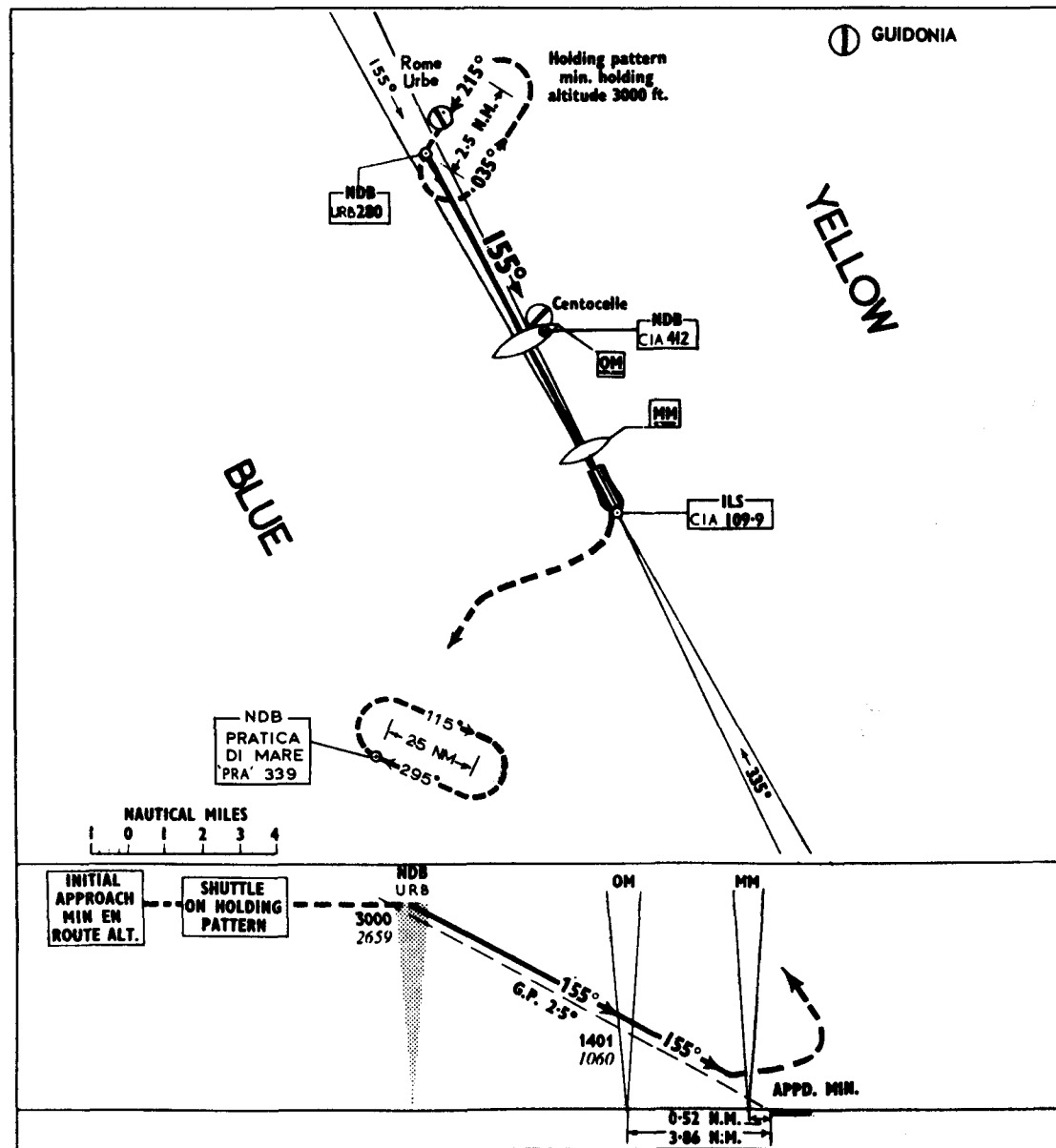
Reason for Re-issue: THRESHOLD LIGHTING

EFFECTIVE 22.10.64

AERODROME CHART
ROME (CIAMPINO)

- WARNINGS**
1. NORTHERN SECTION WESTERN TAXIWAY (THRESHOLD 15R TO APRON) LIGHTS 21m AGL.
 2. R/W 15L/33R. R/W LIGHTS 29m AGL.
 3. RAIN WATER COLLECTS ON R/W APPROX 450m FROM T/H OF R/W 15R.

4. THRESHOLD LIGHTS ARE 27-29ins AGL. R/W LIGHTS ARE 1.2m FROM PAVED EDGES, EXERCISE EXTREME CAUTION ENTERING OR LEAVING RUNWAYS.



ROME (CIAMPINO) ILS R/W 15R

BRITISH EUROPEAN AIRWAYS

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APPROACH ROME APP.

129.0

TOWER CIAMPINO TWR.

120.5 123.5 121.5

TRANS ALT 6000 FT.

G/S (KTS)	70	90	110	130	150
	m s	m s	m s	m s	m s
OM TO T/H	3m19s	2m34s	2m06s	1m47s	1m33s
DESCENT ON G/P (FT/MIN)	309	398	486	575	663
G/S	170	190	210	230	250
	m s	m s	m s	m s	m s
OM TO T/H	1m22s	1m13s	m s	m s	m s
DESCENT ON G/P (FT/MIN)	751	840			

CRITICAL HEIGHTS (FT)

TRIDENT AUTO OR R/D 400 MANUAL 500

OTHER TYPES WITH G/P 400 NO G/P 500

OVERSHOOT

RIGHT TURN CLIMBING TO 3000 2695
TO NDB 'PRA'

ELEVATIONS (FT) a.m.s.l. 3000
above lowest T/H 2659

OFFICIAL AERODROME 423

LOWEST THRESHOLD 341

THRESHOLD R/W 15R 341

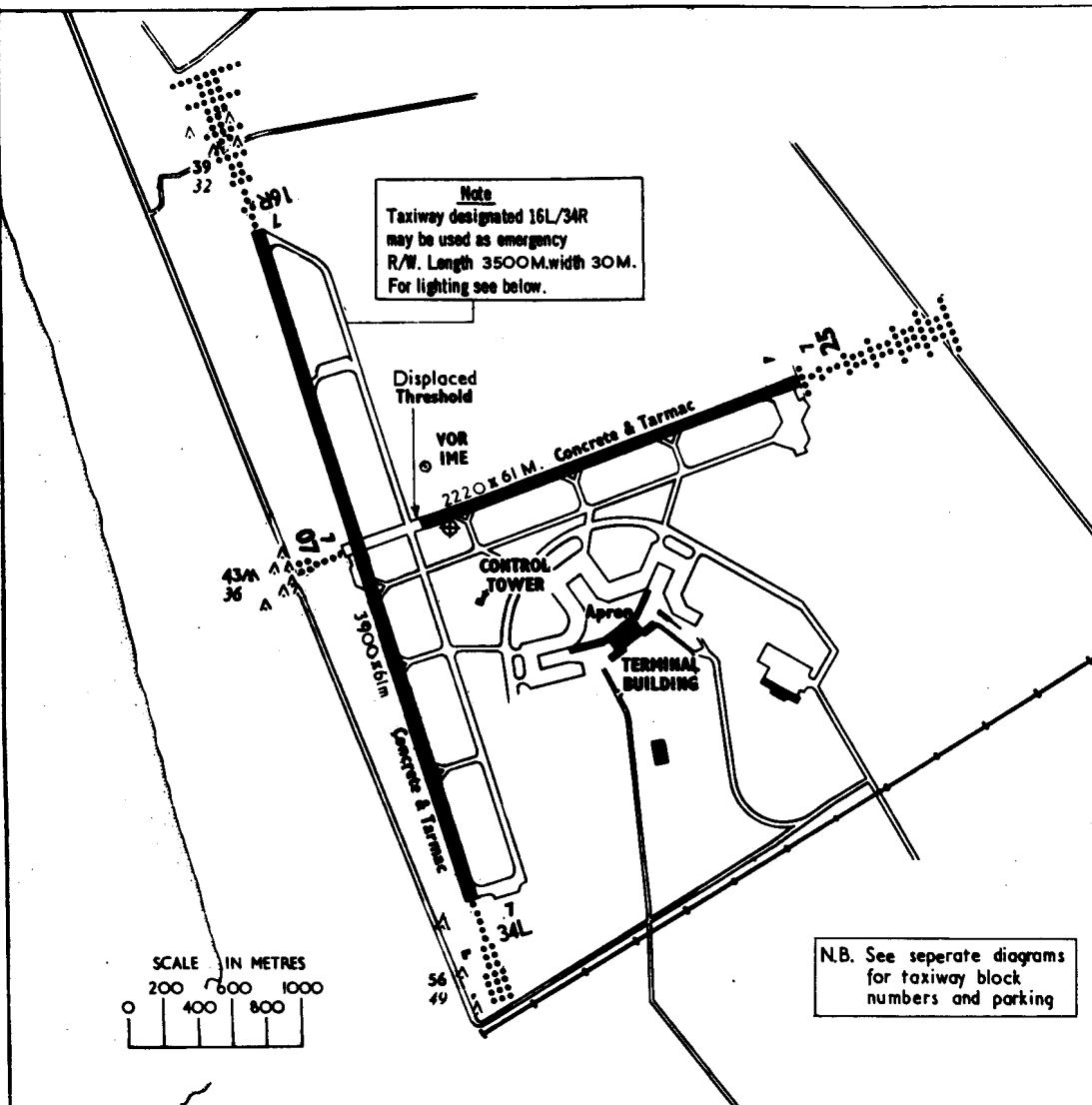
VARIATION 2°W Bearings all magnetic

SERIAL No. 40170/1 DATE: 13.10.64

Reason for Re-issue: NDB IDENTS.

EFFECTIVE 22.10.64

ILS R/W 15R
ROME (CIAMPINO)



ROME (FIUMICINO) AERODROME CHART

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH
07	WHITE H.I.
25	WHITE H.I. CENTRE LINE & BARS
16R	WHITE VAR. INT. CENTRE LINE & BARS WITH WHITE SEQUENCED FLASHERS.
34L	WHITE H.I.
16L	NIL
34R	
AUX	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
07	GREEN	WHITE VAR. INT. (TOUCHDOWN ZONE GREEN)
25		
16R		
34L		
16L	WHITE	WHITE
34R		
AUX		

BLUE, TAXIWAY, A'DME BEACON, APRON (AMBER
WITH GREEN GUIDES) ANGLE OF APPROACH.
STOPWAYS (RED).

ELEVATIONS (FT) :—

OFFICIAL AERODROME	7
LOWEST THRESHOLD ()	7
LAT	41° 48' N, LONG 12° 14' E
VARIATION	*W Bearings all magnetic
SERIAL No. 40404/1	DATE : 25 · 2 · 65
Reason for Re-issue:	
R/W 07/25 LENGTH	

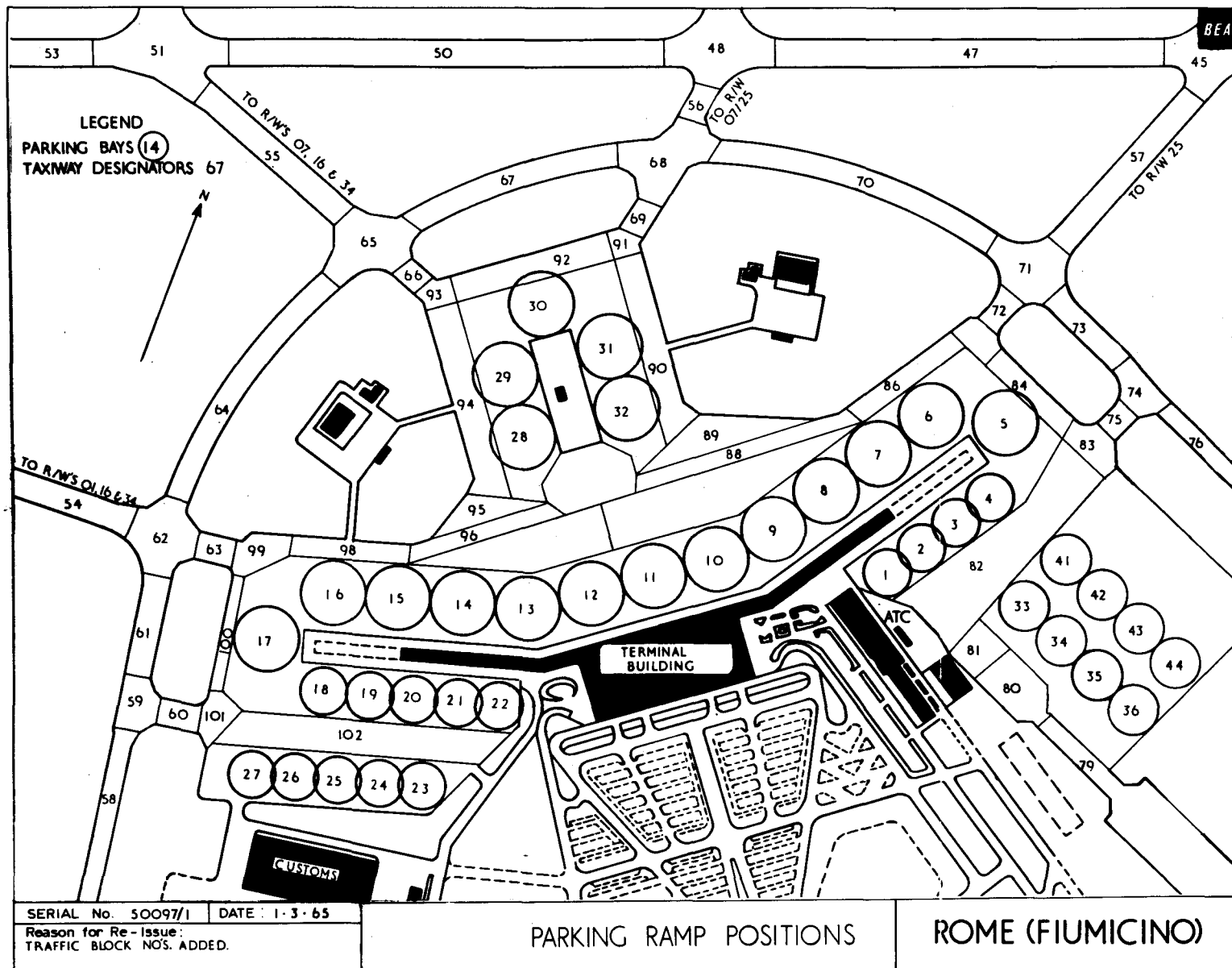
AERODROME CHART ROME (FIUMICINO)

NOTE
HT. OF ALL LIGHTS 20M A.G.L.

STOPWAY
R/W 25-405m.

WARNING
A/C HAVING LANDED ON R/W 16R & BEEN GIVEN TAXIING INSTRUCTIONS
TO CLEAR R/W AT SECOND LEFT ARE WARNED THAT R/W 07 IS NOT
INCLUDED SO THAT THE INTERSECTION REFERRED TO IS TIW 26
R/W 07/25 IS NEVER USED FOR TAXIING EXCEPT WHEN CLOSED FOR TAKE-OFF & LANDING.

NOTE
R/W'S EXTREMELY SLIPPERY WHEN WET.



SALONIKA AERODROME CHART

BRITISH EUROPEAN AIRWAYS

BEA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
28	H.I. WHITE CENTRE-LINE (855M.) & FIVE BARS	
10	VAR. INT. CENTRE-LINE	
17/35	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

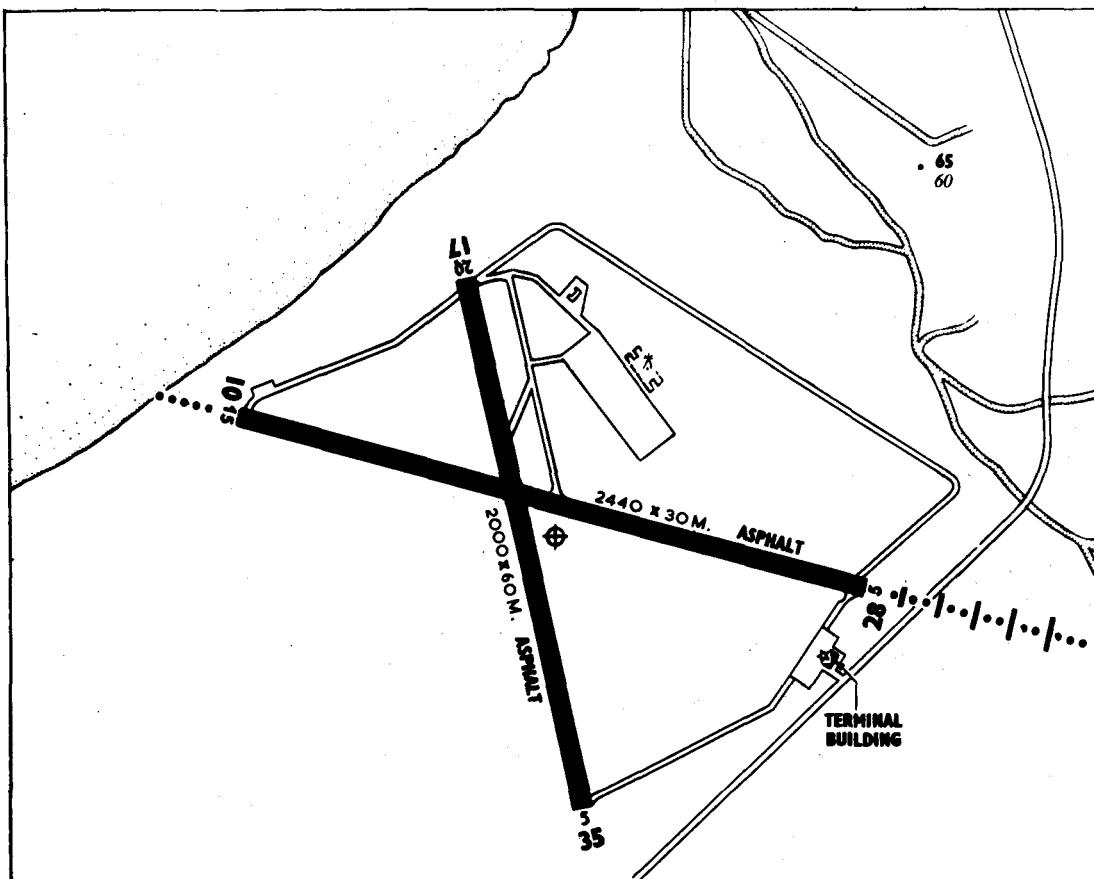
R/W	THRESHOLD	RUNWAY
28	H.I. BI-DIRECTION- AL GREEN	H.I. BI-DIRECTIONAL WHITE
10		
17/35		

OBSTRUCTION, TAXIWAY, IDENT. BEACON
'C' (WHITE), WIND AND LANDING INDICATOR,
FLOODLIGHTS, ADME BEACON.

ELEVATIONS (FT)

OFFICIAL AERODROME	13
LOWEST THRESHOLD (28 & 35)	5
LAT 40° 31' N, LONG 22° 58' E	
VARIATION 1° E Bearings all magnetic	
SERIAL No. 32406	DATE: 5-8-64
Reason for Re-issue:	

AERODROME CHART SALONIKA



SCALE IN METRES

0 200 400 600 800 1000

STOPWAYS: R/W's 10/28 92M.

CIRCUITS: RIGHT HAND FOR R/W's 10 & 17.

WARNING: TAXIWAYS IN POOR CONDITION,
TAXI WITH CAUTION.
NOTE: THE FIRST 200M. R/W 35 IS NOT
VISIBLE FROM CONTROL TOWER.

BEA

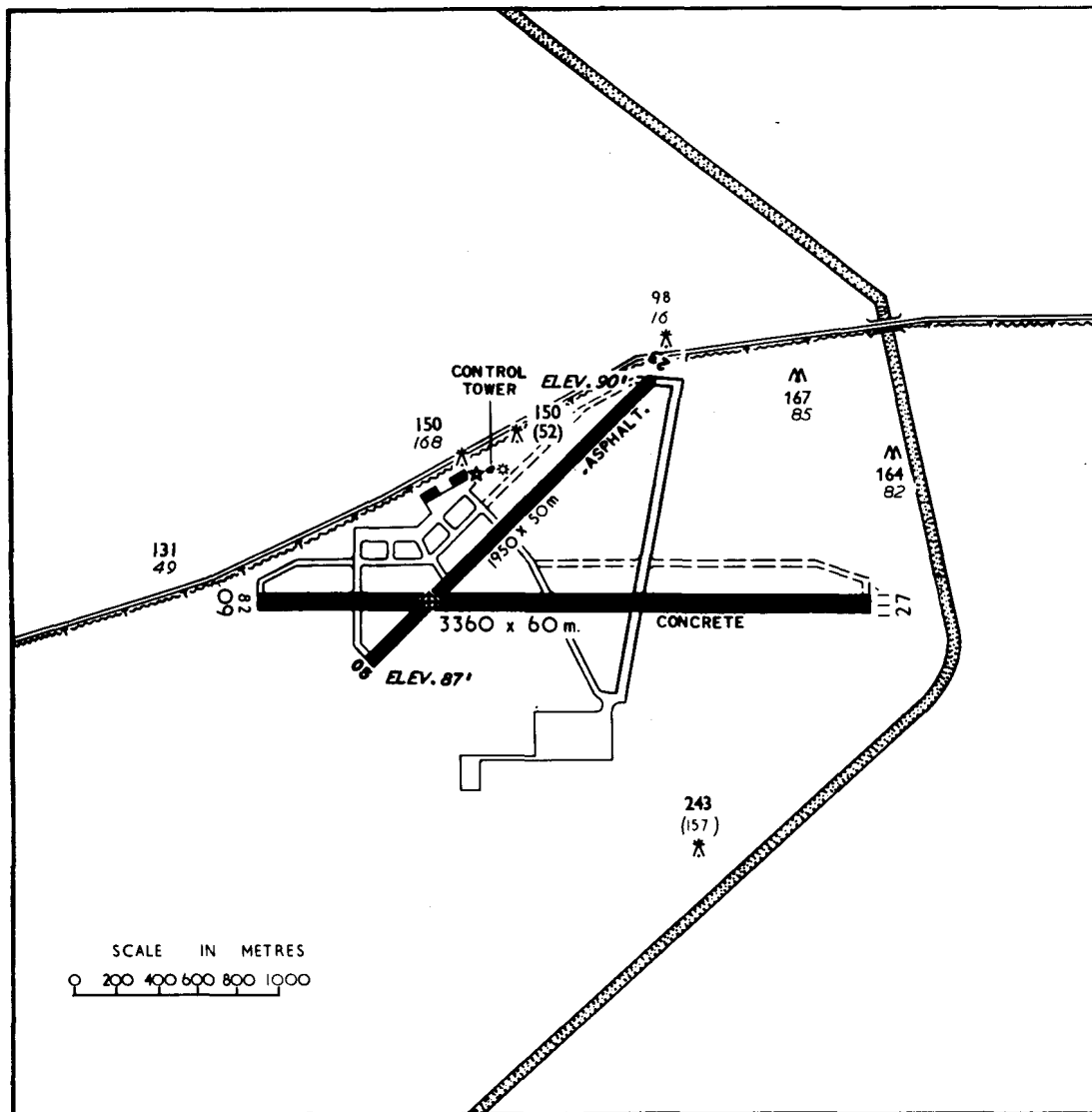
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VOR ROUTES
ADF ROUTES



HOLDING POINT	MIN. ALT. FEET	TIME MINS	INBOUND HEAD °M	TURN
NDB SWC	4000	2	130	RIGHT
VOR SWC (prop)	4000	2	345	LEFT
" " (jet)				
" (south holding)	6000	1	200	LEFT
" (north ")	6000	1	345	LEFT

OUTBOUND ROUTES



SEVILLE (SAN PABLO) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
05/23	NIL	
27	NIL	
09	NIL	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
05/23	L.I. WHITE	L.I. WHITE LAST 580M. YELLOW.
27	MED INT BLUE	HIGH INT. WHITE LAST 610M YELLOW
09		

OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR.

GREEN ARROW/RED CROSS AT EACH
THRESHOLD

ELEVATIONS (FT) : —

OFFICIAL AERODROME	98
LOWEST THRESHOLD (09)	82
LAT 37° 26' N, LONG 05° 54' W	
VARIATION 8° W	Bearings all magnetic
SERIAL No 50122/1	DATE 10.3.65

Reason for Re-issue:

GENERAL REVISION

AERODROME CHART SEVILLE (SAN PABLO)

- NOTES: 1. R/W LIGHTS SET 1½M FROM EDGES. 24 INS. AGL.
2. WHERE TAXIWAY LIGHTING EXISTS LIGHT ARE SET 3 M FROM EDGES. 24 INS. AGL.
3. EXTENSIVE W.I.P. BOTH ENDS R/W 09/27. 08-17

• 440
407

167
134
•

• 171
138

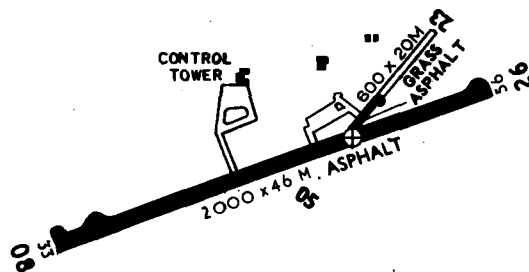
• 190
157

• 266
233

• 272
239

• 295
262

SCALE IN METRES
0 200 400 600 800 1000



TANGIER AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
08 & 26	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
08 & 26	GREEN	ELECTRIC WHITE

TAXIWAY, ADME BEACON
LANDING INDICATOR

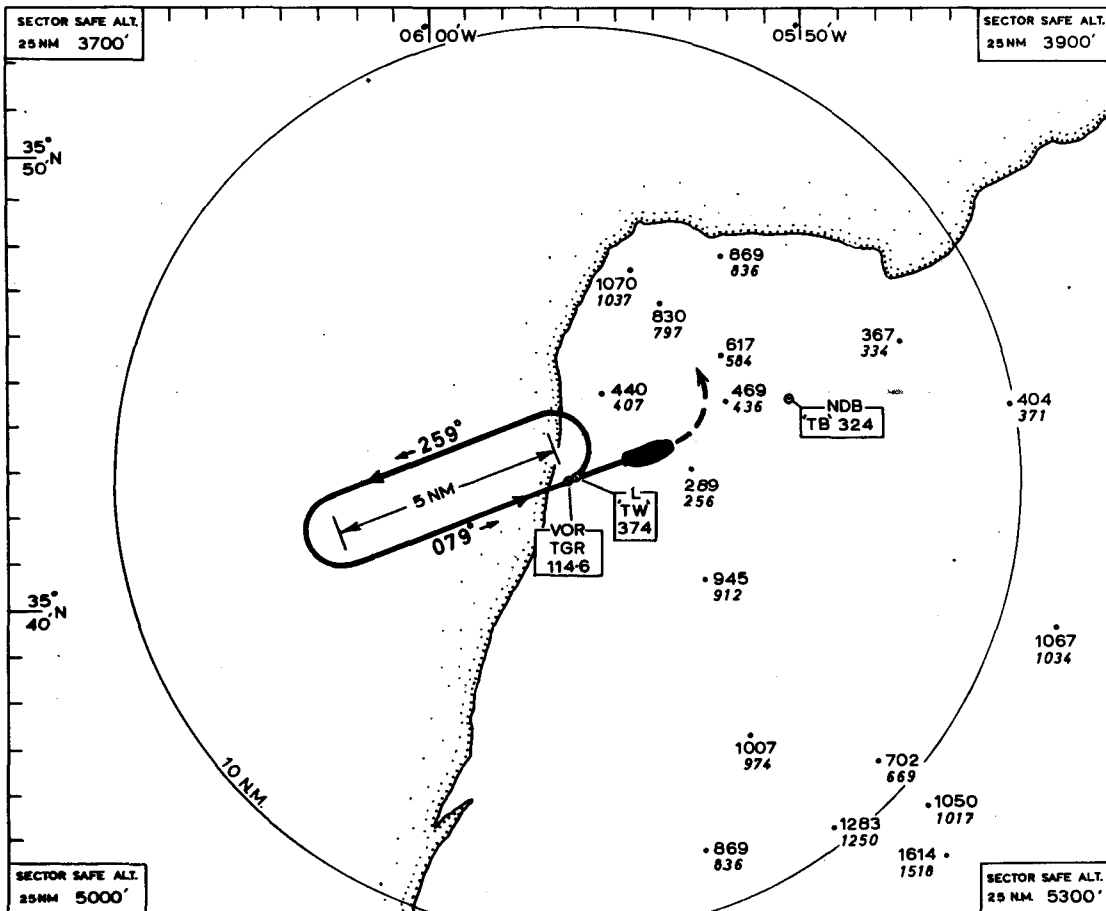
ELEVATIONS (FT.)

OFFICIAL AERODROME	56
LOWEST THRESHOLD (08)	33
LAT 35° 44' N, LONG 05° 54' W	
VARIATION 9° W Bearings all magnetic	
SERIAL No. 48632	DATE: 30.7.64
Reason for Re-issue:	

ELEVATED LIGHTING
RUNWAY, THRESHOLD & TAXIWAY LIGHTING SET
1½ METRES FROM EDGES, 26 INS. AGL.

WARNING 1. T/WAYS EXTREMELY NARROW WITH ELEVATED LIGHTS. FOUR
ENGINED A/C WITH LOW PROP CLEARANCE SHOULD TAXI
ON INBOARD ENGINES ONLY.
2. R/W SHOULDERS SOFT AND SHOULD BE AVOIDED

AERODROME CHART TANGIER



TANGIER

VOR R/W 08

BRITISH EUROPEAN AIRWAYS

BEA

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APPROACH TANGIER APPROACH

121.2

TOWER TANGIER TOWER

118.1

TRANS ALT 2500 FT.

G/S (KTS)	70	90	110	130	150
ABM VOR TO TURN	4.17	3.20	2.44	2.19	2.00
VOR TO ADME	1.07	0.52	0.43	0.36	0.31

G/S	170	190	210	230	250
ABM VOR TO TURN	1.46	1.35	1.26	1.18	1.12
VOR TO ADME	0.28	0.25	0.22	0.20	0.19

OVERSHOOT

CLIMB TO 1533 1500 ON 259°

ELEVATIONS (FT)

OFFICIAL AERODROME 56

LOWEST THRESHOLD 33

THRESHOLD R/W 08 33

VARIATION 9°W Bearings all magnetic

SERIAL No. 50293/1 DATE: 30-6-65

Reason for Re-issue:

APPROACH FREQ.

VOR R/W 08
TANGIER

INITIAL APPROACH MIN EN ROUTE ALT. SHUTTLE ON HOLDING PATTERN 2500 2467 VOR 114.6 RATE ONE TURN LEFT

RATE ONE TURN LEFT

933 900

Appd. min.

WSW

ENE

NM 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 NM

TEL AVIV (LOD) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
12	H.I WHITE (VARIABLE INTENSITY) SYSTEM OF CENTRE LINE BARS (834M IN LENGTH) 1 WHITE CROSS BAR 640M & 1 RED BAR 61M. FROM THRESHOLD	3°
30	NIL	3°
11/29	NIL	

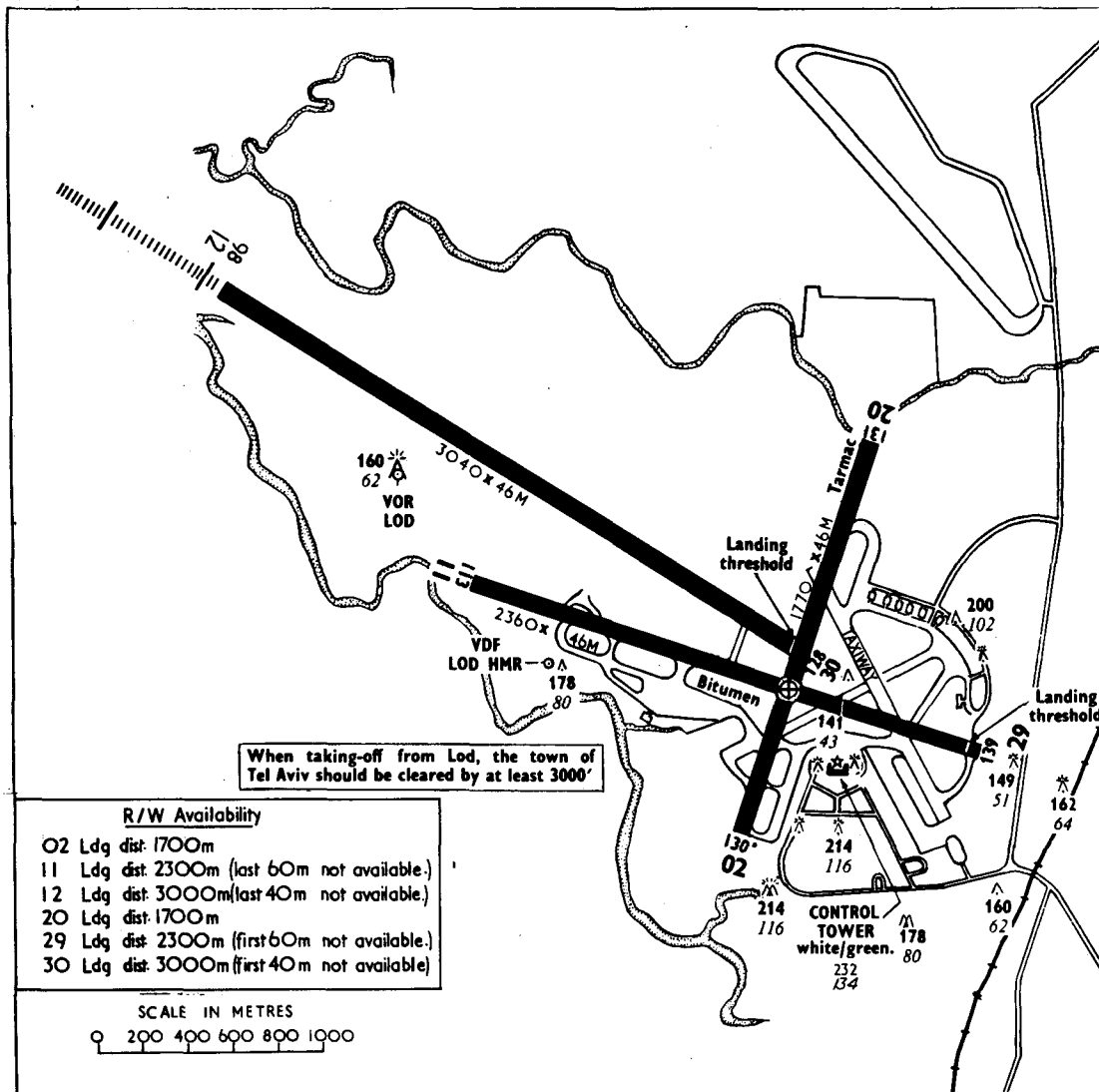
N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
11/29	GREEN (ELEVATED)	WHITE H.I. (ELEVATED 10 INS.) LAST 300M. YELLOW
12/ 30	GREEN R/W 12 ELEVATED	H.I. WHITE LAST 600M. YELLOW

OBSTRUCTION, TAXIWAY, ADME. BEACON

ELEVATIONS (FT)	a.m.s.l.	214
	above lowest T/H	76
OFFICIAL AERODROME		132
LOWEST THRESHOLD (12)		98
LAT 32° 00' N, LONG 34° 54' E		
VARIATION 2° E	Bearings all magnetic	
SERIAL No. 50285/1	DATE: 6.5.65	
Reason for Re-issue:		
APPROACH LIGHTING R/W 12		

AERODROME CHART TEL AVIV (LOD)



R/W Availability

- 02 Ldg dist: 1700m
- 11 Ldg dist: 2300m (last 60m not available.)
- 12 Ldg dist: 3000m (last 40m not available.)
- 20 Ldg dist: 1700m
- 29 Ldg dist: 2300m (first 60m not available.)
- 30 Ldg dist: 3000m (first 40m not available.)

SCALE IN METRES

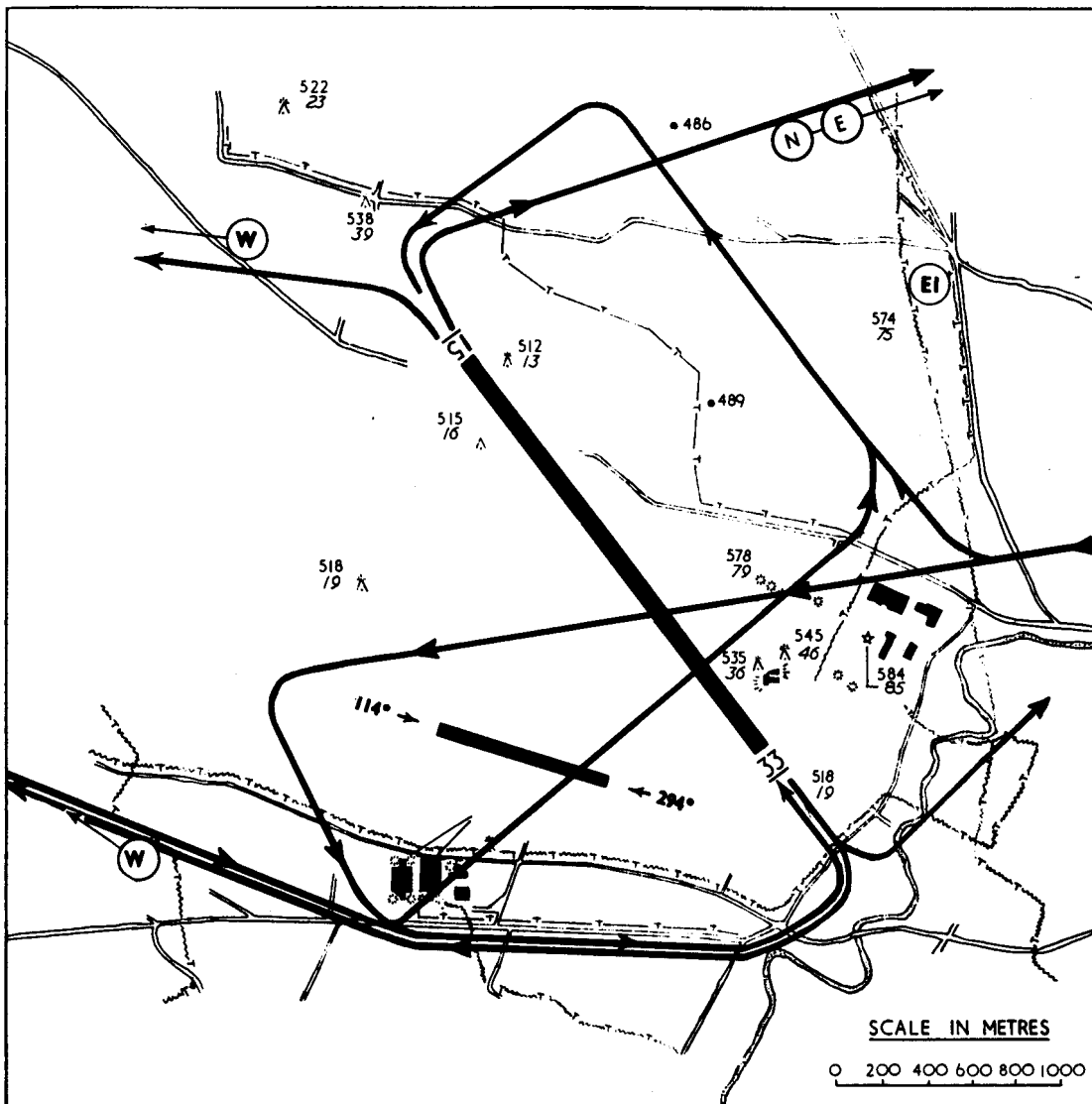
0 200 400 600 800 1000

STOPWAY

R/W 30 60 M. REINFORCED EARTH.
R/W 12 45 M. TARMAC.

NOTE:

AIRCRAFT EXPERIENCING RADIO FAILURE ARE TO CARRY
OUT RIGHT HAND CIRCUITS FOR R/W's 11, 12, & 20.



TOULOUSE VISUAL CIRCUITS

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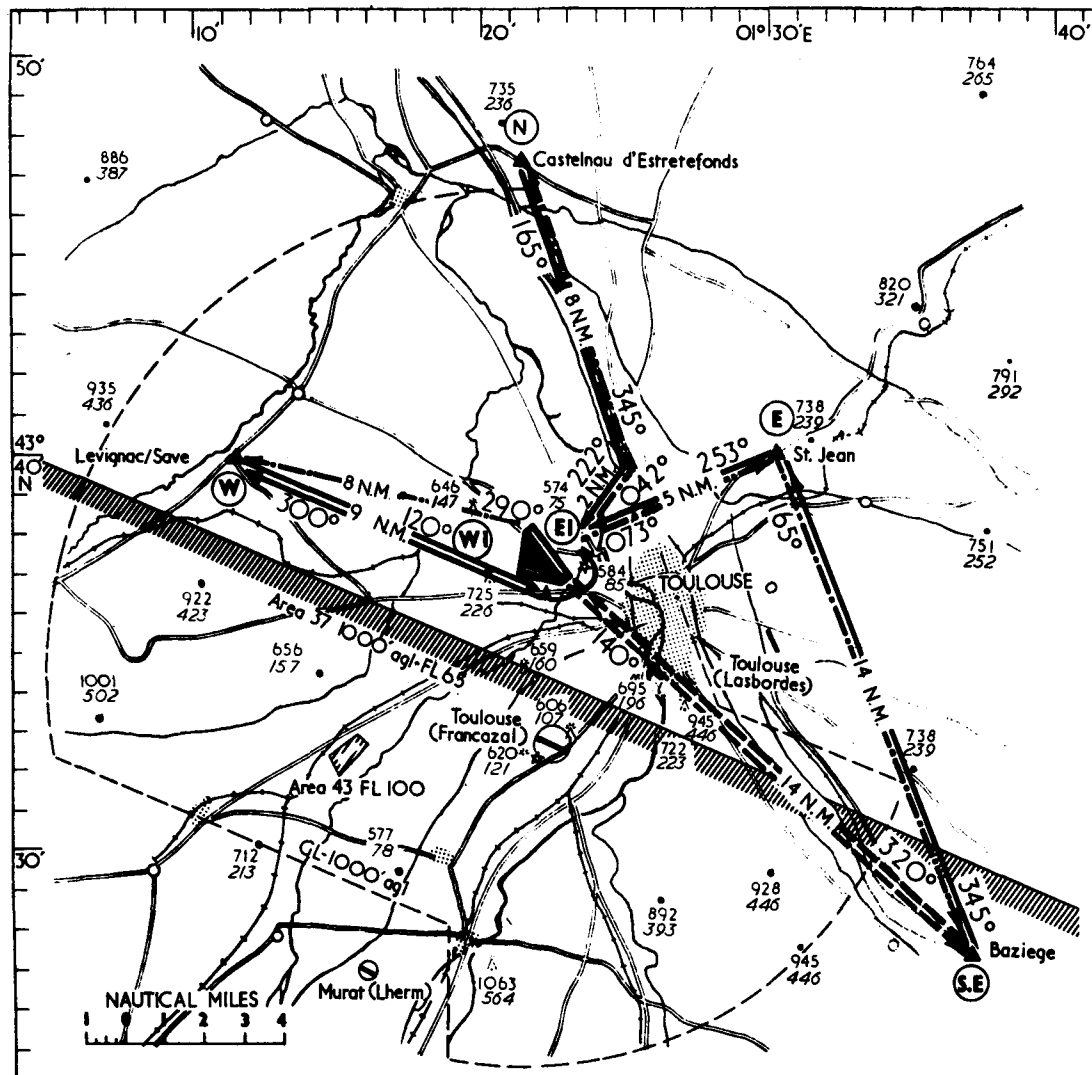
SERIAL No. 40213/1

DATE 18.11.64

Reason for Re-issue:

PROCEDURE

VISUAL CIRCUITS TOULOUSE



TOULOUSE (BLAGNAC)

VISUAL PROCEDURES (ROUTING)

BRITISH EUROPEAN AIRWAYS

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APPROACH

BLAGNAC APP.

121.1

TOWER

BLAGNAC APT.

118.1

TRANS ALT

2450

FT.

ELEVATIONS (FT.) .-

OFFICIAL AERODROME

499




VARIATION 6°W Bearings all magnetic

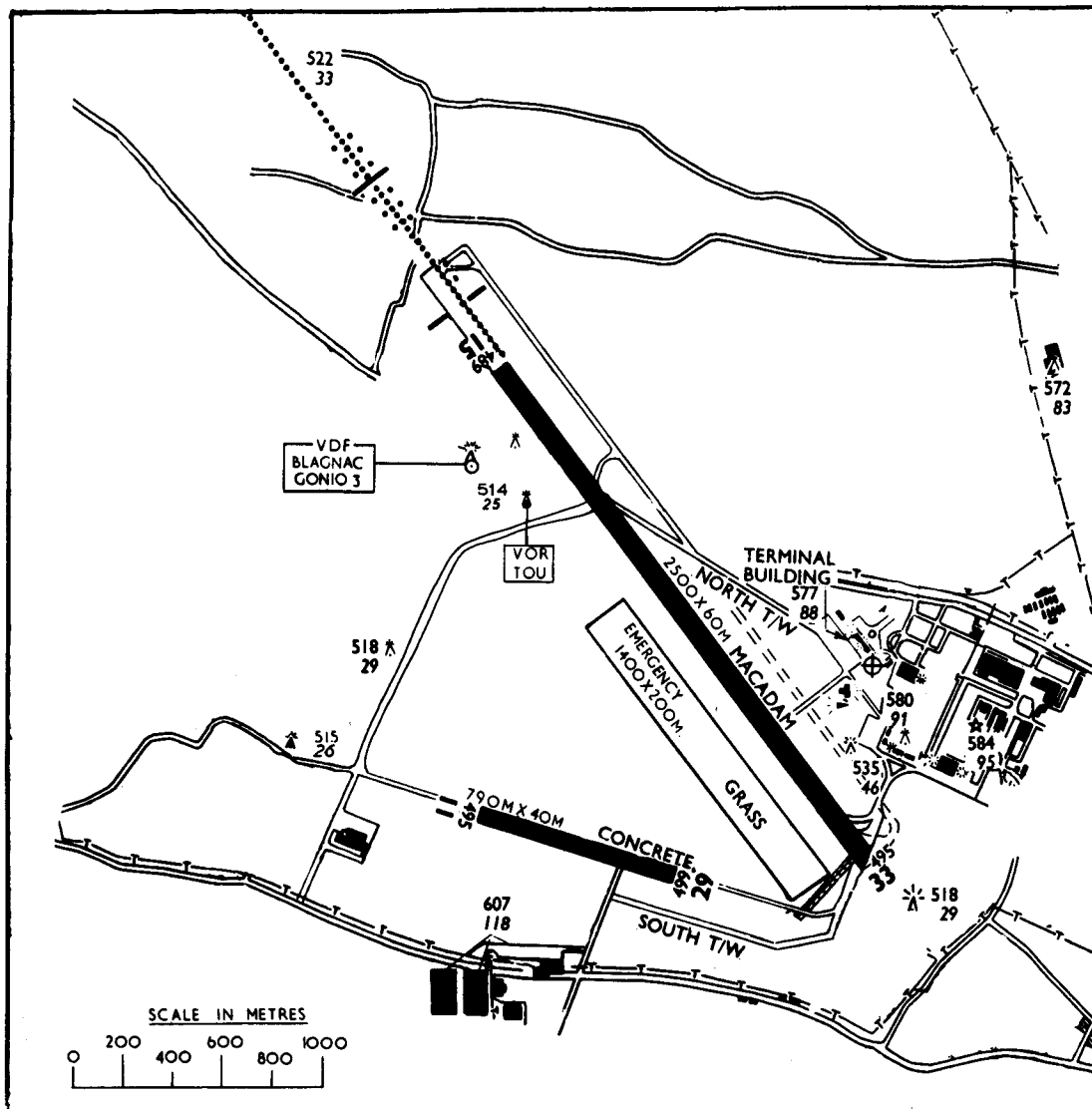
SERIAL No. 40212/1 DATE: 18.11.64

Reason for Re-issue:

PROCEDURE

VISUAL PROCEDURES (ROUTING)
TOULOUSE (BLAGNAC)

 Inbound (report at visual reporting points (WI) & (EI) at 1000 ft. AGL)
 Outbound
 Direct inbound & outbound by arrangement with Blagnac Approach



TOULOUSE (BLAGNAC) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
15	ALTERNATE RED & WHITE (1930M) TO LEFT HAND EDGE OF R/W WHITE BAR 30.5M. FROM THRESHOLD.	
33	NIL	

NB NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
15	GREEN WINGS	H.I. SODIUM L.I. WHITE
33	GREEN	L.I. WHITE

OBSTRUCTION, TAXIWAY, ADME BEACON 'P',
WIND AND LANDING INDICATOR.

ELEVATIONS (FT): -

OFFICIAL AERODROME	499
LOWEST THRESHOLD (15)	489
LAT 43° 37' N, LONG 01° 23' E	
VARIATION 6° W Bearings all magnetic	
SERIAL No. 40214/1	DATE: 15.1.65
Reason for Re-issue:	

TAXIWAYS

AERODROME CHART TOULOUSE (BLAGNAC)

ELEVATED LIGHTING: 1. RUNWAY LIGHTS 30 INS. AGL. 2. THRESHOLD LIGHTS 26 INS. AGL. 3. TAXIWAY LIGHTS 21 INS. AGL.
NOTE: MINIMUM WIDTH OF TAXIWAYS 14M.

SECT. SAFE ALT.
25 N.M. 2500'

10'

20'

01° 30' E

SECT. SAFE ALT.
25 N.M. 3200'

TOULOUSE (BLAGNAC)

L.BCN R/W. 15

BRITISH EUROPEAN AIRWAYS

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APPROACH	BLAGNAC APP.
	121-1 119-7
TOWER	BLAGNAC TWR.
	118-1
TRANS ALT	2450 FT.

G/S (KTS)	70	90	110	130	150
ABM TB TO TURN			2-44	2-19	2-01
TB TO ADME	3-13	2-30	2-02	1-44	1-30
G/S	170	190	210	230	250
ABM TB TO TURN	1-46	1-35	1-26	1-18	1-12
TB TO ADME	1-20	1-11			

CRITICAL HEIGHT (FT)

450

OVERSHOOT

CLIMBING TURN LEFT TO 1989 /500 THEN AS DIRECTED.

ELEVATIONS (FT): -

OFFICIAL AERODROME	499
LOWEST THRESHOLD	489
THRESHOLD R/W 15	489
VARIATION 6°W	Bearings all magnetic
SERIAL No. 40214/1	DATE: 15-1-65
Reason for Re-Issue:	
GENERAL REVISION	

L.BCN R/W. 15

TOULOUSE (BLAGNAC)

HOLDING & LETDOWN PATTERN

L/MKR
TN 378
T 75

575
86

820
337

TB 518

VDF
BLAGNAC
GON 10 3
119.9

VOR
TOU 117.7

584
95

NDB
TLE 356

945
450

TOULOUSE (FRANCAZAL)

606
117

620
131

SECT. SAFE ALT.
25 N.M. 2800'

SECT. SAFE ALT.
25 N.M. 2800'

INITIAL APPROACH
MIN. EN ROUTE ALT.

SHUTTLE ON
HOLDING

L
TB

RATE ONE
TURN LEFT
1989
/500

2 MIN.
1 MIN.

1489
/500

APPD. MIN.

VDF

NNW

SSE

N.M. 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 N.M.

R/W AVAILABILITY
 R/W 07 LANDING 1800 M. (FIRST 355 M. NOT AVAILABLE)
 R/W 25 TAKE-OFF 1800 M. (LAST 355 M. NOT AVAILABLE)

LANDING
THRESHOLD

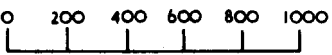
2155 X 46 M.

ASPHALT

CONTROL
TOWER

VDF
TREVISO GOMIO

SCALE IN METRES



TREVISO (SAN ANGELO) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

6 EA

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
07	NIL	
25	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
07	GREEN	WHITE/YELLOW
25	GREEN	WHITE/YELLOW

OBSTRUCTION, TAXIWAY, ADME BEACON
 ANGLE OF APPROACH INDICATORS R/W 07.

ELEVATIONS (FT) a.m.s.l. 254
above lowest T/H 203

OFFICIAL AERODROME 59

LOWEST THRESHOLD (25) 51

LAT 45° 39' N, LONG 12° 12' E

VARIATION 2° W | Bearings all magnetic

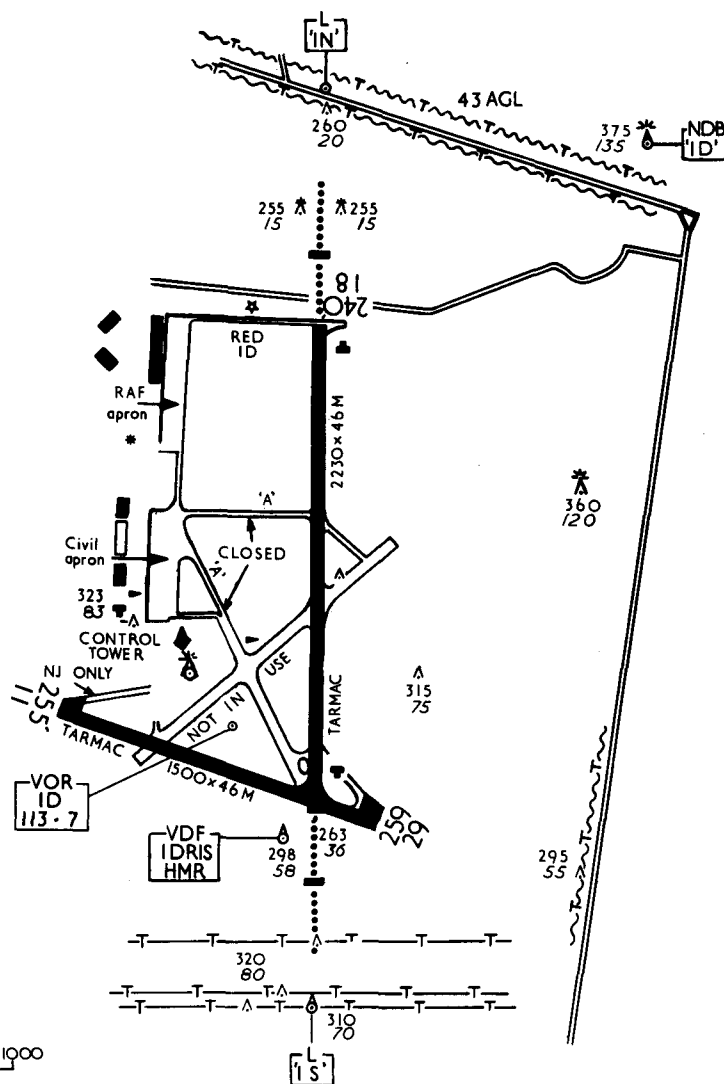
SERIAL No. 30464 DATE: 5.8.64

Reason for Re-issue:

STOPWAYS: R/W 07: -60M. R/W 25: -355M.

NOTES: 1. R/W & TAXIWAY LIGHTS ELEVATED 16"

AERODROME CHART TREVISO (SAN ANGELO)



TRIPOLI (IDRIS) AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
18 36	RED VARIABLE INT. CENTRE-LINE WITH CROSSBAR	
11 29	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
18 36	VAR. INT GREEN ELEVATED 13"	WHITE VAR. INT.
11 29	WHITE	ELECTRIC

OBSTRUCTION, TAXIWAY, BOUNDARY IDENT
BEACON 'ID' (RED). RED STOPBARS 90 METRES
FROM ENDS OF PAVED SURFACES R/W
18 & 36

ELEVATIONS (FT): -

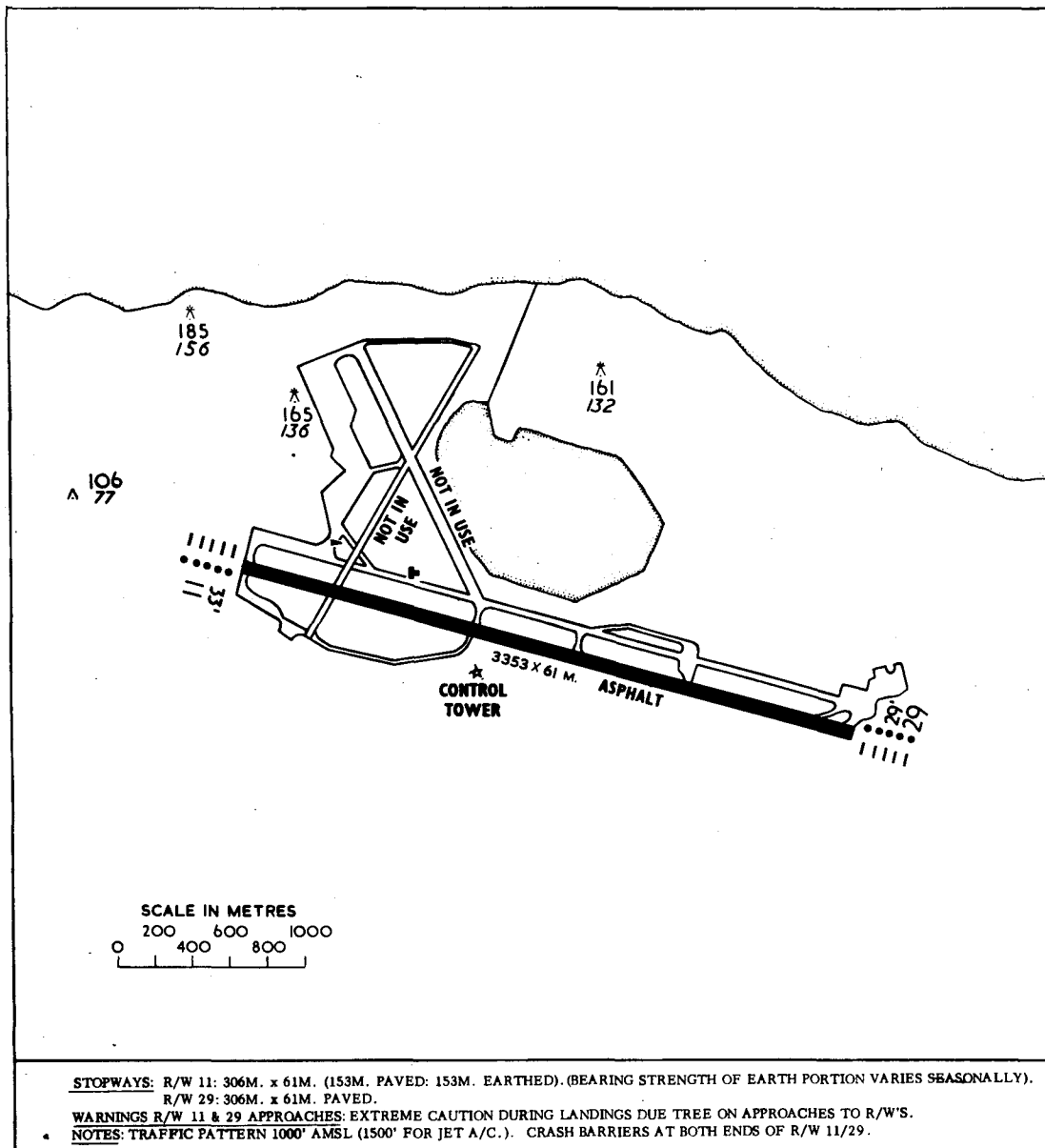
OFFICIAL AERODROME	255
LOWEST THRESHOLD (18)	240
LAT 32° 40' N, LONG 13° 09' E	
VARIATION 3° W Bearings all magnetic	
SERIAL No. 50207/2	DATE: 31.3.65

Reason for Re-Issue:

NDB IDENTS

AERODROME CHART TRIPOLI (IDRIS)

STOPWAYS: R/W 18: 92M. UNPAVED. R/W 36: 92M UNPAVED. (BEARING STRENGTH VARIES SEASONALLY).
WARNINGS: DURING AND AFTER HEAVY RAIN, EXCESSIVE WATER ACCUMULATES ON ALL RUNWAYS. PILOTS TO EXERCISE
EXTREME CAUTION LANDING & TAXIING. TAXIWAY "A" 15M. WIDE.



TRIPOLI (WHEELUS) AERODROME CHART

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LIGHTING

R/W	APPROACH	VASI ICAO Type Angle
11	H.I. WHITE	
29	H.I. WHITE	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
11	GREEN	ELECTRIC (CONTROLLABLE INT.)
29	GREEN	ELECTRIC (CONTROLLABLE INT.)

TAXIWAY, ADME BEACON

ELEVATIONS (FT)

OFFICIAL AERODROME	36
LOWEST THRESHOLD (29)	29
LAT 32° 54' N, LONG 13° 17' E	
VARIATION 2° W	Bearings all magnetic
SERIAL No. 50352/1	DATE: 27.7.65

Reason for Re-issue:

LIGHTING & OBSTRUCTIONS

AERODROME CHART TRIPOLI (WHEELUS)

SECT SAFE ALT
25 N.M. 1500'

SECT SAFE ALT
25 N.M. 1500'

TRIPOLI (WHEELUS) NDB (B.C.P)

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APPROACH

TOWER	WHEELUS	TWR
126.18	117.9	121.5
TRANS ALT		FT.
3000		

G/S (KTS)	70	90	110	130	150
NDB TO TURN	4.17	3.20	2.44	2.19	2.0
NDB TO ADME	2.09	1.40	1.22	1.09	1.0
G/S	170	190	210	230	250
NDB TO TURN	1.46	1.35			
NDB TO ADME	0.53	0.47			

OVERSHOOT

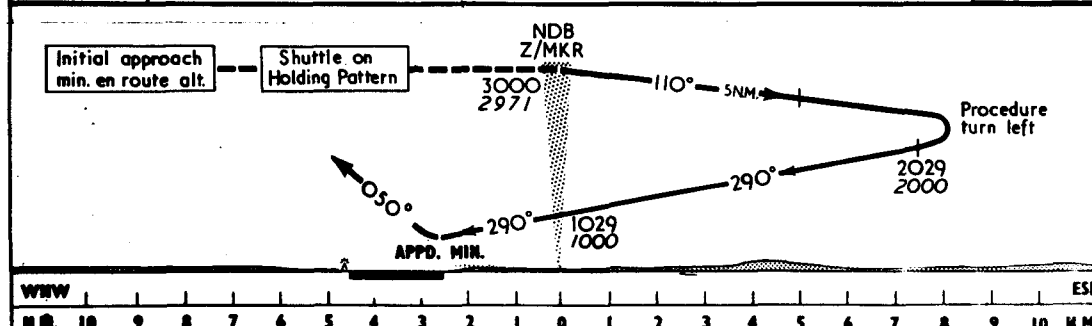
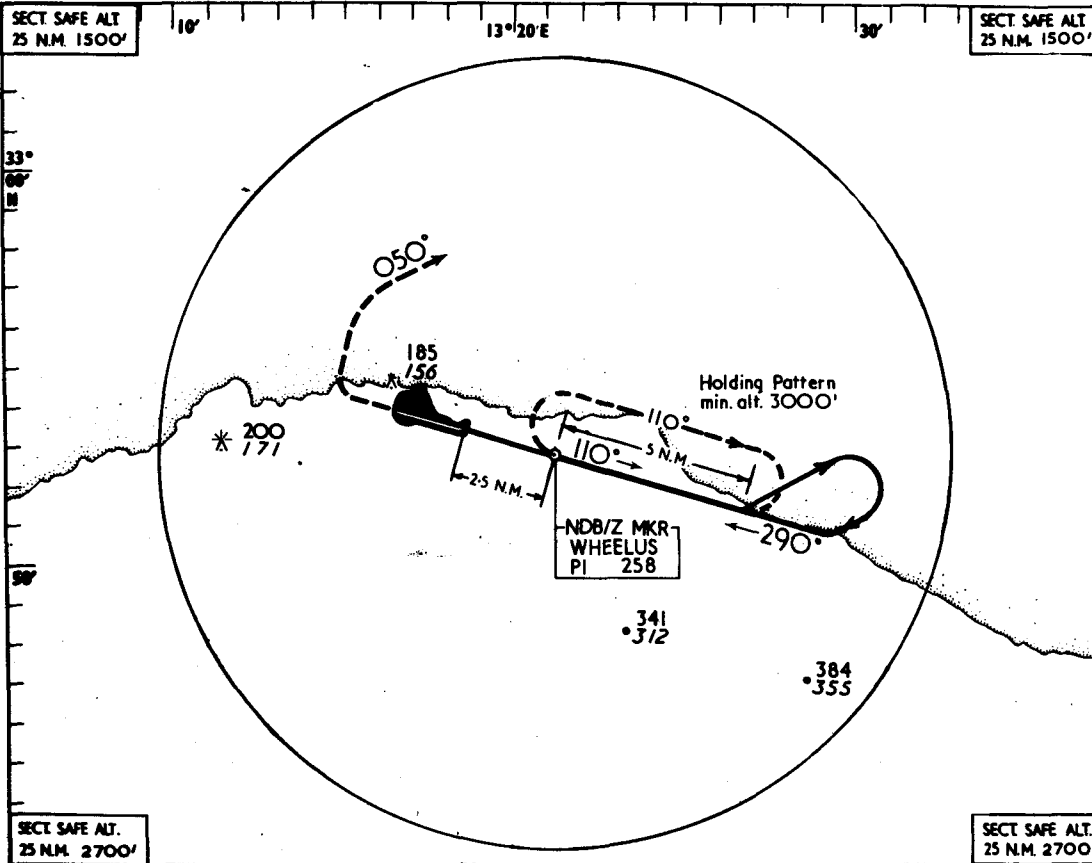
TURN RIGHT ONTO 050° CLIMBING TO
2000 / 971 & CONTACT TRIPOLI

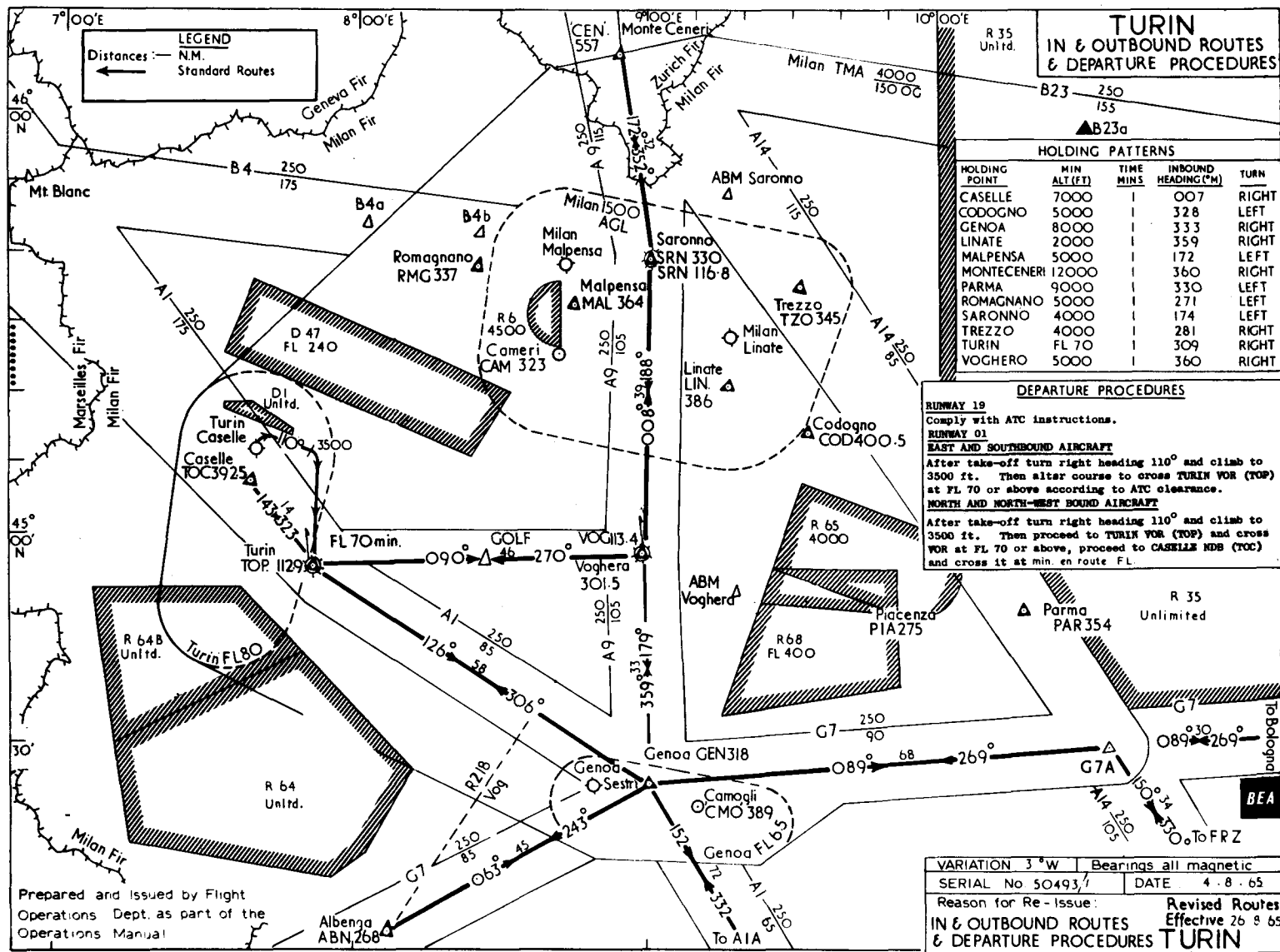
ELEVATIONS (FT)

OFFICIAL AERODROME	36
LOWEST THRESHOLD	29
THRESHOLD R/W 29	29
VARIATION 2°W	Bearings all magnetic
SERIAL No. 50352/1	DATE: 27. 7. 65
Reason for Re-Issue:	

OVERSHOOT & OBSTRUCTIONS

NDB (B.C.P) TRIPOLI (WHEELUS)





VALENCIA AERODROME CHART

BRITISH EUROPEAN AIRWAYS

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
12/30	HIGH INTENSITY	
04	MED. INT. CENTRE-LINE	
22	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

R/W	THRESHOLD	RUNWAY
12/30	GREEN WINGBARS	HI. WHITE (LAST 600M. YELLOW)
04	GREEN WINGBARS	M.I. WHITE (LAST 420M. YELLOW)
22		

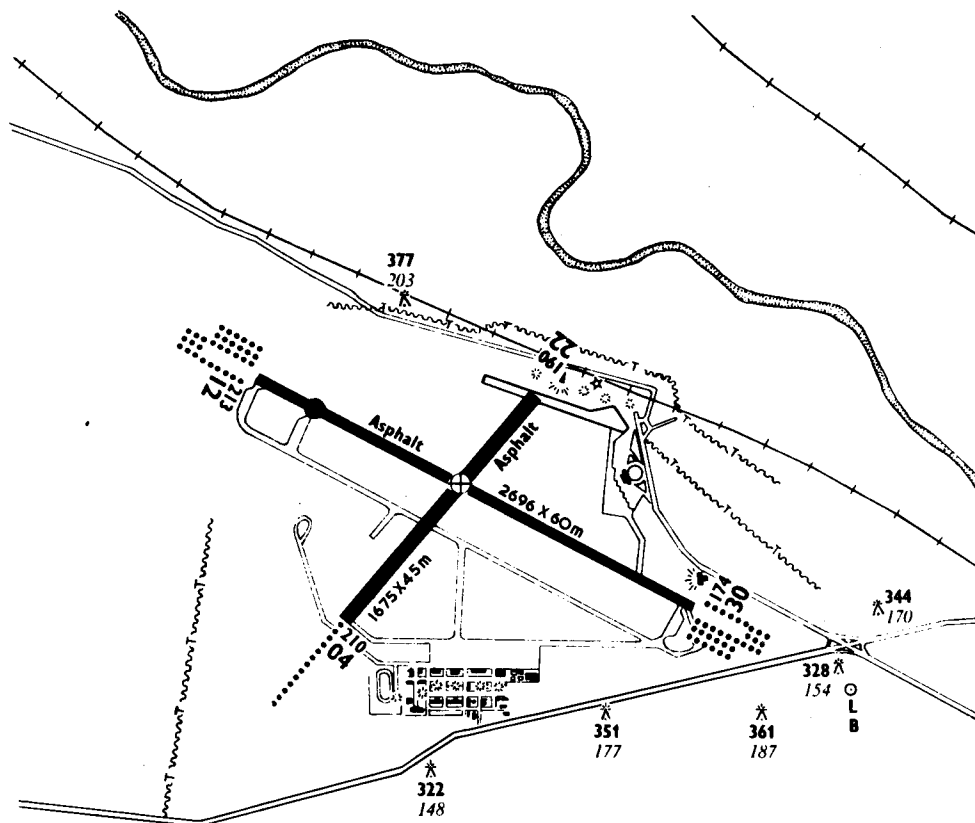
OBSTRUCTION, TAXIWAY, ADME BEACON
WIND INDICATOR, FLOODLIT PARKING AREA

ELEVATIONS (FT) :—

OFFICIAL AERODROME	213
LOWEST THRESHOLD (30)	174
LAT 39° 29' N, LONG 00° 29' W	
VARIATION 6° W	Bearings all magnetic
SERIAL No 50259/3	DATE 16. 5. 65.

Reason for Re-Issue:
VARIATION.

AERODROME CHART VALENCIA



SCALE IN METRES
0 200 400 600 800 1000

STOPWAYS R/Ws 12/30-305m.

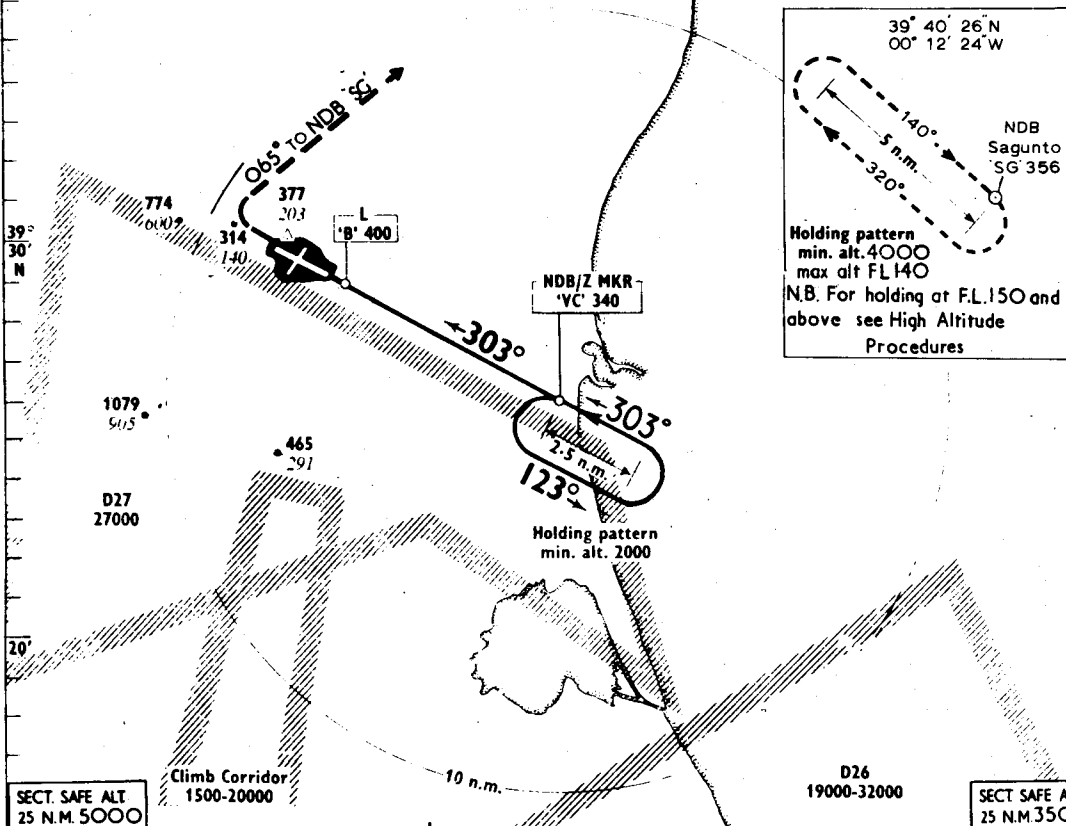
SECT. SAFE ALT
25 N.M. 6500

00°30'W

20'

10'

SECT. SAFE ALT
25 N.M. 5600

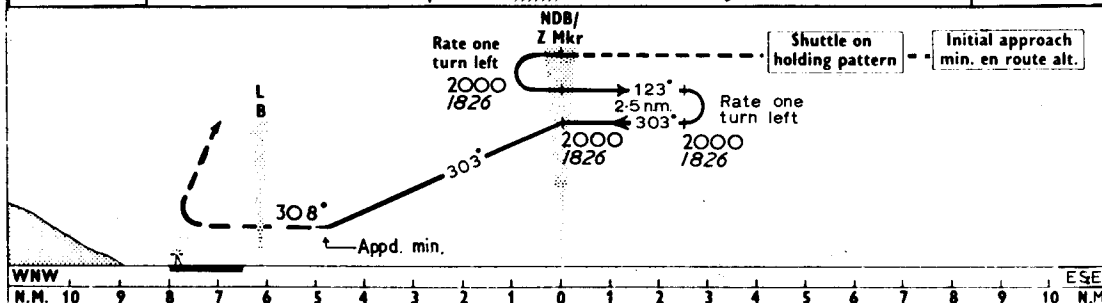


SECT. SAFE ALT
25 N.M. 5000

Climb Corridor
1500-20000

D26
19000-32000

SECT. SAFE ALT
25 N.M. 3500



WNW N.M. 10 9 8 7 6 5 4 3 2 1 0 1 2 3 4 5 6 7 8 9 10 N.M. ESE

VALENCIA NDB R/W 30

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APPROACH	VALENCIA APP.
TOWER	VALENCIA TWR.
TRANS ALT	5000 FT.

G/S (KTS)	70	90	110	130	150
ABM NDB TO TURN			1.22	1.10	1.00
NDB ADME	5.33	4.19	3.32	2.59	2.35
G/S	170	190	210	230	250
ABM NDB TO TURN	0.53	0.48	0.43	0.39	0.36
NDB ADME	2.17	2.03	1.51		

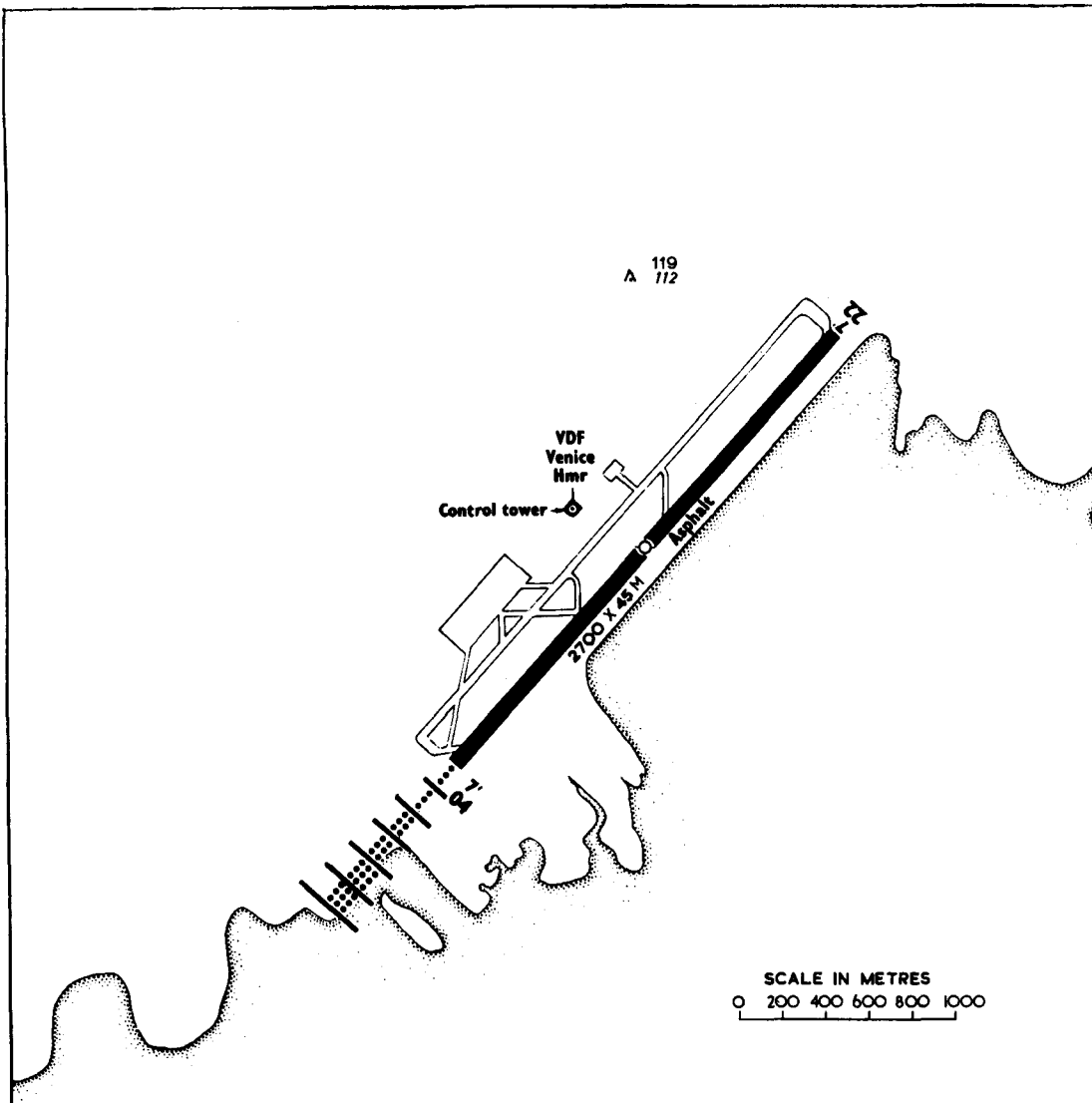
OVERSHOOT

Climb on 308° to 1100 926, turn right
onto 065° to 4000 3826 at NDB 'SG'.

ELEVATIONS (FT)

OFFICIAL AERODROME	213
LOWEST THRESHOLD	174
THRESHOLD R/W 30	174
VARIATION 6°W	Bearings all magnetic
SERIAL No 50259/3	DATE 16.6.65
Reason for Re-Issue:	VARIATION.

NDB R/W 30
VALENCIA



VENICE (TESSERA) AERODROME CHART

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LIGHTING

VASI
ICAO
Type
Angle

R/W	APPROACH	
04	H.I. WHITE CENTRE-LINE & BAR	
22	NIL	

N.B. NO ENTRY UNDER VASI ANGLE INDICATES NO INSTALLATION

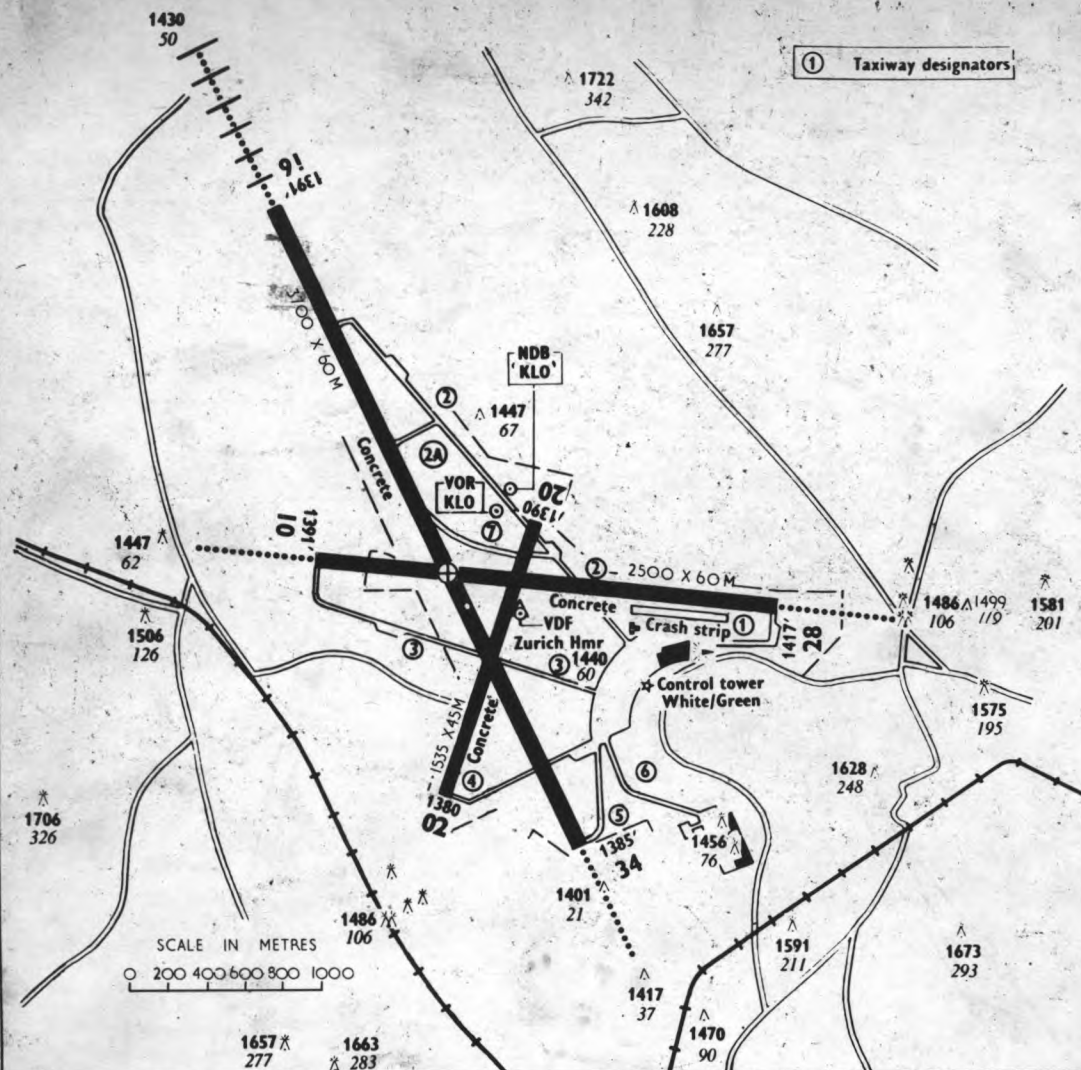
R/W	THRESHOLD	RUNWAY
04	YES	YES
22	YES	YES

OBSTRUCTION, TAXIWAY, ADME BEACON,
LANDING & WIND INDICATOR, IDENT BEACON 'L'
(GREEN)

ELEVATIONS (FT)	a.m.s.l. 276 above lowest T/H 269
OFFICIAL AERODROME	7
LOWEST THRESHOLD (04 & 22)	7
LAT 45° 30' N, LONG 12° 21' E	
VARIATION 2° W	Bearings all magnetic
SERIAL No. 47382	DATE: 5.8.64
Reason for Re-issue:	

AERODROME CHART
VENICE (TESSERA)

STOPWAYS: R/W 04: 60 M. PAVED R/W 22: 60 M. PAVED.



ZURICH AERODROME CHART

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V.A.S.I.	
ICAO	
Type	
Angle	

R/W	APPROACH.	Type of Angle
16	H.I. WHITE CENTRE-LINE & BARS L.I. WHITE CENTRE-LINE	
10	L.I. WHITE CENTRE-LINE	
28	L.I. WHITE CENTRE-LINE	35°
34	L.I. WHITE CENTRE-LINE	
02, 20	NIL	

R/W	THRESHOLD	RUNWAY
16	H.I. & L.I. GREEN	H.I. WHITE ELEVATED (LAST 610M. AMBER) TDZ(DOUBLE-ROW WHITE FOR FIRST 700M.)
10	L.I. GREEN	L.I. WHITE ELEVATED (LAST 610M. AMBER)
28	H.I. & L.I. GREEN	H.I. & L.I. WHITE ELEVATED (LAST 610M. AMBER)
34		
02, 20	L.I. GREEN	L.I. WHITE (LAST 505M. YELLOW).

ELEVATIONS (FT): —

LOWEST THRESHOLD (O2)	1380
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LAT 47° 28' N, LONG 08° 33'

VARIATION 3° W	Bearings all magnetic
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SERIAL No. 5013611	DATE : 8.3.65
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Reason for Re-Issue:

OBSTRUCTION ADDED

AERODROME CHART ZURICH

NOTES

1. THE MINIMUM ALTITUDES AND FLIGHT LEVELS SHOWN ON THE AIRWAYS AND ADR'S ARE THOSE PUBLISHED BY THE STATE CONCERNED AND DO NOT NECESSARILY GIVE SAFE CLEARANCE OVER THE GROUND COVERED BY THE AIRWAY OR ADR.
2. ALL HOLDING PATTERNS 1 MIN. UNLESS OTHERWISE STATED. AIRCRAFT APPROACHING FROM THE NORTH ON G. 9 WILL HOLD AT TRASADINGEN AT FL 70 & BELOW AND AT SCHAFFHAUSEN ABOVE FL 70.
3. TURBINE A/C MUST NOT EXCEED 220 KTS. IAS AT OR BELOW FL 140 OR 240 KTS. IAS AT OR ABOVE FL 145 WHEN HOLDING.
4. IN THE EVENT OF RADIO FAILURE AIRCRAFT SHALL APPROACH VIA THE INBOUND ROUTES SHOWN BY CONTINUOUS LINES AND CARRY OUT A STANDARD INSTRUMENT APPROACH AS DEPICTED ON THE APPROACH CHART.

